
Port Credit Heritage Conservation District Subcommittee

Date: November 29, 2021
Time: 3:00 PM
Location: Online Video Conference

Members

Councillor Stephen Dasko	Ward 1
Antoine Musiol	Citizen Member (Chair)
Donna Gray	Citizen Member
Ian Leonard	Citizen Member
Katyryna Stachiw	

Advance registration is required to attend, participate and/or make a comment in the virtual meeting. Any materials you wish to show the Committee during your presentation must be provided as an attachment to the email. Links to cloud services will not be accepted. Comments submitted will be considered as public information and entered into public record. Please note the Port Credit Heritage Conservation District Subcommittee will not be streamed or video posted afterwards.

To register, please email martha.cameron@mississauga.ca and for Residents without access to the internet via computer, smartphone or tablet, can register by calling Martha Cameron at 905-615-3200 ext.5438 no later than Friday, November 26, 2021 at 12:00 PM. You will be provided with directions on how to participate from Clerks' staff.

Contact

Martha Cameron, Legislative Coordinator, Legislative Services
905-615-3200 ext. 5438
Email martha.cameron@mississauga.ca

NOTE: To support corporate waste reduction efforts the large appendices in this agenda can be viewed online.

Find it Online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

1. **CALL TO ORDER**

2. **INDIGENOUS LAND STATEMENT**

“We acknowledge the lands which constitute the present-day City of Mississauga as being part of the Treaty and Traditional Territory of the Mississaugas of the Credit First Nation, The Haudenosaunee Confederacy the Huron-Wendat and Wyandotte Nations. We recognize these peoples and their ancestors as peoples who inhabited these lands since time immemorial. The City of Mississauga is home to many global Indigenous Peoples.

As a municipality, the City of Mississauga is actively working towards reconciliation by confronting our past and our present, providing space for Indigenous peoples within their territory, to recognize and uphold their Treaty Rights and to support Indigenous Peoples. We formally recognize the Anishinaabe origins of our name and continue to make Mississauga a safe space for all Indigenous peoples.”

3. **APPROVAL OF AGENDA**

4. **DECLARATION OF CONFLICT OF INTEREST**

5. **MINUTES OF PREVIOUS MEETING**

5.1. 2021 08 09 Port Credit Heritage Conservation District Draft Minutes

6. **DEPUTATIONS**

7. **MATTERS TO BE CONSIDERED**

7.1. Request to Alter 169 Lakeshore Road West

8. **OTHER BUSINESS**

9. **DATE OF NEXT MEETING**

January 3, 2022 at 3 p.m.

Virtual Online Conference

10. **ADJOURNMENT**

Port Credit Heritage Conservation District Subcommittee

Date: August 9, 2021
Time: 3:03 PM
Location: Online Video Conference

Members Present	Councillor Stephen Dasko	Ward 1
	Antoine Musiol	Citizen Member (Chair)
	Donna Gray	Citizen Member
	Ian Leonard	Citizen Member
	Katyryna Stachiw	Citizen Member

Members Absent	Louie Manzo	Citizen Member
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Staff Present

John Dunlop, Manager, Heritage Planning and Indigenous Relations
Paula Wubbenhorst, Heritage Planner
Andrew Douglas, Heritage Analyst
Martha Cameron, Legislative Coordinator
Megan Piercey, Legislative Coordinator

1. CALL TO ORDER – 3:03 PM
2. APPROVAL OF AGENDA
Approved (I. Leonard)
3. DECLARATION OF CONFLICT OF INTEREST - Nil
4. MINUTES OF PREVIOUS MEETING
- 4.1 Port Credit Village Heritage Conservation District Minutes- May 3, 2021
Approved (K. Stachiw)
5. DEPUTATIONS - Nil
6. MATTERS TO BE CONSIDERED
- 6.1 Request to Alter 14 Front Street South

Subcommittee Members complimented the architecture.

RECOMMENDATION PCHCD-0009-2021

Moved by I. Leonard

That the request to convert a commercial property to a residential property at 14 Front Street South, as per the memorandum from John Dunlop, Manager of Heritage Planning & Indigenous Relations, dated July 27, 2021, be approved.

Approved

- 6.2 Request to Alter Marina Park, Port Credit

Kristina Martens of ASI and Shahrzad Nezafati of Dillon Consulting Limited presented on Port Credit West Parks.

The Subcommittee members engaged in a discussion regarding the overall design recommendations and commented as follows:

- Antoine Musiol, Chair expressed concern regarding maintaining the history and heritage characteristics in the design features as well as the amount of parking planned in the alteration;
- Donna Grey, Citizen Member, discussed the purpose of the two parking lots and need for temporary parking by the boat launch.

John Dunlop, Manager, Heritage Planning and Indigenous Relations advised the subcommittee that the parks department worked with the consultants on the project, and that staff would continue discussions with Indigenous communities regarding the general design and historical interpretation.

Shahrzad Nezafati of Dillon Consulting Limited advised the subcommittee that 60% of the design package has been submitted for review and confirmed that only the theming remains for presentation. Ms. Nezafati further advised the subcommittee that the fish cleaning station could still incorporate the design comments provided.

Ms. Nezafati advised the subcommittee that the consultants met with the City's Transportation and Works department to discuss parking, the design of Front Street, and a signal light intersection at Lakeshore Road. Ms. Nezafati further advised the subcommittee that Marina Park and Memorial Park are not expected to have major changes.

RECOMMENDATION PCHCD-0010-2021

Moved by D. Gray

That the request to revitalize Marina Park, as per the memorandum and attached Heritage Impact Assessment from John Dunlop, Manager of Heritage Planning & Indigenous Relations, dated July 27, 2021, entitled "Request to Alter Marina Park, Port Credit", be approved, on the condition that the consultation should be carried out with the committee on the final detail/design aspects of the park prior to submission of a heritage application for those aspects of the park revitalization.

Approved -

6.3 Request to Alter 167 Lakeshore Road West

Meagan Sanderson of W.E. Oughtred & Associates advised subcommittee members that she had no further comments on the report.

Katryna Stachiw, Citizen Member inquired about parking in the back of the premises.

RECOMMENDATION PCHCD-0011-2021

Moved by (D. Gray)

That the request to add a new façade to the property at 167 Lakeshore Road West, as per the memorandum from John Dunlop, Manager of Heritage Planning & Indigenous Relations, dated July 27, 2021, entitled "Request to Alter 167 Lakeshore Road West" and revised Heritage Impact Assessment dated July 30, 2021, be approved.

Approved

7. OTHER BUSINESS

8. DATE OF NEXT MEETING - August 30, 2021

9. ADJOURNMENT – 4:12 PM (K. Stachiw)

City of Mississauga Memorandum



Date: November 15, 2021

To: Chair and Members of Port Credit Heritage Conservation District Subcommittee

From: John Dunlop, Manager, Heritage Planning & Indigenous Relations

Meeting date: November 29, 2021

Subject: **Request to Alter 169 Lakeshore Road West**

Recommendation:

That the request to add a new façade to the property at 169 Lakeshore Road West, as per the memorandum from John Dunlop, Manager of Heritage Planning & Indigenous Relations, dated November 15, 2021, be approved.

Background:

The subject property is designated under Part V of the *Ontario Heritage Act* as it forms part of the Old Port Credit Village Heritage Conservation District (HCD). Changes to the property are subject to the Old Port Credit Village HCD Plan, 2018.

Comments:

The owner of 169 Lakeshore Road West proposes exterior alterations to the north and west sides of the property.

The existing structural columns will be enhanced to mimic a roman doric column. An enlarged sign canopy will be constructed to allow for signage and design features. The enlarged sign canopy will continue down the north side of the building. The original windows will be kept while a new door is being proposed.

The HIA concludes that the proposed alterations meet the policies and guidelines set forth in the Old Port Credit Heritage Conservation District Plan.

Although Heritage Planning Staff concur the plans meet the requirements of the Old Port Credit District Plan. Concerns were presented to the consultant that the height discrepancy between the proposed facades on 167 and 169 Lakeshore Road west do not complement each other.

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Conclusion:

The owner of the subject property has applied to add a new façade to the property. The proposed changes are sympathetic to the character of the property and provide accessibility. They should therefore be approved.

Attachments

Appendix:Heritage Impact Assessment

Prepared by: Andrew Douglas, Heritage Analyst, Heritage Planning & Indigenous Relations
Culture Division

169 Lakeshore Road West

Scoped Heritage Impact Assessment - October 5, 2021



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Executive Summary

This *Scoped Heritage Impact Assessment* has been undertaken to determine the impacts of the proposed exterior alterations to 169 Lakeshore Road West. The intention of this review is to determine if the alterations will have any negative impacts on the heritage resource and neighbouring properties identified by the City of Mississauga as:

1. 169 Lakeshore is designated as “Other” within the Old Port Credit Village Heritage Conservation District Plan (HCD).

2. The adjacent property to the east, identified as Clarke Hall, is designated under Part IV of the Ontario Heritage Act.

The assessment included the following undertakings by the W.E. Oughtred & Associates Inc.:

- field review of the subject property
- review of existing historical information
- review of existing heritage evaluations
- review of relevant heritage policies
- consultation with Alexandra Conibear, designer for Loretta Phinney
- evaluation of the property against the Old Port Credit Heritage Conservation District Plan.

Based on this assessment, it has been determined that the proposed alterations will have no negative impact on the subject property or the adjacent property, Clarke Hall. Further, no negative influences; be that setting, character and use of the lands, will be realized as a result of the proposed development. Further, the proposed alterations are in keeping with the policies and guidelines outlined in Sections 7.0 and 8.0 of the Old Port Credit HCD Plan.

Introduction

This Heritage Impact Assessment is scoped according to direction provided by heritage planning staff at the City of Mississauga. The field review was conducted on October 6th, 2021.

Location & Site Description

The subject property is a one-storey yellow brick building built prior to 1966. The property has two units, each with a separate address; 167 and 169 Lakeshore Road West. 169 Lakeshore Road West is occupied by a real-estate office. The adjacent property to the east, 167 Lakeshore Road West, is a bakery, Carlo's Bake Shop. Shared parking is provided at the rear of the property for both premises.

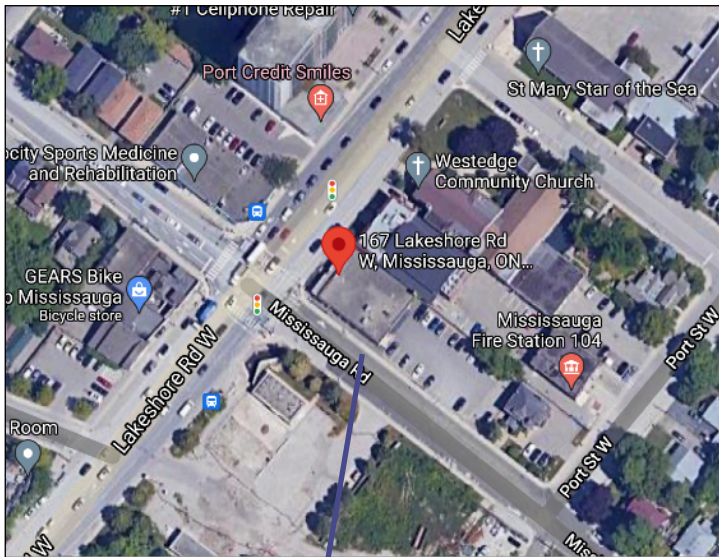


Photo1: Site Location

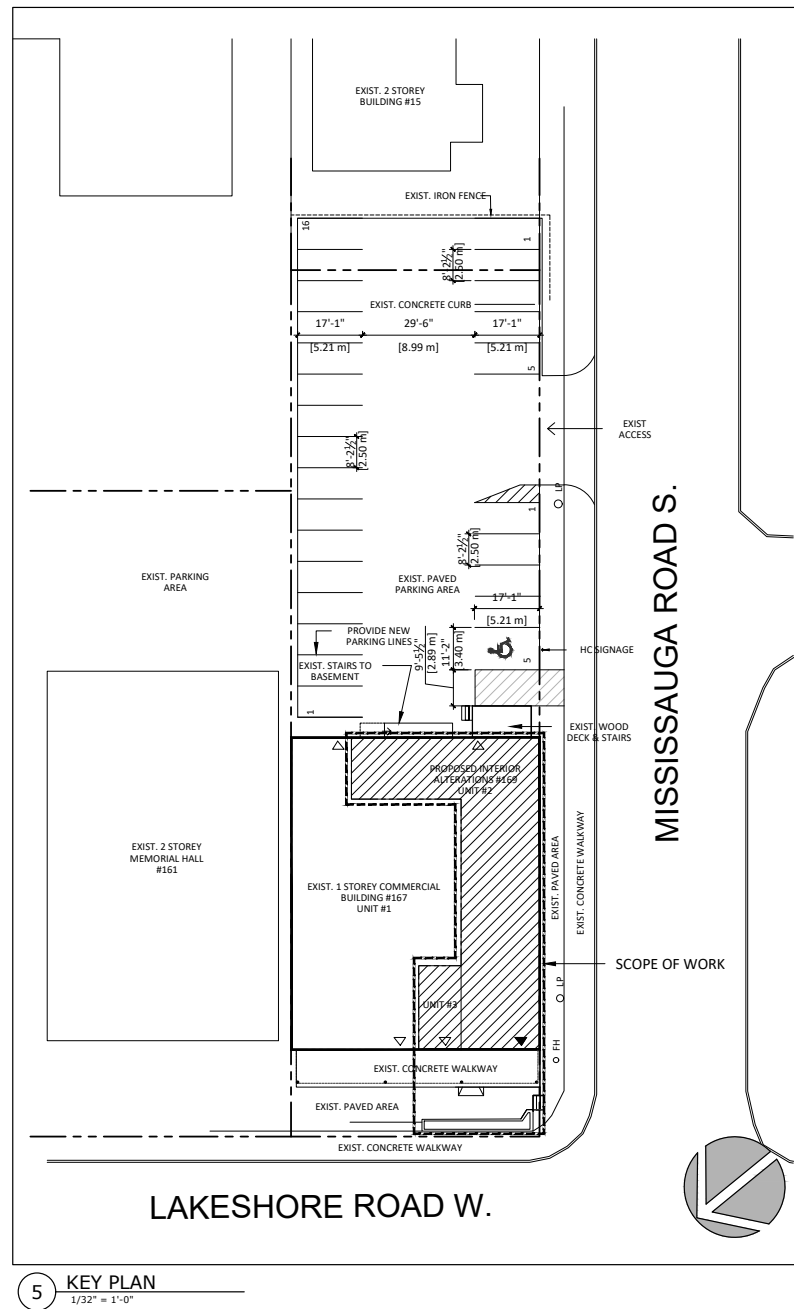
Subject building



Photo 2: Corner Street View

There are no changes to the existing site plan. Changes are proposed to the front elevation only. Interior alterations were done under a previous building permit, code

Figure 1: Existing Site Plan



Heritage Designation

The subject property has an Old Port Credit HCD plan classification as “other”, as per the attached excerpt from the plan.

OLD PORT CREDIT VILLAGE HERITAGE CONSERVATION DISTRICT PROPERTY INVENTORY 2018

1.0 ADDRESS	167-169 Lakeshore Road West
2.0 LOT	
2.1 Land use designation	Convenience Commercial
2.2 Period of construction	1960s
2.3 Zoning	C4-35
2.4 Lot size (m ²)	1005
2.5 Building front yard setback (m)	3.81
2.6 Building side yard setback (m)	0 / 0
3.0 LANDSCAPE / SETTING / CONTEXT	
3.1 Trees and shrubs	n/a
3.2 Soft landscaping	n/a
3.3 Driveways and parking	Asphalt parking lot
3.4 Landscape/property features	n/a
3.5 Fencing	n/a
4.0 ARCHITECTURE	
4.1 Building type	Single-story strip mall
4.2 Building size (m ²)	565
4.3 Wall assembly / cladding	Brick
4.4 Roof shape / pitch / material	Flat
4.5 Storeys	1
4.6 Alterations	
4.7 Architectural style	Commercial shop front
5.0 HERITAGE	
5.1 Current status/designation	Designated under Part V
5.2 HCD plan classification	Other
5.3 Heritage notes	n/a
5.4 Heritage attributes	n/a

Figure 2: Old Port Credit HCD Designation¹

Historically the subject building as a restaurant. The current use is a real estate office. A historical image of the site (1980) is included below.

¹ Old Port Credit District Heritage Plan



Photo 3: 1980 Site Photo, Image reference G597, City of Mississauga

At the time this photo was taken in 1980 the building housed a branch of the Royal Bank. Several other business have been located in the building, which, as of 2010, was a commercial building housing Adobe Home Collection and Hatty's Bar & Grill.²

The subject property is adjacent to Clarke Hall, a designated property. Clarke Memorial Hall is one of the key buildings within the Old Port Credit Village Heritage Conservation District, forming part of a significant institutional block on Lakeshore Road. Its size, location along the west bank of the Credit River and architecture make this building a landmark structure.³

² City of Mississauga, Historic Images Gallery

³ historicplaces.ca



Photo 4: 1975 Photo, Image Reference: MI0069, City of Mississauga (1975)

Erected by Mary Louise Clarke in 1922 as a memorial to her husband, Alfred Russel Clarke. He had died of pneumonia contracted as a result of the sinking in 1915 of the Lusitania, on which he was a passenger. The Hall was donated to the United Church and used as a Sunday School. During the Depression it was handed over to the Village of Port Credit and used as a Community Hall. It was the site of the Port Credit Municipal Offices from 1941 to 1974. Since then it has been used for meetings, auctions and various events, and as a theatre. The Hall is built in Spanish Colonial Revival style.⁴

Proposal

The proposal is the refurbishment of the exterior of the real estate office. In 2017 Loretta Phinney, Real Estate Agent, relocated to this property. Alterations to the interior and exterior were undertaken under City of Mississauga Building Permit 17-8132, prior to the establishment of the Port Credit Heritage District.

⁴ ⁴ City of Mississauga, Historical Image Gallery

The existing front elevation remains relatively unchanged over the years aside from the signage. The existing structural columns will be enhanced to mimic a roman doric column. An enlarged sign canopy will be constructed to allow for signage and design features.

Photo 5: Existing street view



Figure 3:
Proposed
elevation



TYPE EXTERIOR ELEVATIONS

DATE 27/09/2021

DESIGNED FOR LORETTA PHINNEY TEAM

SCALE NTS



Alex Conibear
Interior Design | Fine Art
416-414-0191
alex@conibeardesign.com
www.conibeardesign.com



DESIGN PLANS ARE PROVIDED FOR THE JOINT USE BY THE CLIENT OR HIS AGENT IN COMPLETION OF THE PROJECT AS LISTED WITHIN THIS CONTRACT. DESIGN PLANS REMAIN THE PROPERTY OF THE FIRM AND CAN NOT BE USED OR REPRODUCED WITHOUT PERMISSION. ALL DIMENSIONS AND SCALE COORDINATIONS GIVEN SUBJECT TO VERIFICATION ON JOB SITE AND ADJUSTMENT TO FIT JOB CONDITIONS. ANY ALTERATIONS OR DEVIATIONS FROM THE SPECIFICATIONS ON THIS DRAWING ARE SUBJECT EXTRA COST RESPONSIBLE BY CLIENT.

Figure 4: Proposed Side Elevation



TYPE EXTERIOR ELEVATIONS

DATE 27/09/2021

DESIGNED FOR LORETTA PHINNEY TEAM

SCALE NTS



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Excerpts from Heritage Conservation District Plan

The subject property is classed as 'other' within the Old Port Credit Heritage Conservation District Plan. Sections 7.0, Alterations and additions to other properties; and Section 8.0, Properties fronting Lakeshore Road East and West, of the HCD Plan provide the guidelines for alterations and additions to 'other' properties.

7.0 Alterations and additions to other properties

There are some other residential properties in the District, although new residential construction may be permitted in the future.

New construction and work undertaken to non-heritage residential buildings should respect the overall character of the Old Port Credit Village Heritage Conservation District and be sensitive to any neighbouring heritage buildings. The following should be consulted for general guidance:

7.1 Policies for alterations and additions to other properties

7.1.1 Property owners must abide by the Property Standards By-Law 654-98.

7.1.2 When permitted additions to buildings on other properties are proposed, the design will respect the District's general historical character but integrate with the existing building. In designing additions, property owners will also have regard for any impact the addition may have on adjacent properties in terms of scale, massing, height and setback.

7.1.3 Animated signs are prohibited.

7.2 Guidelines for alterations and additions to other properties

7.2.1 Location

a) Exterior additions are encouraged to be located at the rear or on an inconspicuous side of the building, limited in size and scale to complement the existing building and neighbouring properties.

7.2.2 Height

a) New additions should be no higher than the existing building height. Where additions are proposed that exceed the existing building height, a heritage impact assessment must be completed that demonstrates that the additional height does not negatively impact the District.

7.2.3 Width

a) New additions should be designed in a building mass that extends rearward in depth on the lot rather than along the horizontal width.

7.2.4 Setback

a) In streetscapes of similar building setbacks new construction should match existing.

7.2.5 Roofs

a) Roof types encouraged in new construction are front gabled and side gabled. Flat roofs are permitted for additions to existing multi-unit residential buildings, and new multi-unit residential buildings on other properties.

b) Asphalt, wood shingles or metal are appropriate for new construction.

c) Any required roof vents, skylights, satellite dishes, solar panels, metal chimneys and flues, other venting devices and roof features should be located to the rear of new additions.

d) Roof lines should reflect the horizontal cornice of adjacent heritage buildings where adjacent properties are of a comparable scale.

7.2.6 Exterior cladding

d) Most of the properties in the District are clad in traditional materials such as brick, stucco or wood siding. These materials are encouraged for new additions.

8.1 Policies for properties fronting Lakeshore Road East and West

8.1.1 Property owners must abide by the Property Standards By-Law 654-98.

8.2 Guidelines for properties fronting Lakeshore Road East and West

8.2.1 Location

a) Exterior additions are encouraged to be located at the rear or on an inconspicuous side of the building, limited in size and scale to complement the existing building and neighbouring properties.

8.2.2 Height

a) The height of additions should respect the height of existing adjacent buildings.

8.2.3 Relation to street

b) Additions and alterations to commercial buildings on other properties shall continue to maintain the street wall with appropriate use of materials on the ground and upper floors.

c) Additions to commercial buildings on corner sites shall address the side lot line with appropriate materials, glazing, entrances and other architectural features to ensure continuity with street front façade design and avoiding the construction of large blank walls.

8.2.4 Windows and entrances

a) Storefronts should be primarily glazed, and upper storey windows should be rectangular and vertical in proportion.

b) Entrances from the sidewalk may be flush or recessed.

c) Blank windowless walls are discouraged.

8.2.5 Exterior cladding

a) The principal building cladding for commercial buildings in the District has been historically either brick or rough cast stucco. Residential buildings in commercial use are frame with stucco or

horizontal weatherboard cladding. These materials will be encouraged for major additions, with an emphasis on distinguishing old and new portions of the building.

b) The installation of Exterior Insulation and Finish Systems (EIFS) on buildings on other properties is discouraged within the District.⁵

Compliance with Heritage District Conservation Plan

Table 1: Compliance with HCD

HCD Plan Section	Compliance (yes/no)	Comments
7.1.1 Property Standards	Yes	By-law will be adhered to.
7.1.2 Permitted additions	Yes	The alteration is sympathetic to the adjacent properties.
7.1.3 Animated Signs	Yes	No animated sign is proposed.
7.2.1 Location	Yes	It is not an addition
7.2.2 Height	Yes and No	The proposed addition to the front is for signage. There is no height increase to the overall building.
7.2.3 Width	N/A	
7.2.4 Setback	Yes	No changes to the existing front yard setback
7.2.5 Roofs	Yes	No alterations proposed to the existing roof (unless structural changes are required).
7.2.6 Exterior Cladding	Yes	No changes proposed to the existing cladding.
8.1.1 Property Standards	Yes	By-law will be adhered to.
8.2.1 Location	Yes	Technically, this is not an addition, as no GFA is being added to the building.
8.2.2 Height	Yes	The proposed alteration to the front will minimally affect the overall height of the front elevation.
8.2.3 Relation to Street	Yes	The proposed materials are compatible and complimentary to the adjacent buildings.
8.2.4 Windows and entrances	Yes	The existing windows are to be maintained. New doors are proposed.
8.2.5 Exterior Cladding	No	A small portion of the exterior brick will be covered with panels to mimic the new door.

⁵ Old Port Credit Heritage Conservation District Plan

Streetscapes



Figure 5: Existing Streetscape



TYPE	EXTERIOR ELEVATIONS-STREETSCAPE	DATE	13/10/21
DESIGNED FOR	LORETTA PHINNEY TEAM	SCALE	NTS



Alex Conibear
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416-414-0191
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Figure 6: Proposed Streetscape

Mitigation Measures

The subject building is located on the fringe of the boundary of the Old Port Credit Village HCD. Situated immediately east of the subject property is Clarke Memorial Hall, a designated heritage property and the Port Credit Methodist Church.

Subject property

Old Port Credit Village Heritage Conservation District Plan 2018
Heritage Conservation District Plan

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Figure 16: Old Port Credit Village HCD property types

George Robb Architect | MHBC | WSLA | HHI

July 2019

Figure 7: HCD Mapping⁶

⁶ Old Port Credit Heritage District Conservation Plan



Photo 6: Lakeshore Road West & Mississauga Road Intersection⁷

Approaching the intersection of Mississauga Road and Lakeshore Road West, from the west (travelling east), there is no clear indication that you are entering the Old Port Credit Heritage Conservation District. The north side of Lakeshore Road reflects more modern structures, including a twenty storey residential tower. The south side is low rise mixed use buildings. The subject property is contained within the corner building at the south east corner of Lakeshore Road West and Mississauga Road, Clarke Memorial Hall is not easily identifiable as a designated property, nor is the Port Credit Methodist Church. Neither building has a plaque or something out front screaming “I am a heritage building” The proposed exterior alterations to the subject building will not obstruct the adjacent properties

heritage value or the nature of the HCD.

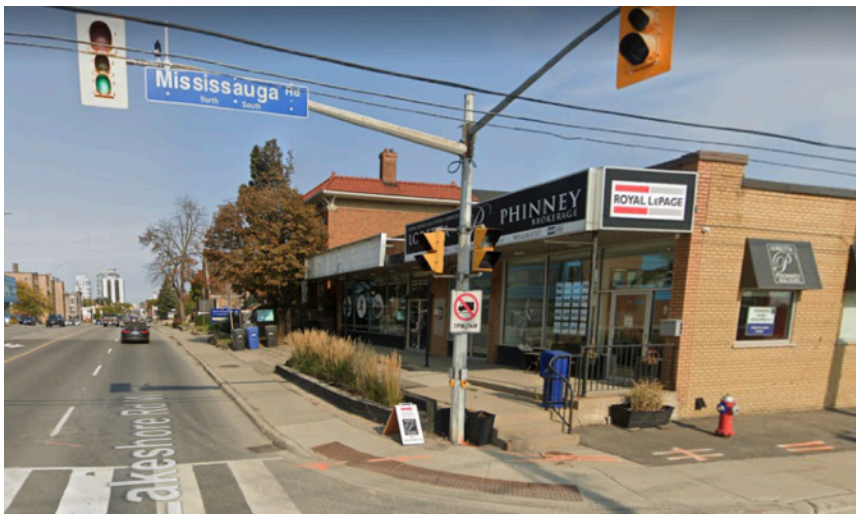


Photo 7: View of Subject property⁸

⁸ Google Streetview

Photo 8 (above) demonstrates that the current view of Clarke Memorial hall is the west side of the building which does not provide any indication that it is a heritage designated building.

Mitigation measures, outlined in the table below, have been considered to ensure the heritage resources of the adjacent property and the conservation district are not negatively impacted.

Table 2: Mitigation Measures

Potential Impact	Identified Impact	Mitigation
Destruction of any, or part of any, significant heritage attributes or features	None	Not required
Alteration that is not sympathetic, or is incompatible with the historic fabric and appearance	None	Not required
Isolation of a heritage attribute from its surrounding environment, context or a significant relationship	None	Not required
A change in land use where the change in use negates the property's cultural heritage value	N/A	Not required
Removal of natural heritage features, including trees	None	Not required
Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature, or plantings, such as a garden	None	Not required
Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features	None	Not required
Land disturbances such as change in grade that alter soils, and drainage patterns that adversely affect cultural heritage resources.	None	Not required

Mitigation methods recommended by The Guide to Cultural Heritage Resources in the Land Use Planning Process are identified in Table 2. As there are no site alterations proposed, only facade and side elevation, these mitigation methods are not applicable or not required. Further, we have reviewed the report and have compiled Table 3, outlined below, of further considerations that have been undertaken to avoid or limit the impacts of the proposed development.

Table 3: Additional Mitigation Measures

Strategy	Mitigation Measure
Vegetation screening, fencing	The existing mature tree between the subject property and the property to the east will be maintained.
Buffers	See above
Site Lines	The proposed alteration to the building is for signage only. This will not obstruct the view of the adjacent heritage property.
Facade treatment	Subdued / muted colours have been chosen so as not to provide a 'stark' contrast to adjacent buildings

Conclusions, Recommendations

The subject property contains a one-storey commercial building built in the 1960's. The proposed alterations will have no impact on the cultural heritage resource or the adjacent property to the east. Mitigation measures, as noted above, have been considered and no further action is required.

ABOUT THE AUTHOR

William Oughtred of W.E. Oughtred & Associates Inc., is a development and land use consultant who has been practicing in the Mississauga and GTA area for over 30 years. Mr. Oughtred has a Bachelor of Arts from McMaster University. Mr. Oughtred is well versed in both Planning and building procedures and the City of Mississauga Zoning By-law and Official Plan.

Mr. Oughtred specializes in infill development projects. His extensive experience has afforded him the opportunity to see the City evolve and be at the forefront of growing trends and patterns in land development in Mississauga. He consults regularly on both heritage and urban design for infill projects.

Heritage Impact Statements and Assessments have been completed for many properties in Mississauga, including, but not limited to the properties listed below.

-
- ❖ 167 Lakeshore Road West
 - ❖ 965 Whittier Crescent
 - ❖ 1503 Petrie Way
 - ❖ 1470 Pinetree Way
 - ❖ 2375 Mississauga Road
 - ❖ 943 Whittier Crescent
 - ❖ 2417 Mississauga Road
 - ❖ 1641 Blythe Road

REFERENCES

www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf

<https://www.mississauga.ca/portal/services/property>

Historic_properties.ca

Wikipedia.com

google.com

Ministry of Tourism & Sport, Guide to Cultural Heritage Resources in the Land Use Planning Process

Old Port Credit Heritage District Conservation Plan