



1130 – 1140 Clarkson Road North, Mississauga, Ontario

Clarkson House & General Store

Issued: March 2021

HERITAGE IMPACT ASSESSMENT

Prepared for: CIMA+

Prepared by: David Eckler, BES, B.Arch., OAA, MRAIC



AREA, Architects Rasch Eckler Associates Ltd.
15 Lola Road, Toronto ON M5P 1E5



Architects Rasch Eckler Associates Ltd.
Project No. 20-682

TABLE OF CONTENTS

TABLE OF CONTENTS	2
TABLE OF FIGURES	4
LIST OF TABLES	5
1.0 INTRODUCTION TO THE REPORT	6
1.1 EXECUTIVE SUMMARY	6
1.2 METHODOLOGY OF HIA REPORT	7
2.0 POLICY FRAMEWORK.....	9
2.1 PROVINCIAL POLICY STATEMENT (PPS, 2020)	9
2.2 A PLACE TO GROW - GROWTH PLAN FOR THE GREATER GOLDEN HORSESHOE (OFFICE CONSOLIDATION 2020)	10
2.3 REGION OF PEEL OFFICIAL PLAN (ROP OP, OFFICE CONSOLIDATION 2018)	11
2.4 THE ONTARIO HERITAGE ACT AND ONTARIO REGULATION 9/06	12
2.4.1 MINISTRY OF HERITAGE, SPORT, TOURISM AND CULTURE INDUSTRIES.....	13
2.5 CITY OF MISSISSAUGA PLANS AND POLICIES	14
2.5.1 CITY OF MISSISSAUGA OFFICIAL PLAN POLICIES.....	14
2.5.2 ZONING BY-LAW	15
3.0 INTRODUCTION TO SUBJECT PROPERTY	17
3.1 LOCATION OF THE SUBJECT PROPERTY	17
3.2 DESCRIPTION OF SUBJECT PROPERTY	20
3.2.1 SITE	20
3.2.2 BUILDINGS	22
3.2.3 CONTEXT	24
3.3 EXISTING CONDITION	25
3.4 HERITAGE STATUS OF CHR AND ADJACENT HERITAGE PROPERTIES	27
4.0 STATEMENT OF CULTURAL HERITAGE VALUE OR INTEREST	28
4.1 HISTORICAL / ASSOCIATIVE SUMMARY	30
4.1.1 TORONTO TOWNSHIP	30
4.1.2 TOWNSHIP OF CLARKSON	31
4.1.3 CLARKSON ROAD NORTH	31
4.1.4 HISTORY OF OWNERSHIP	32
4.1.5 HISTORY OF LANDUSE	35
4.2 DESIGN / PHYSICAL SUMMARY	39
4.2.1 BUILDERS	39
4.2.2 CLARKSON STORE	39
4.2.3 WILLIAM CLARKSON HOUSE.....	40

4.3 SUMMARY	42
5.0 DESCRIPTION AND EXAMINATION OF PROPOSED DEVELOPMENT/SITE ALTERATIONS 43	
5.1 PROPOSAL OVERVIEW	43
5.2 PURPOSE	43
5.3 ISSUES AND RISKS POSED BY THE EXISTING OVERHEAD SYSTEM.....	44
5.3.1 OPERATIONAL ISSUES	44
5.3.2 ACCESSIBILITY ISSUES	45
5.4 DESCRIPTION OF THE PROPOSED DEVELOPMENT	46
5.4.1 SCOPE OF WORK (FIGURE 54):	47
5.5 SITE ALTERATION.....	49
5.6 IMPACT ASSESSMENT	51
5.6.1 ASSESSMENT OF ADVERSE IMPACTS	51
5.6.2 RESULTS OF IMPACT ASSESSMENT	54
5.7 CONSIDERED ALTERNATIVES AND MITIGATION STRATEGIES	54
6.0 CONSERVATION AND IMPLEMENTATION STRATEGIES.....	57
6.1 PRESERVATION APPROACH.....	57
6.1.1 PLANNING FOR STABILIZATION AND MONITORING	57
6.1.2 RECOMMENDATIONS AND APPROVALS	57
7.0 SUMMARY STATEMENT & RECOMMENDATIONS	58
8.0 APPENDICES	59
APPENDIX A: BIBLIOGRAPHY & REFERENCES.....	60
APPENDIX B: SITE PHOTOS (FEBRUARY 2021)	64
APPENDIX C: CITY OF MISSISSAUGA'S CULTURAL HERITAGE IMPACT ASSESSMENT, TERMS OF REFERENCE ("HIA-TOR")	67
APPENDIX D: METROLINX GO ELECTRICAL LWL. – CONFLICT #LW26, CLARKSON RD. N - LAKESHORE RD. W TO TRUSCOTT DRIVE, ALECTRA UTILITIES CORPORATION, ISSUED JULY 2020	73
APPENDIX E: CLARKSON FAMILY FILE, WILLIAM PERKINS BULL FONDS, REGION OF PEEL ARCHIVES	80
APPENDIX F: ARCHIVAL PHOTOS AND HISTORICAL PLAN.....	108
APPENDIX G: THE CORPORATION OF THE CITY OF MISSISSAUGA BY-LAW 0057-2017	111
APPENDIX H: RESUME	115

TABLE OF FIGURES

Figure 1: Aerial photograph showing the location of the subject property in red in the City of Mississauga (Google 2020)	6
Figure 2: Zoning Map for 1130-1140 Clarkson Road N, Annotated by AREA (Mississauga Zoning and Property Information)	16
Figure 3: D Zone permitted uses and Zone Regulations	16
Figure 4: Property Index Map showing the approximate extents, lot number, block number of the subject property. (Source: Ontario Land Registry 2020)	18
Figure 5: Aerial map showing the location of the subject property in red in the City of Mississauga (Google Maps 2021).	18
Figure 6: Plan showing the subject property and surrounding lots and city roads.	19
Figure 7: Map 16-5: Clarkson-Lorne Park Neighbourhood Character Area, annotated by AREA	20
Figure 8: GO Transit tracks adjacent to the subject property	21
Figure 9: Rear of the lot (West elevation), proposed location for the underground ductbank.	21
Figure 10: East Elevation (Front)	21
Figure 11: West Elevation (Rear)	21
Figure 12: South Elevation (facing the tracks)	21
Figure 13: North Elevation showing the side elevation of William Clarkson House and Store	21
Figure 14: Aerial view of the subject property	22
Figure 15: The main elevation of the William Clarkson House.	23
Figure 16: William Clarkson Store built in 1835	23
Figure 17: William Clarkson House, 2021	23
Figure 18: General Store and Post Office building, February 2021	24
Figure 19: North Elevation of the William Clarkson House and store	25
Figure 20: View of the William Clarkson House and store from Balsam Avenue looking southeast	25
Figure 21: View of the subject lot from Clarkson Road North looking southwest.	25
Figure 22: View of the subject lot from Clarkson Road North looking northwest	25
Figure 23: View of adjacent residential property west of the subject lot from Balsam Avenue looking southwest.	26
Figure 24: View of adjacent listed heritage property from Clarkson Road North looking northwest.	26
Figure 25: Intersection at Clarkson Road North and Balsam Avenue looking north.	26
Figure 26: View of GO Train tracks and property beyond from the subject lot looking south.	26
Figure 27: Looking north on Clarkson Road North	26
Figure 28: Clarkson Go Station south west of the subject lot.	26
Figure 29: Adjacent Heritage properties, Mississauga's Municipal Register of Cultural Heritage Resources (2020)	27
Figure 30: Concession Map showing the resident families in the village of Clarkson, 1833	31
Figure 31: Land Registry Records, Township of Toronto	32
Figure 32: Warren Clarkson's property in the village of Clarkson in late 1850s	33
Figure 33: The William Clarkson property on Lots 28,29 & 30, Concession 2 SDS and the village of Clarkson in the late 1870s. The building annotated by AREA shows the current subject site and is marked as the property of W.W.C 'William Warren Clarkson.	33
Figure 34: 1936 Clarkson Fire insurance Map (Underwriters' Insurance Bureau 1936)	34
Figure 35: Personal Census, Enumeration District 1, Township of Toronto, County of Peel	35
Figure 36: Henry Shook Clarkson' house. (Source: Mississauga Library – Historic Image Gallery)	36
Figure 37: The front elevation of the Carload Groceteria ('Clarkson Store') on Clarkson Road North (Mississauga Library – Historic Image Gallery, n.d)	37
Figure 38: View of the former railway station (Mississauga Library – Historic Image Gallery, 1900)	37
Figure 39: View of the Clarkson Store and Post office prior to the 1906-1910 renovations (Mississauga Library – Historic Image Gallery, n.d)	37

Figure 40: Interior View of the Clarkson Store and Post Office (Mississauga Library – Historic Image Gallery, 1910).....	37
Figure 41: View of the former railway station on the north side of the tracks (Mississauga Library – Historic Image Gallery, n.d).....	37
Figure 42: View of Alex Durie’s store (right) and the Edith (Warren Clarkson’s granddaughter) Clarkson Store (left). (Mississauga Library – Historic Image Gallery, 1900)	37
Figure 43: Orthoimagery Aerial, 1967, Region of Peel Archives	38
Figure 44: East (front) elevation of the Clarkson Store, left - July 2015 (Source: CHER: 1130-1140 Clarkson Road North, City of Mississauga, Ontario, October 2015, 26), right – February 2021.....	39
Figure 45: Selection of drawings from the 1864 The Canada Farmer column showing the suggested plan and principal elevation of the rural Ontario gothic cottage being advertised.....	40
Figure 46: The main elevation of the William Clarkson House, early 20 th century (Source: Mississauga, Historical Images Gallery, BA0154)	41
Figure 47: The main elevation of the William Clarkson House, 1980 (Source: Mississauga, Historical Images Gallery).....	41
Figure 48: The main elevation of the William Clarkson House, 2004 (Source: Heritage Mississauga 2005)	41
Figure 49: The main elevation of the William Clarkson House, 2020 (Source: Google Maps 2021)	41
Figure 50: View of the subject property and existing overhead electric distribution system from Balsam Avenue looking southeast (February 2021).....	44
Figure 51: View of the subject property and existing overhead electric system from Clarkson Rd. N looking northwest (February 2021)	45
Figure 52: View of the subject property and existing overhead electric system from Clarkson Rd. N looking southwest. The hydro pole fronting the Clarkson general store is proposed to be removed, annotated by AREA in red. (February 2021).....	45
Figure 53: Schematic diagram showing the existing and proposed electrical lines within and adjacent to the subject site. (Source: Metrolinx GO Elec. LWL – CONFLICT #LW26 Clarkson Road North-Lakeshore Road West to Truscott Drive, Primary Schematic, Sheet No. D07-396295-001)	46
Figure 54: Electric Plan and the Trenching Plan of the proposed development.....	48
Figure 55: Sketch of the 2 new anchors to be installed on the existing pole in front of the property to provide structural stability, overlayed on the current conditions at 1140 Clarkson Rd N.	49
Figure 56: Profile view of the proposed ductbank within the subject site. Distance buffer of the William Clarkson house from the ductback annotated by AREA.....	50
Figure 57: Edith and Cora Clarkson.....	108
Figure 58: Edith Clarkson House 1160 Clarkson Road North, Plan G-13, Lot 1	108
Figure 59: An opening announcement of the Carload Groceteria in the local newspaper	109
Figure 60: Plan G-13, Plan of Subdivision of Parts Lot 28, 29 and 30, Concession 2, SDS, Township of Toronto, Co. Peel, Speights and Van Nostrand, O.L Surveyors Toronto, May 15, 1913.	110

LIST OF TABLES

TABLE 1: INVESTMENT DRIVERS	43
TABLE 2: ASSESSMENT OF DIRECT AND INDIRECT ADVERSE IMPACTS	51
TABLE 3: ANALYSIS OF OPTIONS FOR CONSIDERED ALTERNATIVES.....	55

1.0 INTRODUCTION TO THE REPORT

1.1 EXECUTIVE SUMMARY

AREA, Architects Rasch Eckler Associates Ltd. (AREA) was retained by CIMA+ to prepare this Heritage Impact Assessment report (HIA) of the property with current municipal address 1130-1140 Clarkson Road North, City of Mississauga, Ontario and legal description Part of Lot 67, Registered Plan G-13, designated as Parts 1 and 2, Plan 43R-37471, formerly part of Lot 28, Concession 2. SDS, Toronto Township.

This HIA is required as the subject property is included in the City's Heritage Register as a designated heritage property and will form a part of an already submitted PUCG development application (no. AUC-017-19 R4) to the City of Mississauga PUCG Department.

The property located at 1130-1140 Clarkson Road North ("subject site" or "Clarkson House & General Store") is designated under the terms of the Ontario Heritage Act ('OHA'), Heritage By-Law 0057-2017, as being of cultural heritage value or interest. This HIA has been prepared to review the impact of the proposed development on the heritage resource.



Figure 1: Aerial photograph showing the location of the subject property in red in the City of Mississauga (Google 2020)

The proposed development involves converting existing Alectra distribution overhead lines on the subject site to underground cabling to accommodate Metrolinx's expansion of GO Transit and meet its clearance requirements to provide faster and frequent service on the rail corridor with electrified rail lines.

Based on the results of archival research, site investigation and existing conditions, this HIA will describe the cultural heritage resource, a detailed land-use history of the site and photographic documentation, a description of the proposed development and an assessment of impacts of the proposed undertaking. No alterations are proposed for the heritage building. This HIA concludes that the proposed adjacent development will have no impact on the cultural heritage value of the heritage property.

To ensure the long-term sustainability and use of the Clarkson House & General Store as a valued built heritage resource, *AREA* recommends to:

- preserve and protect the Clarkson House & General Store at its original location within the property in the proposed development. This operation will require the following short-term actions:

Short-term Conservation Actions:

- Ensure that the vacant heritage resource is protected in accordance with the City of Mississauga *Property Standards By-Law 654-98 Section 14.2 BOARDED BUILDINGS ON HERITAGE PROPERTIES (368-06)* and to the satisfaction of the City of Mississauga.

1.2 METHODOLOGY OF HIA REPORT

This HIA was prepared based on the City of Mississauga's Terms of Reference for HIAs and best practices in Ontario municipalities. This HIA report's scope involves the evaluation of the existing heritage resources and the impact on them from the proposed development on and around the subject property.

Archival research, site and building investigations were also incorporated as part of *AREA*'s comprehensive heritage consulting services. Representatives of *AREA* undertook site visit in February, 2021 to view and photograph the Clarkson House & General Store and their surroundings.

Historical research for this report was based on property background information included in existing Geotechnical Studies. Additional background research for this report was based on information gathered from available Land Registry records, historical maps, aerial photographs, personal and agricultural census records, and other published materials. Primary and secondary research was conducted on-line due to the closure of the Peel Art Gallery, Museum and Archives (PAMA). Samantha Thompson, archivist at the Peel Art Gallery, Museum & Archives provided files on the Clarkson family (Appendix E) that were assembled by a Peel historical research team in the 1930s on settler families.

This HIA is being submitted in compliance with the requirements of the Ontario Heritage Act (OHA), and by Council through the Municipal Register. It also references technical drawings and documents associated with the subject property, other provincial and municipal heritage standards and guidelines, as well as archive documents from various sources. These references include but are not limited to:

- City of Mississauga's Cultural Heritage Impact Assessment, Terms of Reference ("HIA-ToR"), 2017 (Appendix C)
- City of Mississauga Official Plan ('OP'), September 2020 Office consolidation
- Heritage Register for Mississauga, 2018
- Geotechnical Investigation, Area 5 – Lakeshore West Corridor, Metrolinx Electrical Utility Crossing, Alectra Utilities Corporation, October 2020
- Metrolinx Go Electrical Lwl. – Conflict #LW26, Clarkson Rd. N - Lakeshore Rd. W to Truscott Drive, Primary Schematic, Alectra Utilities Corporation, November 2018 (Appendix D)
- Mississauga Library – Historical Image Gallery, Online Archives
- Ontario Heritage Act ('OHA');
- Ontario Heritage Tool Kit (OHTK), Ontario Ministry of Culture (now Tourism, Culture, and Sport), 2006;
- Provincial Policy Statement ('PPS') of the Planning Act, with revisions up to 2020

This report's assessment process will reference the above-listed reports, drawings, and heritage conservation standards for managing the built heritage resource. Among the report figures, all current photographs were taken by the author of this HIA from his site visit unless indicated otherwise. This HIA will form part of the subsequent SPA application, subject to the Mississauga Heritage Advisory Committee review, and ultimately, Council.

David Eckler, B.E.S., B.Arch., OAA, MRAIC of AREA, whose curriculum vitae and firm profile are attached (Appendix H) is the primary author responsible for the overall preparation and recommendations of this HIA.

Following the guidelines provided by the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the City of *Mississauga Official Plan and Heritage Impact Assessment Terms of Reference*, and Parks Canada's *Standards and Guidelines for the Conservation of Historic Places in Canada (2010)*, this HIA identifies the heritage policies applicable to new development, summarizes the property's geography and history, and provides an inventory and evaluation of the property's built features. Based on this understanding of the property, the potential impacts resulting from the proposed development are assessed, and future conservation actions are recommended.

2.0 POLICY FRAMEWORK

The subject property at 1130-1140 Clarkson Road N, Mississauga is subject to several provincial and municipal heritage planning policies. The subject property is designated 'Mixed-Use' on Schedule 10 - Land Use designations in the Mississauga Official Plan, which permits developments that provide a variety of retail, service and other mixed uses to support the surrounding residents and businesses. Furthermore, the subject property is located north of the Clarkson Village Community Node, as defined by the City of Mississauga Official Plan, and adjacent to the Canadian National Railway Line (C.N.R). The existing land-use 'Mixed Residential Commercial' designation does not allow for single-detached, semi-detached and duplex dwellings¹. The Clarkson House and General store building located on the subject property have been designated as a 'Heritage Resource' and will be not be altered.

Several provincial and municipal heritage planning policies should be considered during the decision-making process in the cultural heritage environment. The following policies are relevant to the proposed development:

2.1 PROVINCIAL POLICY STATEMENT (PPS, 2020)

The PPS 2020 identifies conservation of resources of significant architectural, cultural, historical, archaeological, or scientific interest as a provincial interest and it further recognizes that protecting cultural heritage and archaeological resources has economic, environmental, and social benefits, and contributes to the long-term prosperity, environmental health, and social well-being of Ontarians. The following sections of the PPS 2020 recognize the importance of identifying and evaluating built heritage and cultural heritage landscapes:

2.6 Cultural Heritage and Archaeology

2.6.1 *Significant built heritage resources and significant cultural heritage landscapes shall be conserved.*

2.6.3 Planning authorities shall not permit *development and site alteration on adjacent lands to protected heritage property* except where the proposed *development and site alteration* has been evaluated and it has been demonstrated that the *heritage attributes* of the protected *heritage property* will be *conserved*.

PPS 2020 defines *significant resources* in regard to cultural heritage and archaeology resources that have been 'determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the *Ontario Heritage Act*' and *conserved* means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted

¹ General Land Use, Section 11.2.6 Mixed-Use, City of Mississauga Official Plan (Office Consolidation September 2020). Retrieved from:

by the relevant planning authority and/or decisionmaker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments.

Built heritage resources, cultural heritage landscapes, heritage attributes, and protected heritage property are also defined in the PPS 2020:

Built heritage resources: means a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the *Ontario Heritage Act*, or that may be included on local, provincial, federal and/or international registers.

Cultural heritage landscape: means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the *Ontario Heritage Act*, or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms.

Heritage attributes: means the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built, constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (e.g. significant views or vistas to or from a *protected heritage property*).

Protected heritage property: means property designated under Parts IV, V or VI of the Ontario Heritage Act; property subject to a heritage conservation easement under Parts II or IV of the Ontario Heritage Act; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites.

For municipalities, PPS 2020 is implemented through an 'Official Plan' which may outline further heritage policies (see Section 2.5.1).

2.2 A Place to Grow - Growth Plan for The Greater Golden Horseshoe (Office Consolidation 2020)

The Greater Golden Horseshoe (GGH) is one of the North America's fastest growing regions. The GGH City Region includes the City of Toronto and 15 surrounding counties. The subject property is located within the identified 'Urban System' in the Region of Peel (Region of Peel Official Plan Schedule D). Like other provincial plans, this Plan builds upon the policy foundation provided by the PPS and provides additional and more specific land use planning policies to address issues facing specific geographic areas in Ontario. This Plan is to be read in conjunction with the PPS.

The policies of this Plan take precedence over the policies of the PPS to the extent of any conflict, except where the relevant legislation provides otherwise.²

The following guidelines and policies stated under Section 4.2.7 *Cultural Heritage Resources* of the Growth Plan for GGH³ (August 2020 Consolidation) are applicable and relevant for the subject property and its associated redevelopment:

1. *Cultural heritage resources will be conserved in order to foster a sense of place and benefit communities, particularly in strategic growth areas.*

2.3 Region of Peel Official Plan (ROP OP, Office Consolidation 2018)

The Regional Official Plan (ROP) is a long-term plan used to manage Peel's growth and development. It is a public document which provides *Regional Council* a policy framework for decision making and sets the regional context for more detailed planning by protecting the environment, managing resources and directing growth.

The subject property is located within the Region of Peel's "Urban System" (Region of Peel Official Plan Schedule D – Regional Structure). The subject property is also located within a "Designated Greenfield Area". Moreover, the subject property is situated proximally to an "Other Rapid Transit Corridor" as show on Schedule G – Rapid Transit Corridors of the Region of Peel Official Plan. The following guidelines and policies stated under Section 3.6 *Cultural Heritage* of the ROP OP⁴ (Office 2018 Consolidation) are applicable and relevant for the subject property and its associated redevelopment:

- 3.6.2.4 *Require and support cultural heritage resource impact assessments, where appropriate, for infrastructure projects, including Region of Peel projects.*

- 3.6.2.8 *Direct the area municipalities to only permit developments and site alteration on adjacent lands to protected heritage property where the proposed property has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.*

² Relationship with PPS 2020, Place to Grow Growth Plan for The Greater Golden Horseshoe Office Consolidation 2020. Retrieved from: <https://files.ontario.ca/mmah-place-to-grow-office-consolidation-en-2020-08-28.pdf>

³ Cultural Heritage Resources, Place to Grow Growth Plan for The Greater Golden Horseshoe Office Consolidation 2020. Retrieved from <https://files.ontario.ca/mmah-place-to-grow-office-consolidation-en-2020-08-28.pdf>

⁴ Cultural Heritage, Region of Peel Official Plan (Office Consolidation 2018). Retrieved from: https://www.peelregion.ca/planning/officialplan/pdfs/ropdec18/ROPConsolidationDec2018_TextSchedules_Final_TE_XT.pdf

2.4 The Ontario Heritage Act and Ontario Regulation 9/06

The Province and municipalities are enabled to conserve significant individual properties and areas through the *Ontario Heritage Act (OHA)*. Under Part III of the OHA, compliance with the Standards and Guidelines for the Conservation of Provincial Heritage Properties is mandatory for Provincially-owned and administered heritage properties.

For municipalities, Part IV and Part V of the *OHA* enables councils to 'designate' individual properties (Part IV), or properties within a heritage conservation district (HCD) (Part V), as being of 'cultural heritage value or interest' (CHVI). Evaluation for CHVI under the *OHA* is guided by *Ontario Regulation 9/06*, which prescribes the *criteria for determining cultural heritage value or interest*. The criteria are as follows:

1. (1) The criteria set out in subsection (2) are prescribed for the purposes of clause 29 (1) (a) of the Act.

(2) A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:
 1. The property has **design value or physical value** because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.
 2. The property has **historical value or associative value** because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
 3. The property has **contextual value** because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark.

If a property meets one or more of these criteria, it may be eligible for designation under Part IV, Section 29 of the *OHA*. Once a property is placed on the Register, it gains public recognition, and a level of heritage due diligence is exercised for planning, building and/or demolition permit applications. The City of Mississauga maintains one register⁵, a register of properties that are listed or designated cultural heritage resources under the *Ontario Heritage Act*. This register is known as the "**Heritage Register of Mississauga**".

⁵ Heritage Register for Mississauga. Accessed from: https://www.mississauga.ca/wp-content/uploads/2020/06/10135918/2018-07-01_Mississauga_Heritage_Register_Web.pdf

2.4.1 MINISTRY OF HERITAGE, SPORT, TOURISM AND CULTURE INDUSTRIES

As mentioned above, heritage conservation on provincial properties must comply with the MHSTCI *Standards and Guidelines for the Conservation of Provincial Heritage Properties*. To advise municipalities, organizations, and individuals on heritage protection and conservation, the MHSTCI developed the following:

- **Standards and Guidelines for Conservation of Provincial Heritage Properties**
These standards and guidelines set out the criteria and process for identifying provincial heritage properties and to set standards for their protection, maintenance, use and disposal.
- **Ontario Heritage Tool kit:**
The Ontario Heritage Tool Kit is a series of guides for municipal councils, municipal staff, Municipal Heritage Committees, land use planners, heritage professionals, heritage organizations, property owners and others. It was designed to help them understand the heritage conservation process in Ontario.⁶

The *Heritage Resources in the Land Use Planning Process* (MHSTCI) defines an HIA as follows: 'A heritage impact assessment (or equivalent study) is a study to determine if any cultural heritage resources (including those previously identified and those found as part of the site assessment) or in any areas of archaeological potential, are impacted by a specific proposed development or site alteration. It can also demonstrate how the cultural heritage resource will be conserved in the context of redevelopment or site alteration. Mitigative or avoidance measures or alternative development or site alteration approaches may be recommended.'

Determining the optimal conservation or mitigation strategy is further guided by the MHSTCI 'Eight guiding principles in the conservation of historical properties.'

The following guiding principles for the conservation of historical properties are based on international charters that have been established over the past century:⁷

1. Respect for documentary evidence

Do not base restoration on conjecture. Conservation work should be based on historical documentation, such as historical photographs, drawings and physical evidence.

2. Respect for the original location

Do not move buildings unless there is no other means to save them. Site is an integral component of a building. Any change in site diminishes heritage value considerably.

⁶ Ontario Heritage Tool kit, Culture, Heritage, Ministry of Heritage, Sport, Tourism and Culture Industries. Accessed from: http://www.mtc.gov.on.ca/en/heritage/heritage_toolkit.shtml

⁷ Eight guiding principles in the conservation of historical properties, Ontario Heritage Trust. Accessed from: <https://www.heritagetrust.on.ca/en/pages/tools/tools-for-conservation/eight-guiding-principles>

3. Respect for historical material

Repair or conserve rather than replace building materials and finishes, except where absolutely necessary. Minimal intervention maintains the historical content of the resource.

4. Respect for original fabric

Repair with like materials, to return the resource to its prior condition without altering its integrity.

5. Respect for the building's history

Do not restore to one period at the expense of another. Do not destroy later additions to a house solely to restore it to a single time period.

6. Reversibility

Alterations should be able to be returned to original conditions. This conserves earlier building design and technique. For instance, when a new door opening is put in a stone wall, the original stones are numbered, removed and stored, allowing for future restoration.

7. Legibility

New work should be distinguishable from old. Buildings should be recognized as products of their own time, and new additions should not blur the distinction between old and new.

8. Maintenance

With continuous care, future restoration will not be necessary. With regular upkeep, major conservation projects and their high costs can be avoided.

2.5 CITY OF MISSISSAUGA PLANS AND POLICIES**2.5.1 City of Mississauga Official Plan Policies**

City Council adopted Mississauga's current Official Plan in September 2010. The updated September 2020 Office Consolidation includes LPAT decisions that have resolved several of the appeals to the 2010 Official Plan and amendments made to reflect Council decisions.⁸ The following guidelines and policies stated under Section 7.4 Heritage Planning of the Mississauga Official Plan⁹ (September 2020 Consolidation) provide direction for development on or adjacent to the heritage resource:

7.4.1.2 Mississauga will discourage the demolition, destruction or inappropriate alteration or reuse of cultural heritage resources.

7.4.1.3 Mississauga will require development to maintain location and settings for cultural heritage resources that are compatible with and enhance the character of the cultural heritage resource.

⁸ Mississauga Official Plan, Consolidated Versions. Accessed from:

http://www6.mississauga.ca/onlinemaps/planbldg/MOP/OfficeConsolidation_and_Information/2-Subtab-I-Consolidation-Versions-September3,2020.pdf

⁹ Heritage Planning, Cultural Heritage Resources, Mississauga Official Plan (Office Consolidation September 2020). Retrieved from: <http://www6.mississauga.ca/onlinemaps/planbldg/MOP/Chapter7-CompleteCommunities-July13-2016.pdf>

7.4.1.11 Cultural heritage resources designated under the Ontario Heritage Act, will be required to preserve the heritage attributes and not detract or destroy any of the heritage attributes in keeping with the Ontario Heritage Tool Kit, the Ontario Ministry of Culture, and the Standards and Guidelines for the Conservation of Historic Places in Canada, Parks Canada.

7.4.2.3 Development adjacent to a cultural heritage property will be encouraged to be compatible with the cultural heritage property.

9.3.1.10 Consideration will be given to the location of utilities on private property and the public right-of-way. Utilities will be grouped or located underground where possible to minimize visual impact. The City encourages utility providers to consider innovative methods of containing utility services.

9.5.2.12 Heating, venting and air conditioning equipment and mechanical/utility functions will be located away from the public realm and not be visible from public view.

The following policy outlined in the City of Mississauga's Official Plan, direct the undertaking of Heritage Impact Assessment within the City:

7.4.1.12 The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Statement, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.

2.5.2 Zoning By-Law

The Mississauga Zoning and Property Information online portal displays the existing zoning 'D - Development' on the subject property at 1130-1140 Clarkson Road N with land use designation as 'Mixed Residential Commercial < five storeys under the City of Mississauga Zoning By-law 0225-2007'¹⁰, see property report (Appendix G).

¹⁰ 1130-1140 Clarkson Road North, Mississauga Property Report. Retrieved from:
<https://ext.maps.mississauga.ca/Html5Viewer/index.html?viewer=izbl.HTML5#>



Figure 2: Zoning Map for 1130-1140 Clarkson Road N, Annotated by AREA (Mississauga Zoning and Property Information).

Column A		B
Line 1.0	ZONE	D
PERMITTED USE		
2.0	DEVELOPMENT	
2.1	A building or structure legally existing on the date of passing of this By-law and the existing legal use of such building or structure	✓
ZONE REGULATION		
3.0	The erection of new buildings or structures and the enlargement or replacement of existing buildings and structures shall not be permitted	✓

Figure 3: D Zone permitted uses and Zone Regulations
(Retrieved from: <https://web.mississauga.ca/apps/zoningbylaw/#/show/12,3248,3250>)

3.0 INTRODUCTION TO SUBJECT PROPERTY

3.1 LOCATION OF THE SUBJECT PROPERTY

Municipal Address	1130-1140 Clarkson Road N, Mississauga, Ontario (Figure 1)
Legal Description	Part of Lot 67, Registered Plan G-13, designated as Parts 1 and 2, Plan 43R-37471, formerly part of Lot 28, Concession 2. SDS, Toronto Township (Figure 4).
Square Area	The subject property has an estimated area of 19045 square ft. (1769 sq.m.)
Location & Boundaries	This property is located at 1130-1140 Clarkson Street North, north of Lakeshore Road West on the northwest corner of the Canadian National / Go Transit rail corridor (Figures 1,2,4)
Official Plan Designation	The subject property is located within the Clarkson-Lorne Park Neighbourhood Character Area, as defined by the City of Mississauga Official Plan, and is designated 'Mixed-Use' (Schedule 10 Land Use) and 'Neighbourhood' (Schedule 9 City Structure).
Zoning By-Law	The existing zoning on the subject property is 'Development' under the City of Mississauga Zoning By-law 0225-2007 (Figure 3)

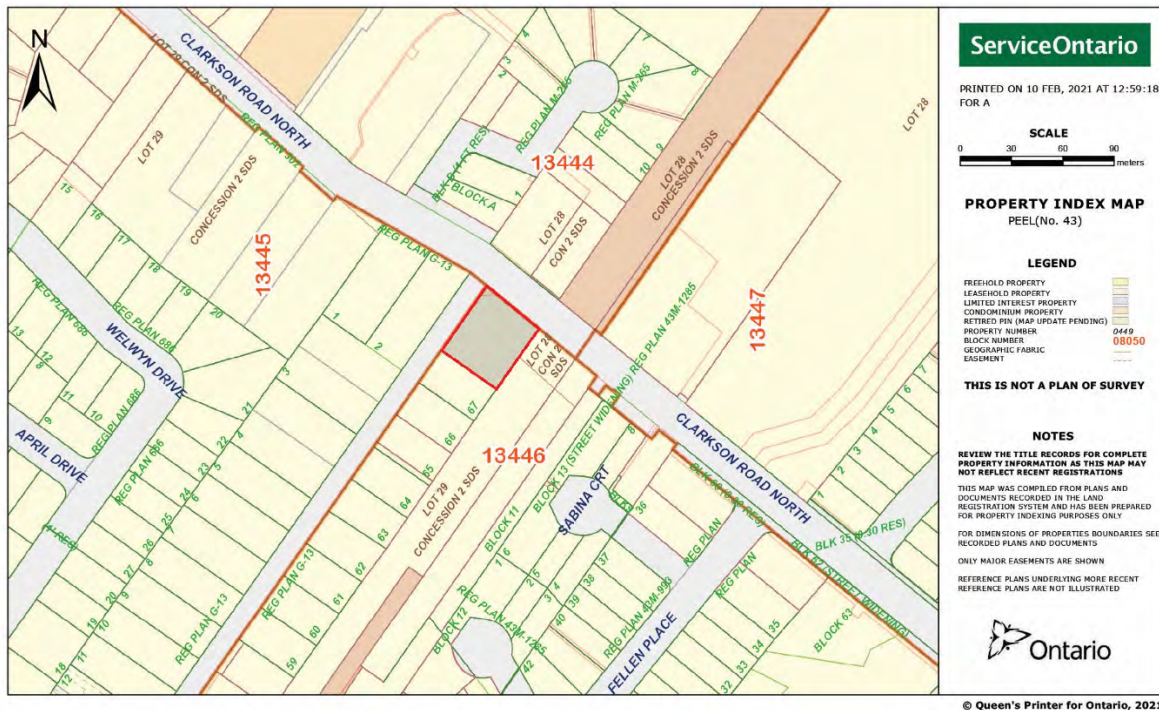


Figure 4: Property Index Map showing the approximate extents, lot number, block number of the subject property¹¹. (Source: Ontario Land Registry 2020)

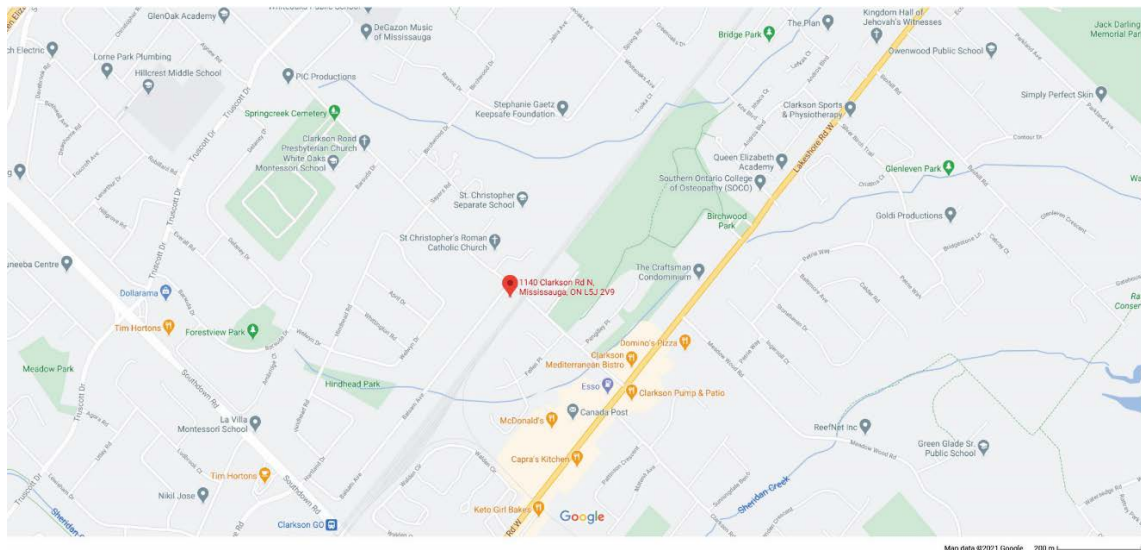


Figure 5: Aerial map showing the location of the subject property in red in the City of Mississauga (Google Maps 2021).

¹¹ Property Index Map, Ontario Land Registry. Retrieved from:
https://www.onland.ca/api/cmv/export/_ags_WebMap_afaa6370-6bc9-11eb-9809-005056975444.pdf

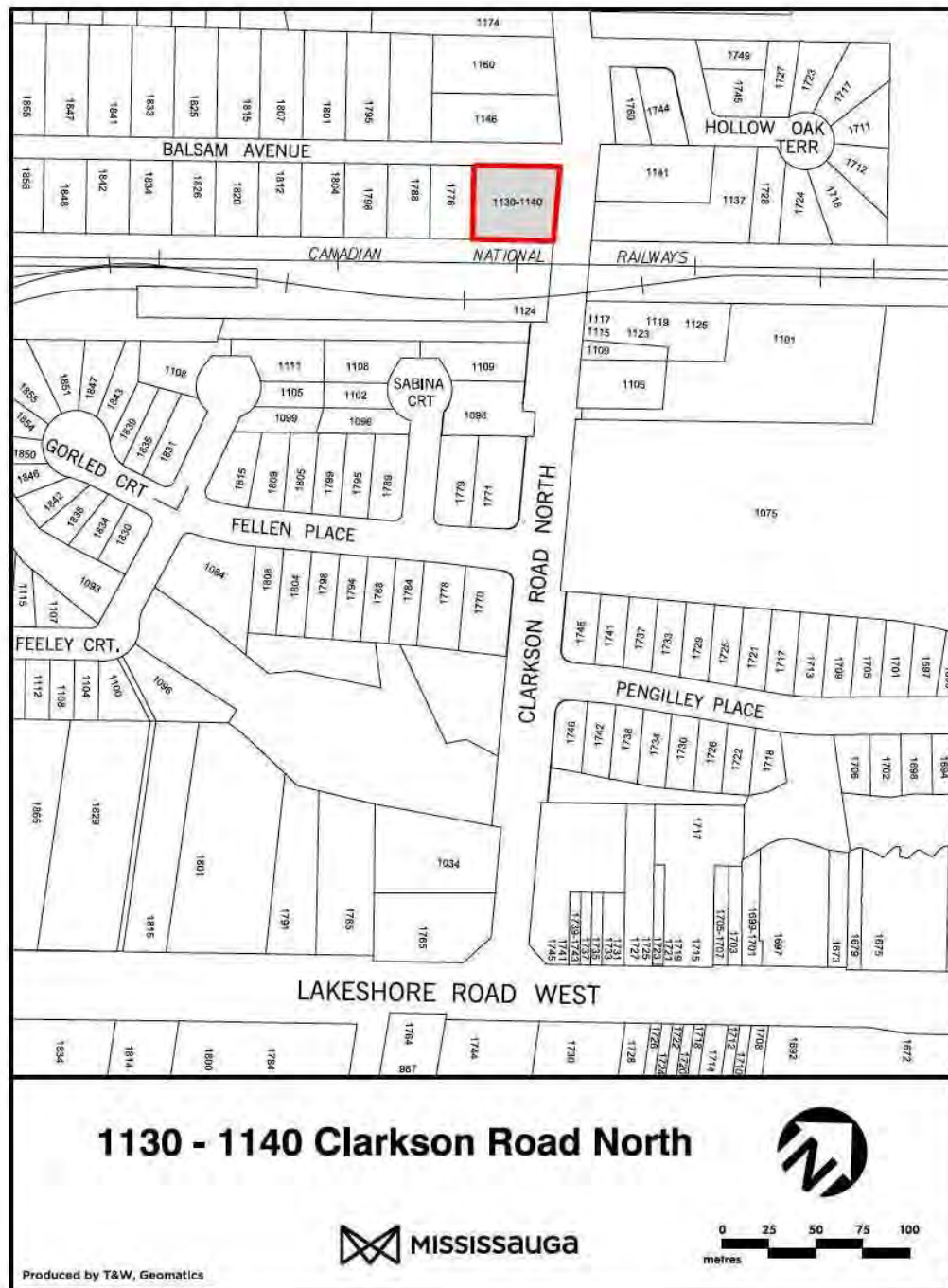
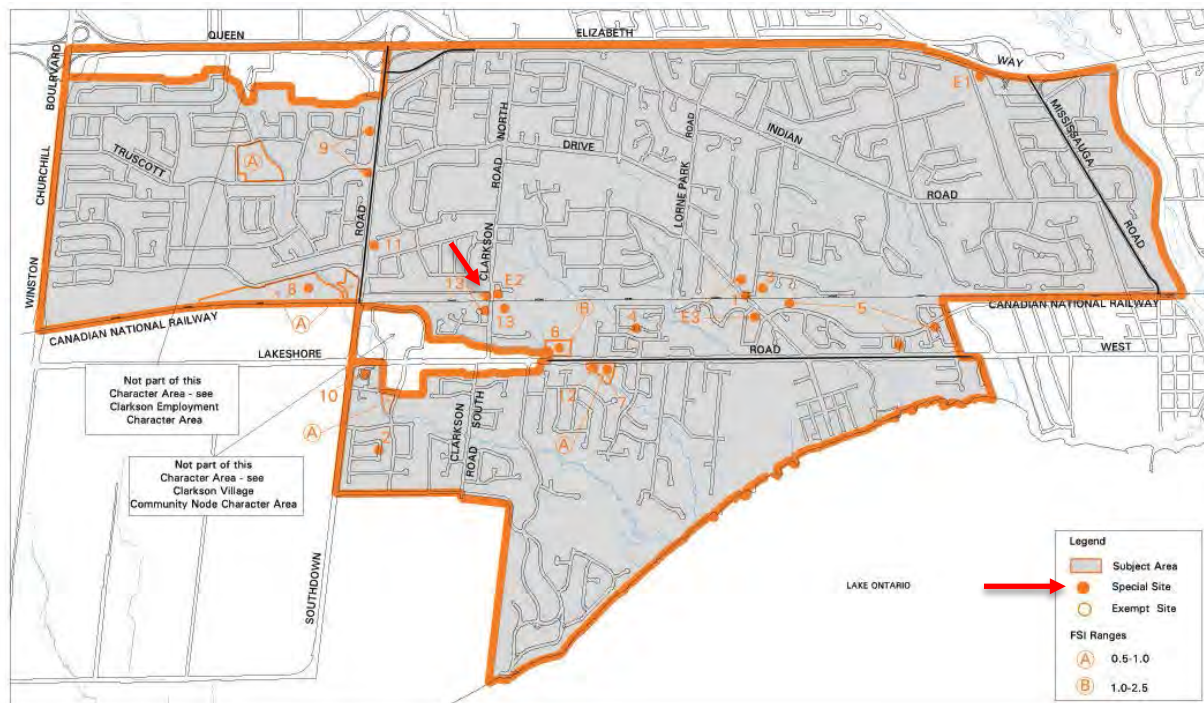


Figure 6: Plan showing the subject property and surrounding lots and city roads.
(Source: Culture Division, City of Mississauga)

3.2 DESCRIPTION OF SUBJECT PROPERTY

The subject property's address is 1130-1140 Clarkson Road North, northwest corner of the Canadian National / Go Transit rail corridor in the City of Mississauga (Figure 5). The property is located within the Clarkson-Lorne Park Neighbourhood Character Area (Figure 7) which is an established neighbourhood containing large lots and mature vegetation. The land is identified as a 'Special Site 13', located on the west side of Clarkson Road, south of Balsam Avenue.



Map 16-5: Clarkson-Lorne Park Neighbourhood Character Area

Figure 7: Map 16-5: Clarkson-Lorne Park Neighbourhood Character Area¹², annotated by AREA
(Source: Mississauga Official Plan – Part 3)

3.2.1 Site

This site is in close proximity to Lakeshore Road West and Clarkson Village Community Node, a predominantly mixed-use area. The immediate area contains mostly detached homes as well as some commercial uses north and south of the CN Railway on the east side of Clarkson Road North. 1141 Clarkson Road North, is listed on the City's Heritage Register and contains the 'Clarkson Paisley House' that was initially commissioned by Henry Clarkson in 1936. The local area is historically known as 'Clarkson Corners', and was a service stop on the railway in the 1800s. The rail station was the hub of the community from 1850 to 1950.

¹² Map 16-5: Clarkson-Lorne Park Neighbourhood Character Area, Mississauga Official Plan – Part 3. Retrieved from: <http://www6.mississauga.ca/onlineplans/planbldg/MOP/Chapter16-Neighbourhoods-March13-2019.pdf>



Figure 8: GO Transit tracks adjacent to the subject property



Figure 9: Rear of the lot (West elevation), proposed location for the underground ductbank.



Figure 10: East Elevation (Front)



Figure 11: West Elevation (Rear)



Figure 12: South Elevation (facing the tracks)



Figure 13: North Elevation showing the side elevation of William Clarkson House and Store.

3.2.2 Buildings

1130-1140 Clarkson Road North is a rectangular shaped property and is comprised of the following three buildings that are listed on the City's Municipal Heritage Register as cultural heritage resources (Figure 8):

- A. The Clarkson General Store;
- B. The former Post Office building &
- C. The William Clarkson House.



Figure 14: Aerial view of the subject property
(Source: Google maps 2020).

The buildings front onto Clarkson Road North and the southwest portion of the lot has soft landscaping with no driveway. The above properties at 1130-1140 Clarkson Road North were designated as being of cultural heritage value or interest under the Ontario Heritage Act. The By-Law 0057-2017 was enacted and passed on 26th April 2017 (Appendix G). The properties are described separately.

William Clarkson House

The William Clarkson House is a one-and-a-half storey Gothic Revival home dating back from the latter half of the 19th century. William was the son of Warren Clarkson.



Figure 15: The main elevation of the William Clarkson House.
(Source: Mississauga, Historical Images Gallery)



Figure 16: William Clarkson Store built in 1835 and Post Office, Clarkson, built in 1837
(Source: Mississauga, Historical Images Gallery)

The William Clarkson House is a vernacular wood frame residence with a front/cross gable roof, rectangular floor plan, symmetrical arrangement of openings on the principal facades and a wrap-around verandah. The exterior cladding material is clapboard siding with corner boards and door and window openings with triangular shaped heads on the front elevation. Despite later alterations to the exterior including the installation of new siding, the addition of two roof dormers, removal of the original chimneys and some modifications to the door and window openings, the building retains its dominant form and exterior elements of its original design character.



Figure 17: William Clarkson House, 2021

General Store and Post Office building

William took over operation of the family store and post office in 1865. (Figure 16). The General Store and Post Office provided one of the cornerstones of the community. On this site Warren Clarkson built the first store around 1835. The current building dates from the later half of the 19th century. The Clarkson Store was substantially altered from its 19th century form c. 1906-1910. These early 20th century alterations are considered to be of sufficient age as to form an integral part of the history of the building. Although more recent exterior alterations have been completed (Figure 18), the store building still retains its 1906-1910 height, scale, shape, form and massing and a commercial storefront of an early 20th century rural store¹³.



Figure 18: General Store and Post Office building, February 2021

3.2.3 Context

The subject property is located at the intersection of Balsam Rd. and Clarkson Road North.

The site's present surroundings are representative of suburban development in Ontario. Immediate north & west of the subject site are large residential lots with detached dwellings. Lands south comprise of the Go Transit rail corridor (Figure 24) & east of the subject site is designated mixed-use. Further southeast, a large community park is located.

Historically the vicinity was dominated by agricultural uses.

¹³ Schedule B to By-Law No. 0057-2017, Designation Statement.

3.3 EXISTING CONDITION

A high-level condition assessment of the original Clarkson House and General Store building was undertaken during a February, 2021 site review. Conditions were assessed with a visual review from grade with no destructive testing used. Interiors of the property were not accessed as no alterations both interior and exterior are proposed for the cultural heritage resources.



Figure 19: North Elevation of the William Clarkson House and store



Figure 20: View of the William Clarkson House and store from Balsam Avenue looking southeast



Figure 21: View of the subject lot from Clarkson Road North looking southwest.



Figure 22: View of the subject lot from Clarkson Road North looking northwest.

The windows and doors were all boarded at the time of review (Figures 19-22). The William Clarkson House had undergone substantial renovations to the exterior and interior of the building in 2014. No further modifications were observed till date. The rear addition to the house dates back to 1913. A wrap-around verandah possibly existed on the south side along with a rear building which was demolished later (See Appendix F, Figure 59). The Clarkson Store was a brick structure which was later reclad. After the addition of the second floor and a north addition

around 1910, further storefront renovations were carried out in 1934 followed by alterations in 2014-2015. The following photos provide views of the current development adjacent to the subject lot.



Figure 23: View of adjacent residential property west of the subject lot from Balsam Avenue looking southwest.



Figure 24: View of adjacent listed heritage property from Clarkson Road North looking northwest.



Figure 25: Intersection at Clarkson Road North and Balsam Avenue looking north.



Figure 26: View of GO Train tracks and property beyond from the subject lot looking south.



Figure 27: Looking north on Clarkson Road North



Figure 28: Clarkson Go Station south west of the subject lot.

3.4 HERITAGE STATUS OF CHR AND ADJACENT HERITAGE PROPERTIES

1130-1140 Clarkson Road North is included on Mississauga's Municipal Register of Cultural Heritage Resources as a 'Designated' heritage property.¹⁴

The following adjacent heritage properties are included in the Mississauga's Municipal Register of Cultural Heritage Resources (2020) as 'Listed' heritage properties¹⁵.



Figure 29: Adjacent Heritage properties, Mississauga's Municipal Register of Cultural Heritage Resources (2020)

1. 1141 Clarkson Road North (Listed on The Heritage Register but Not Designated)
2. 1160 Clarkson Road North (Listed on The Heritage Register but Not Designated)
3. 1182 Clarkson Road North (Listed on The Heritage Register but Not Designated)
4. 1188 Clarkson Road North (Listed on The Heritage Register but Not Designated)

¹⁴ Mississauga Heritage Board, "Municipal Register of Cultural Heritage Resources 'Listed' Heritage Properties Last Updated: 2020,"

https://www.Mississauga.ca/EN/Arts-Culture-Tourism/Cultural-Heritage/Documents1/Listed_Register.pdf

¹⁵ Ibid, 30.

4.0 STATEMENT OF CULTURAL HERITAGE VALUE OR INTEREST

The Designation Statement in By-Law 0057-2017 includes the following Statement of Cultural Heritage Value or Interest for the subject property:

'The subject property, located at 1130-1140 Clarkson Road North in the City of Mississauga is an important and significant cultural heritage property that has design, associative and contextual values.

Design Value

The design/physical value of the subject property relates specifically to the Clarkson Store and the William Clarkson House.

The Clarkson Store was substantially altered from its 19th century form c. 1906-1910. These early 20th century alterations are considered to be of sufficient age as to form an integral part of the history of the building. Although more recent exterior alterations have been completed, the store building still retains its 1906-1910 height, scale, shape, form and massing and a commercial storefront of an early 20th century rural store. Examples of rural commercial stores of this age are considered to be a diminished resource in the City.

The William Clarkson House is a vernacular, mid-19th century, wood frame residence with characteristics of the Gothic Revival style. When built, it was one and one-half storeys high with a front/cross gable roof, rectangular floor plan, symmetrical arrangement of openings on the principal facades and a wrap-around verandah. Exterior details included clapboard siding with corner boards and door and window openings with triangular shaped heads on the front elevation. Despite later alterations to the exterior including the installation of new siding, the addition of two roof dormers, removal of the original chimneys and some modifications to the door and window openings, the building retains its dominant form and exterior elements of its original design character.

Historical/Associative Value

The property has historical value for its associations with an important settlement family that contributed significantly to the settlement of Clarkson and Toronto Township in the 19th century.

In recognition of the significance of the Clarkson store to the community, the road passing in front of the general store, and later post office, the train station, and the village were all named Clarkson. The local GO Transit train station still bears the family name. The Clarkson family was fundamental to the development of the local community. Warren Clarkson was one of the first settlers in this area of Toronto Township. His decision to locate the first general store in the area, the construction of the railway beside the store, which ensured the community's economic viability, and the location of the first postal service in the store represent and illustrate important social and development periods of the community's history. The property was the site of the first post office in Clarkson, an event that meant local residents no longer had to travel to Erindale to retrieve their mail. The post office was located in the Clarkson Store, the focal point of the

community. It was located on the subject property for 80 years from 1875 to 1955. A new, separate post office building was constructed in the late 1940s beside the store and house.

Contextual Value

The subject property, in its entirety, has been identified as having contextual value relating to the historical, visual, physical and functional links between the property, its buildings and the surrounding neighbourhood that once formed the nucleus of Clarkson's Corners, later Clarkson, people and activities that were significant to the community and its ability to yield information that contributes to an understanding of the community.

The property was the site of Warren Clarkson's store in 1835. The store became the nucleus of the historic settlement community and in 1875, the location of the first post office to serve Clarkson. The three buildings on the subject property are positioned on the north side of the railway tracks facing onto and set close to Clarkson Road North, an important transportation north to south route both historically and currently. This setting of the subject property is valued for maintaining the 19th and early 20th century character of the historic village core of Clarkson village. The original Clarkson land holdings were subdivided as Plan G-13 in 1913, and the buildings were placed on Lot 67. The configuration of Lot 67 has not been subdivided since 1913 and is clearly delineated in the environment between the railway tracks to the south and Balsam Avenue to the north.

The property with its buildings is the pivot point for historically, visually and contextually linking the adjacent properties associated with the Clarkson family and the village. The Clarkson Store on the south end beside the railway track and the William Clarkson House on the north end of the property at Balsam Avenue are clearly visible in the streetscape. The subject property at 1130-1140 Clarkson Road North is considered to be a physical landmark in the community.

Heritage Attributes

The heritage attributes of the subject property located at 1130-1140 Clarkson Road North include, but are not limited to the following listed.

- The placement and orientation of the buildings, the Clarkson Store, the former Post Office Building and the William Clarkson House, in their original positions on the west side of Clarkson Road North to the north at the railway tracks with the principal elevations facing onto the said road.
- The integrity of Lot 67, Plan G-13 on which the three buildings are located.
- Its pivotal position in the historic core of Clarkson as an historical, visual and contextual link adjacent to the neighbourhood properties associated with the Clarkson family, former commercial, industrial and residential buildings that were clustered around the store and the location of the former Clarkson's railway station.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings and the commercial storefront of the Clarkson Store.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings, and verandah of the William Clarkson House.
- The north and south views to the subject property and its three buildings from Clarkson Road North.

4.1 HISTORICAL / ASSOCIATIVE SUMMARY

4.1.1 TORONTO TOWNSHIP

The property is located within the Traditional Lands of the Mississaugas of Credit First Nation. The Mississaugas were hunter-gatherers whose way of life involved seasonal migration. During the summer season they would camp along creeks, cultivating corn along flats and fishing for salmon. After the harvest they returned to the interior hunting grounds for the colder months. Until the latter 18th century, contact with Europeans was limited mostly to French traders through travel or at forts and outposts.

In 1763, the Seven Years' War between Great Britain and France ended with France ceding its north American territories to Great Britain. King George III issued the Royal Proclamation which established the basis of government administration in north American territories. The Proclamation forbade the settlement of territories by non-First Nations and established that First Nations lands could only be transferred through negotiation and sale to the Crown. By this process, large portions of southern Ontario were acquired via treaty and subsequently divided into counties and townships. The surveys created a grid of concessions and lots (typically 200 acres) that formed the basis for private ownership and settlement.

Settlement in this portion of southern Ontario began after the American Revolution (1775-1783) when Loyalists and discharged British soldiers migrated north. In 1806 the Mississaugas of the Credit and the British signed the Head of the Lake Treaty (No. 14) covering the lands along Lake Ontario between Etobicoke Creek and Burlington Bay and north for six miles. The Mississaugas maintained two reserves, a mile on each side of the Credit River as well as the sole right of fisheries on 12- and 16-Mile Creeks. In addition, the Mississaugas of the Credit held another 648,000 acres of land north of the Head of the Lake lands. Following the War of 1812, the influx of settlers into the Mississaugas' lands and fisheries weakened their traditional economy, putting their rapidly declining population into a state of impoverishment. In 1818, Chief Ajetance, on behalf of the assembled people, agreed to the sale of the 648,000 acres for £522.10 of goods paid annually.

Immediately following the Ajetance Treaty, the land was surveyed and divided into the townships of Chinguacousy, Toronto Gore, Albion and Caledon and the enlarged Toronto Township.¹⁶ Toronto Township came into being, comprising 29,569 acres, with a mile (1.4 kilometers) on either side of the Credit River designated as the Mississauga Indian Reserve¹⁷. This brought the establishment of small communities in the Toronto Township. Sydenham (Dixie) & Harrisville (Cooksville) along Dundas, Lakeview to the east and Clarkson to the west along the shores of Lake Ontario were formed throughout the Toronto Township.

¹⁶ When these townships came into existence, they were administered by the Home District Court of Quarter Sessions out of York. In 1850, Districts were dissolved and replaced with Counties which were smaller administrative units.

¹⁷ Kathleen A. Hicks, *Clarkson and Its Many Corners* (Mississauga, Ontario: Mississauga Library System, 2003), XVI

4.1.2 TOWNSHIP OF CLARKSON

Settlement first began in this rural village in 1807 after the first survey and among the first settlers were the Bradley, Clarkson, Gable, Greeniaus, Hammond, Hendershott, Jarvis, Marlatt, Merigold, Monger, Oliphant, Shook, and Thompson families (Figure 30). The area was first referred to as “Merigold’s Point”, and later became known as “Clarkson’s Corners” after early settler Warren Clarkson, who also operated the post office and general store. Peter Hess received the Crown Patent for all 200 acres of Lot 29, Concession 2 SDS, in 1815 and sold 120 acres to Malcolm Wright in the same year. His heir Archibald Wright sold part of Lot 29, Concession 2 SDS, to Warren Clarkson in February 1819 on which he built a generous for room timber frame house 20ft x 30ft on a stone foundation. This structure was abandoned in 1822. By 1850, the road bordering Warren Clarkson’s property was known as Clarkson Road North and a train station for the Great Western Railway was built in 1855 on part of Warren Clarkson’s property (Heritage Mississauga 2009).¹⁸

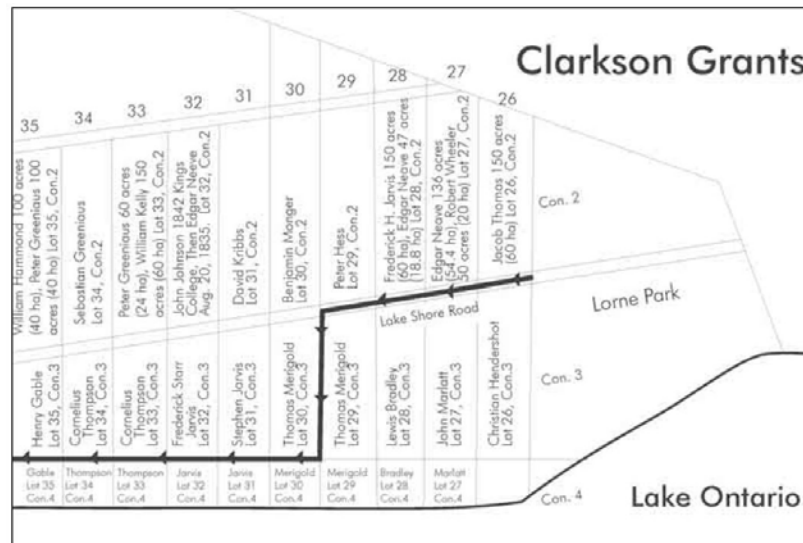


Figure 30: Concession Map showing the resident families in the village of Clarkson, 1833
(Source: Clarkson and its Many Corners, Kathleen A.Hicks, 2003)

4.1.3 CLARKSON ROAD NORTH

Clarkson Road North consisted mainly of a wagon trail until 1845, when Captain James Harris and Warren Clarkson donated the land to enhance the road. Clarkson had a store on the west side of the road, north of Lake Shore Road. There is a short jog in the road, which starts in front of the subject property, which was created so that Clarkson’s store could front the new road. The road was officially opened and named in 1850 by Toronto Township Council¹⁹. This ensured that the road would be maintained by the Township.

¹⁸ ASI, Heritage Impact Assessment 1141 Clarkson Road North City of Mississauga, Ontario, (2017)

¹⁹ Kathleen A. Hicks, *Clarkson and Its Many Corners* (Mississauga, Ontario: Mississauga Library System, 2003), 52

4.1.4 HISTORY OF OWNERSHIP

The Gables (1807), the Greeniauses (1808), the Marigolds and the Mongers (1808) were the first residents of the Clarkson area (Figure 30). It was not until 1833 that the initial patent of the 150 acres at Concession 2, WHS, Lot 28 (the property) was made to Frederick Jarvis (Figure 31). The same year, Jarvis sold the land to Edgar Neave who sold 50 acres to Warren Clarkson in 1835 (Figure 30). Neave owned a number of lots in the area and is best known for being the original builder of "Benares", the house and present-day museum at 1503 Clarkson Road. The General Directory for the City of Toronto and Gazetteer of the Counties of York and Peel, for 1866 also lists Warren Clarkson, Henry Warren Clarkson and William Clarkson as settlers in the Township of Toronto Ward No.1. (Appendix E)

Township of Toronto LOT 28 CONCESSION 2000 PAGE NO. 1

THIS IS A COMPILED
ABSTRACT FOR LOT 28
SHOWING DEEDS 1-13

REGISTRATION NUMBER	INSTRUMENT	DATE OF INSTRUMENT	REGISTRATION DATE	GRANTOR	GRANTEE	CONSIDERATION ETC.	LAND AND REMARKS
	Patent	31 Dec 1833		The Crown	Frederick Jarvis		150 acres
10439	Bt. J.	20 Dec 1833	17 Jan 1834	Frederick Jarvis et ux	Edgar Neave	£300	150 acres
12120	Bt. J.	14 Aug 1835	15 Sept 1835	Edgar Neave et ux	Warren C. Clarkson	\$162.107	50 acres & 1/2
12141	Bt. J.	21 Aug 1835	22 Sept 1835	Edgar Neave et ux	George Simcott	£57	100 acres
14525	Bt. J.	31 Aug 1837	19 Sept 1837	George Simcott	James B. Harris	£850	148.5 ac + 0.1
27	J.	15 Feb 1853	19 Feb 1853	Warren Clarkson	Hamilton & Toronto Ry. Co.	£4111	2 1/2"
2395	Bt. J.	17 Feb 1855	25 Jan 1856	Warren Clarkson	Henry J. Clarkson	£200	42 1/2 sq ft
8407	Deed	17 Feb 1855	4 Sept 1860	Henry J. Clarkson	Warren Clarkson		42 1/2 sq ft
	Patent	16 Oct 1834		The Crown	Edgar Neave		47 acres
12141	Bt. J.	21 Aug 1835	22 Sept 1835	Edgar Neave et ux	George Simcott	£57	47 acres
14525	Bt. J.	31 Aug 1837	19 Sept 1837	George Simcott	James B. Harris	£850	48 1/2
	Book 8						
3712	Will	15 May 1877	25 Sept 1882	Warren Clarkson	Mary Ann Clarkson et al		Part and O.L.
5204	Will	24 Aug 1875	22 Feb 1884	James B. Harris	Elizabeth Harris et al		Part and O.L.
6167	B. D.	1 Jul 1887	14 Jul 1887	Arthur B. Harris et ux	Harry D. White	\$122700	8.18 ac.
7439		28 Jan 1891	31 Jan 1891	Wm. W. Clarkson et ux	Bank of W. England Co.	2500.00	Part and O.L. NW of Railway
7449	Release	9 Feb 1891	13 Feb 1891	Matthew Deby Ltd	Wm. W. Clarkson	31.00	Part and O.L.
7652	Bt. J.	26 Jan 1891	17 Aug 1891	Harry D. White et ux	Robert Sloan	2649.50	8.18 ac
7927	Release	7 Feb 1891	5 Jul 1892	Matthew Deby Ltd	Henry D. Clarkson	100.00	Part and O.L.
8276	B. D.	11 Oct 1893	13 Oct 1893	Arthur B. Harris et ux	David A. Noble	541.12	14.43 ac
8277	B. D.	11 Oct 1893	13 Oct 1893	David A. Noble	Arthur B. Harris	1623.38	14.43 ac
9546	Release	13 Jul 1897	22 Oct 1897	David A. Noble (unmarried)	Arthur B. Harris	1.00	14.43 ac. Rel of Equit. of Red
10358		31 Oct 1900	12 Dec 1900	Bank of W. England Co.	George Wooderham	2000.00	Assigning No. 7139
10366	B. D.	10 Dec 1900	22 Dec 1900	Warren C. Clarkson et ux	George Wooderham	1000.00	Part and O.L. NW of Railway
10540	Deed	1 Jun 1901	17 Jun 1901	Henry D. Clarkson	Samuel Wm. Marchmont	150.00	payable 5 years for the term of 5 years. 1.58 ac NW of Railway
10590	B. D.	11 Dec 1901	12 Dec 1901	George Wooderham CP	Emma Clarkson et al (CP)		25 ac. and O.L. Deft. debared & foreclosed
11629	B. D.	1 May 1904	13 Sep 1904	Arthur B. Harris et ux	The Toronto & Niagara Power Co.	2000.00	Part and O.L.
11994	B. D.	17 Jul 1905	25 Jul 1905	Arthur B. Harris et ux	The Toronto & Niagara Power Co.	2000.00	154/1000 ac.

Figure 31: Land Registry Records, Township of Toronto
Retrieved from: Ontario Land Registry Access – Historical Books
<https://www.onland.ca/ui/43/books/42302/viewer/706285865?page=787>



Figure 34: 1936 Clarkson Fire insurance Map (Underwriters' Insurance Bureau 1936)
Study area annotated by AREA in red, Adjacent listed Heritage property 1141 Clarkson Road N in grey.

4.1.5 HISTORY OF LANDUSE

The property is located in the village of Clarkson. The history of the property reflects the agricultural character of the area. The 1861 Census identified Warren Clarkson as a 69-year-old farmer, residing with his wife at the property. The personal census lists the family as living in a 1 ½ storey frame house (Figure 35).²⁰

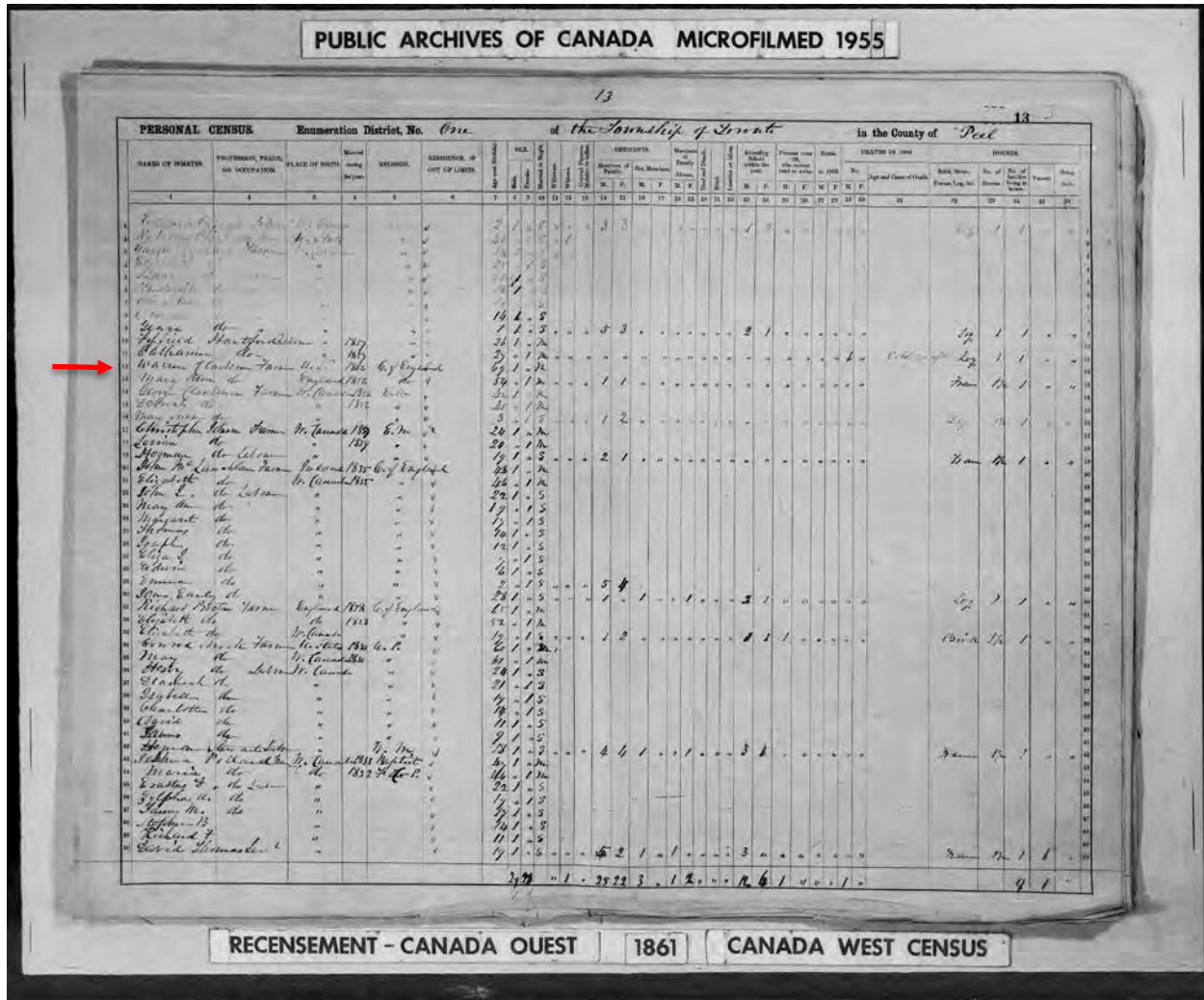


Figure 35: Personal Census, Enumeration District 1, Township of Toronto, County of Peel
(Source: Library and Archives Canada)

In 1835, Warren Clarkson bought 50 acres of the southern portion of Lot 28, from Edgar Neave for £162 (Peel Region Land Registry Office, Conc. 2SDS, Lot 28). In addition to the land in Lot

²⁰ Library and Archives Canada, 1861 Personal Census, Enumeration District 1, Township of Toronto, County of Peel, p. 13.

28, Clarkson owned the southern portions of Lots 29 and 30, which he bought in 1819.²¹ In 1856, Clarkson put the 50 acres in Lot 28 in his son Henry's name (Figure 29). Henry went to the United States for a short time in the late-1850s to work as a railway surveyor-foreman, and according to the 1861 Census (District 1, Toronto Township), his brother George lived on the property and farmed potatoes. When Henry returned to Canada, he built a house and assisted his father with the farm's operations.²² The house does not appear on the 1860 Tremaine Map (Figure 30) or the 1877 County Atlas Map (Figure 31). The following excerpt from 'Clarkson and its Many Corners' by Kathleen A. Hicks narrates the land use development under the ownership of Warren Clarkson:

'By 1835, Warren Clarkson owned land on Lots 28, 29 Concession 2SDS, on either side of the wagon trail running north from Lakeshore Road. The same year Warren built and opened the first grocery store in the area on the dirt trail that would become Clarkson Road. With the stagecoaches travelling along Dundas Street and south from Erindale, it became a waiting room for travelers. In 1837, he built what he called a trading post in the Port Credit area where the harbour was a port of call for the vessels that plied Lake Ontario. In 1852, he sold the Port Credit store to Robert Cotton, who sold it to James Hamilton in 1860. This is where the first telephone in Toronto Township would be installed in 1881.'

Later in 1853, the Great Western Railroad purchased nearly six acres of Warren's property for the right-of-way, a station and siding space. The station was called "Clarkson's". William Clarkson took over the management of the Clarkson Store from his father, and built a house for his family, beside the store, this how is now the William Clarkson house. Historical research suggests a mid-1860s construction date however it may have been constructed later in 1870s or early 1880s. William's brother Henry Clarkson also built a house on Clarkson Road North near the family grocery store and house in the 19th century. By the end of the 19th century, the village of Clarkson included, but was not limited to, the Clarkson Store and post office, the William Clarkson House, the house built by Warren and Susan Clarkson and a house owned by Henry Clarkson and Clarkson's railway station²³.



Figure 36: Henry Shook Clarkson' house. (Source: Mississauga Library – Historic Image Gallery)

²¹ Kathleen A. Hicks, *Clarkson and Its Many Corners* (Mississauga, Ontario: Mississauga Library System, 2003), XVI

²² Ibid

²³ ASI, Heritage Impact Assessment 1141 Clarkson Road North City of Mississauga, Ontario, (2017)



Figure 37: The front elevation of the Carload Groceteria ('Clarkson Store') on Clarkson Road North (Mississauga Library – Historic Image Gallery, n.d)



Figure 38: View of the former railway station (Mississauga Library – Historic Image Gallery, 1900)



Figure 39: View of the Clarkson Store and Post office prior to the 1906-1910 renovations (Mississauga Library – Historic Image Gallery, n.d)



Figure 40: Interior View of the Clarkson Store and Post Office (Mississauga Library – Historic Image Gallery, 1910)



Figure 41: View of the former railway station on the north side of the tracks (Mississauga Library – Historic Image Gallery, n.d)



Figure 42: View of Alex Durie's store (right) and the Edith (Warren Clarkson's granddaughter) Clarkson Store (left). (Mississauga Library – Historic Image Gallery, 1900)



Figure 43: Orthoimagery Aerial, 1967, Region of Peel Archives
(Annotated by AREA to show the location of the subject property and adjacent streets)

— — — Clarkson Road North

— — — Balsam Avenue

↓
NORTH

4.2 DESIGN / PHYSICAL SUMMARY

4.2.1 Builders

Clarkson Store was built by Warren Clarkson in 1835. William built his house next door to his father Warren's store on Clarkson Road, when he took over the management in the mid-1860s. In the 1950s, it was Bill Elliot's fruit market²⁴. The buildings are currently vacant.

4.2.2 Clarkson Store

The original Clarkson store built in 1835 was replaced with a simple, one-and-a-half storey, wood frame building with a front gable roof circa 1865. The exterior cladding was horizontal clapboard siding. This building was significantly altered between 1906 and 1910 when a second floor was added along with an addition to the north side transforming it into a two-and-a-half storey 'L-shaped structure' with a front gable roof and storefront. In 2015, the building was again renovated (Figure 44).



Figure 44: East (front) elevation of the Clarkson Store, left - July 2015 (Source: CHER: 1130-1140 Clarkson Road North, City of Mississauga, Ontario, October 2015, 26), right – February 2021

In 2005, a Heritage Mississauga report described the Clarkson Store as follows,

The store is 'L' shaped in plan and is 2 ½ storeys with a low pitch gable roof, Classic Revival in style. There are pediments on the east-front, west-rear and north facades. The store has been boarded up but according to an architectural assessment done on the property in 1989, the: first floor is divided into two stores, each having a central door flanked by display windows. The doors on the left stands in a recess decorated by two thin Corinthian pillars. The second storey is defined by a moulded wooden entablature over the store windows. There are three over the store windows. There are three rectangular sash windows on the second floor, and a small rectangular window in a round-headed

²⁴ Kathleen A. Hicks, *Clarkson and Its Many Corners* (Mississauga, Ontario: Mississauga Library System, 2003), 65

opening in the gable. The south façade has a door on the first floor and three sash windows on the second. At the rear are, a door on the first floor, two sash windows on the second and a small round-headed window in the gable. There is also a rectangular sash window on the north side inside the 'L'. The building is covered with a brick pattern pressed metal, exposed on the northern façade and otherwise covered with asphalt siding. The cornice is decorated throughout with patterned moulding. The wide soffits are panelled.

The pressed metal brick pattern siding was popular in the early 20th century, it is believed that the siding was applied between 1900-1920, after the building was constructed, covering 1x10-inch horizontal boards.²⁵

The Clarkson Store is located on its original site however has undergone some exterior renovations since it was built. The massing, height, scale and form of the building remains as original.

The windows and doors are presently boarded and the building is vacant.

4.2.3 William Clarkson House

The William Clarkson House is a representative example of a 19th century vernacular style dwelling of wood frame construction with Gothic Revival characteristics (Figure 45).

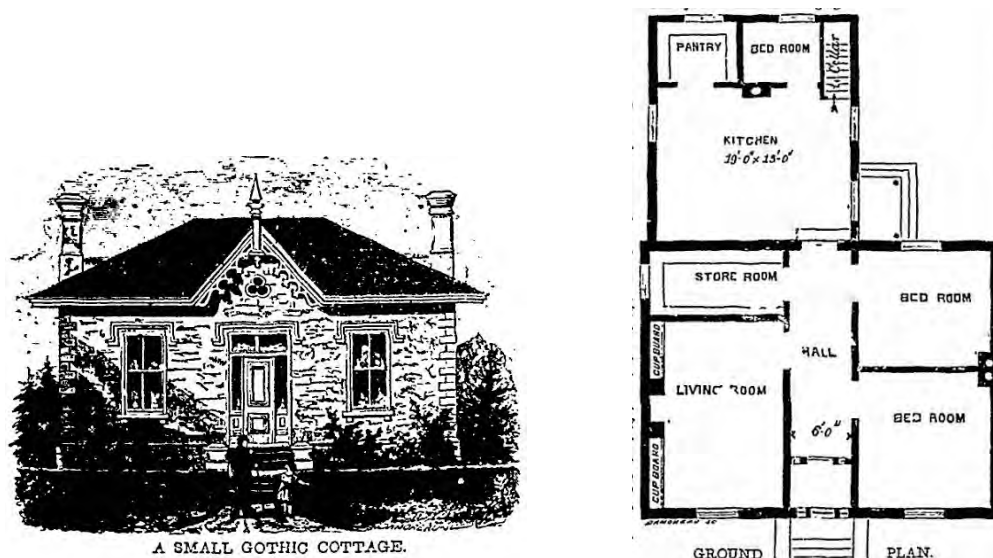


Figure 45: Selection of drawings from the 1864 The Canada Farmer column showing the suggested plan and principal elevation of the rural Ontario gothic cottage being advertised (The Canada Farmer, Volume 1, No.2, February 1, 1864).

²⁵ Heritage Mississauga. "Recommendations for Designation on the Heritage Registry, 1130-1132 and 1140 Clarkson Road North (Clarkson Store and William Clarkson House)" built 1865-1894 (July 15, 2005), 3

The house is noted by key identifiers: steeply pitched roof, one-and-a-half storey, rectangular floor plan and gables with decorative bargeboard. The gable is present in the center of the façade. The windows are symmetrically placed in composition and a verandah wraps south side. The original exterior cladding included clapboard siding. The William Clarkson house is located on its original site however has undergone some exterior renovations since it was built. The massing, height, scale and form of the building remains as original.



Figure 46: The main elevation of the William Clarkson House, early 20th century (Source: Mississauga, Historical Images Gallery, BA0154)



Figure 47: The main elevation of the William Clarkson House, 1980 (Source: Mississauga, Historical Images Gallery)



Figure 48: The main elevation of the William Clarkson House, 2004 (Source: Heritage Mississauga 2005)



Figure 49: The main elevation of the William Clarkson House, 2020 (Source: Google Maps 2021)

In 2005, Heritage Mississauga Report described the exterior of the house as follows.

The Clarkson House is a one-and-a-half storey Victorian Gothic with an 'L-shaped' floor plan. A single storey later addition was built on the rear, with a hipped roof. The original house was a cross gable roof, the front gable of the house faces Clarkson Road North. The peak has a hammer beam fretwork decoration. This is new as it is not present in the older picture. The front verandah extends beyond the south wall indicating that it may have

been a wrap around verandah, however, the side portion is gone. This may also specify the sets of bay windows on the south façade are an addition. A wide reed moulding surrounds the front door. The original exterior covering is unknown as it was covered by pressed steel siding in brick pattern, which was also present on the store, this siding was available in the 19th century but became popular in the early 20th century. The new plywood covering conceals the majority of the pressed metal sheathing.

and,

Recent renovations to the house include dark yellow plywood siding with vertical laths, resembling board and batten, as well as blue window trim. The window trim is respectful of the heritage of the home. The trim details at the top of the windows comes to a slight point in the centre. This feature looks similar to a picture of the Clarkson house in the Barnett Scrapbook. Two chimneys visible in the older photograph are no longer present from the exterior.²⁶

The windows and doors are presently boarded and the building is vacant.

4.3 SUMMARY

The subject property, located at 1130-1140 Clarkson Road North in the City of Mississauga is an important and significant cultural heritage property that has design, associative and contextual values. Unterman McPhail Associates Heritage Resource Management Consultants provided the City with a Cultural Heritage Evaluation Report for the subject property in 2015²⁷ for consideration in municipally designating the subject property under Part IV of the Ontario Heritage Act (OHA).

The above properties at 1130-1140 Clarkson Road North were designated as being of cultural heritage value or interest under the Ontario Heritage Act. The By-Law 0057-2017 was enacted and passed on 26th April 2017 (Appendix G).

²⁶ Heritage Mississauga. "Recommendations for Designation on the Heritage Registry, 1130-1132 and 1140 Clarkson Road North (Clarkson Store and William Clarkson House" built 1865-1894 (July 15, 2005),3

²⁷ Cultural Heritage Evaluation Report 1130-1140 Clarkson Road North City of Mississauga, Ontario. Unterman McPhail Associates Heritage Resource Management Consultants, 2015. Accessed from:
[http://www7.mississauga.ca/documents/agendas/committees/heritage/2016/1 - 01 12 16 - HAC Agenda.pdf](http://www7.mississauga.ca/documents/agendas/committees/heritage/2016/1_-_01_12_16_-_HAC_Agenda.pdf)

5.0 DESCRIPTION AND EXAMINATION OF PROPOSED DEVELOPMENT/SITE ALTERATIONS

5.1 PROPOSAL OVERVIEW

The proposed development by CIMA+ involves converting the existing Alectra distribution overhead lines to underground (UG) cabling for the rail crossing adjacent to the subject site to meet Metrolinx's clearance requirements. Metrolinx aims to expand the GO Transit to provide faster service on the Lakeshore West corridor with electrified lines in Mississauga.

The conversion of the overhead system to underground infrastructure is proposed within the subject site in proximity to the heritage resource, this HIA is thus required to identify and assess the impacts of the property's proposed undertaking at 1130-1140 Clarkson Road North that involves the above-described execution. The existing heritage resource, however, will not undergo any alterations. This part of the HIA provides an overview of the risks and issues posed by the existing overhead system and subject site's development proposal, and its investment benefits.

The development proposal's description is based on the permit drawings (July 2020) prepared by CIMA+ for Alectra Utilities Corporation of the electrical work to be carried out on the subject property.

5.2 PURPOSE

Appendix A007 of the Alectra Utilities 2020-24 Distribution System Plan defines and summarizes the primary and secondary drivers for Overhead Legacy Infrastructure:²⁸

TABLE 1: INVESTMENT DRIVERS

INVESTMENT DRIVER	REASONING AND INVESTMENT BENEFITS
Primary Driver: Functional Obsolescence	<p>The primary driver for these investments is functional obsolescence. Overhead legacy assets, including legacy rear-lot construction and legacy under-classed poles, conveying four feeder circuits, no longer align with Alectra Utilities' standards and practices. Rear lot infrastructure introduces accessibility and safety issues.</p> <p>These assets cannot be maintained using efficient maintenance and operational practices the leverage labour saving equipment and device such as bucket trucks.</p>
Secondary Driver: Safety	Rear Lot infrastructure poses an elevated safety risk to (i) field crews, due to the non-standard and manual procedures that must

²⁸ EB-2019-0018 Alectra Utilities Corporation 2020 EDR Application Exhibit 04 Tab 01 Schedule 01 Appendix A07 — Rear Lot Conversion, Pg. 8 of 21

	be utilized to maintain and operate this infrastructure on a regular basis, and (ii) the general public, due to the limited clearances between the electrical plant and customer properties.
Secondary Driver: Reliability	Given that like-for-like replacement would perpetuate some of the inherent shortcomings associated with rear lot supply, the proposed rear lot conversion investment will involve renewal to present-day standards which provides reliability benefits including an expected 11-fold improvement to rear-lot SAIFI and more than a 3-fold improvement to rear-lot SAIDI when compared the three-year historical system SAIFI and SAIDI.

Based on the criteria above, Alectra has determined that underground ductbank at the rear of the subject lot is their preference over the overhead electrical distribution system.

5.3 ISSUES AND RISKS POSED BY THE EXISTING OVERHEAD SYSTEM

5.3.1 OPERATIONAL ISSUES

According to the Alectra Utilities 2020-24 Distribution System Plan, Appendix A05 – Overhead Asset Renewal, operational issues on some feeders and direct buried cables were identified in the Clarkson Station area²⁹. Alectra Utilities' extensive maintenance program to manage the substation assets has allowed facilities to operate past their typical useful life. However, due to the operational issues identified, these facilities are now outdated and should be replaced with better infrastructure.



Figure 50: View of the subject property and existing overhead electric distribution system from Balsam Avenue looking southeast (February 2021)

²⁹ Alectra Utilities 2020-24 Distribution System Plan, EB-2019-0018 Alectra Utilities Corporation 2020 EDR Application Exhibit 04 Tab 01 Schedule 01 Appendix A05 — Overhead Asset Renewal, Pg. 46 of 53

5.3.2 ACCESSIBILITY ISSUES

As the infrastructure continues to age and deteriorate over time, it becomes increasingly difficult to monitor asset deterioration due to the following accessibility challenges³⁰:

- Wood poles are generally congested, due to multiple service attachments and communication drops. This makes it impossible to sufficiently climb poles. Crews must, therefore, use ladders to access these poles. Alectra Utilities is limited in utilizing ladders to access the overhead system due to Ministry of Labour restrictions for congested areas which complicates the situation further. Figures 51 & 52 show the existing wood poles.



Figure 51: View of the subject property and existing overhead electric system from Clarkson Rd. N looking northwest (February 2021)

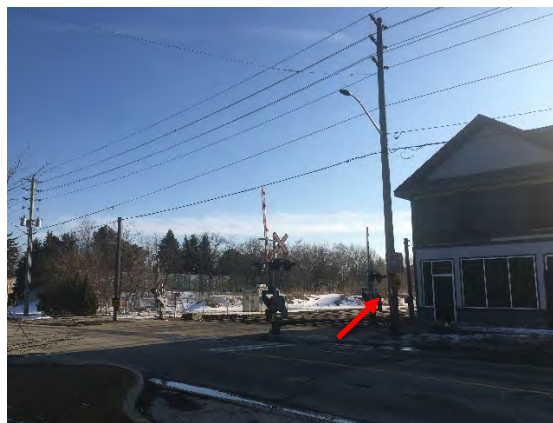
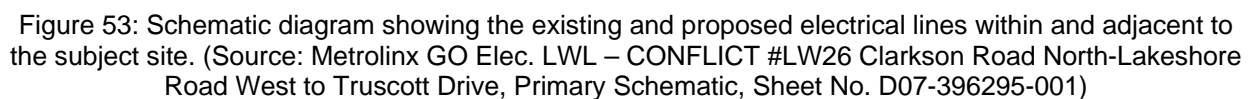


Figure 52: View of the subject property and existing overhead electric system from Clarkson Rd. N looking southwest. The hydro pole fronting the Clarkson general store is proposed to be removed, annotated by AREA in red. (February 2021)

- Configuration is generally unsafe to the public due to the large trees growing near energized power lines. In tandem with such an unsafe configuration, there are also line clearing hazards and related additional costs to do this work.

³⁰ Ibid, Pg. 2 of 21

The proposal involves the conversion of the existing overhead system to current-standard front-lot underground infrastructure, including the installation of padmounted transformers, tree retardant cross-linked polyethylene ("TRXLPE") underground cables in conduit and solid dielectric padmounted underground switches (Figure 53). All primary voltage assets will be converted accordingly to underground infrastructure. Similarly, all secondary voltage assets will also be converted such that customer meter bases will be supplied via underground connections³¹.



46

5.4.1 SCOPE OF WORK³² (Figure 54):**Poles:**

Install poles as indicated on electrical layout: 2- 55'cp and remove existing pole p11747, p11748 and pn/a.

Guying:

Install/remove guying as indicated on electrical layout. All guying to be as per ehmsd's 9-101 & 9-107. For anchors install 12" single helix pisa anchor (ehm item #10722), except where otherwise indicated.

Grounding:

Install grounding at p1, & p2 using ehmsd. 41-99, 41-103 as indicated on the drawing.

Existing overhead primary & secondary:

Ehm to transfer the existing primary & secondary to the new poles as indicated on the drawing, as per std. 13-101 on table 13-101.1 and 13-123 on table 13-123.1.

Overhead switches & primary risers:

Install underground to overhead three-phase risers to pole p1, & p2 As per alectra-ehmsd std 37-190. Alectra to supply and install new solid blade switches on poles p1, & p2 as per alectra-ehmsd std 37-98.

Underground primary cables:

Supply and install approximately 200m of 6-1000kcmil cu., tr-xlpei, cn, llcpej, 28kv primary cables (item# 16798) c/w 500kcmil rwu90. Neutral (item#16406) as shown on the drawings.

Ductbank:

Civil contractor to supply and install all 4" (100mm) pvc and hdpe ducts as required. All direct buried ducts to be constructed in accordance with ehmsd. 37-142d. Ductbank shall be subject to Alectra-ehmsd inspection prior to backfill and/or pouring concrete. Further inspections shall be completed after construction, prior to cables installation.

Supply and install all concrete encased Ductbank at road crossings, bends and at other locations as indicated on the drawings as per 10-106.1 to 10-106.7 and per individual cross-section sketches. Supply and install steel casing duct bank at railway crossing up to cn railway r.o.w. all high voltage ducts (100mm) shall have 60" radius bends, concrete encased to 1.0m past the duct bend as per 10-107c.

Street lighting:

Power services to transfer the existing street lights and relocate the luminaires to the proposed poles if applicable.

³² Metrolinx GO Elec. LWL – CONFLICT #LW26 Clarkson Road North-Lakeshore Road West to Truscott Drive, Standards and Notes, Sheet No. D07-396295-005



Figure 54: Electric Plan and the Trenching Plan of the proposed development showing the Heritage Resource fronting onto Clarkson Road North, annotated by AREA (Source: Metrolinx GO Elec. LWL – CONFLICT #LW26 Clarkson Road North-Lakeshore Road West to Truscott Drive, Electrical and Trenching, Sheet No. D07-396295-002)

The above electric and trenching plan show the location of both existing and proposed single phase transformers, power trenches, cables, hydro poles, circuits, switches etc. For further details, drawing legends and construction notes, please refer to Appendix D Electrical Drawings.



Anchors proposed to be installed on the existing pole in front of the property to provide structural stability, this will be visible above ground (Figures 54 & 55).



Figure 55: Sketch of the 2 new anchors to be installed on the existing pole in front of the property to provide structural stability, overlayed on the current conditions at 1140 Clarkson Rd N.
(Source: CIMA+)

5.5 SITE ALTERATION

The existing heritage resource will be retained and protected at its original position within the lot. The subject property will be utilized as a mixed-use building, no changes to land use or building alterations are proposed. The building will be protected and monitored before and throughout the period of construction. A concrete encased ductbank (18x100mm PVC Ducts & 2x100mm HDPE Ducts) is proposed 9.2m west of the William Clarkson house and 1.4m below existing grade level (Figure 56). This ductbank is proposed to be connected to a 20x100mm Steel casing ductbank which will be placed 4.1m below the railway tracks. A minimum clearance of 4.1m from the base of rail to top of the steel casing will be maintained as shown in Figure 56. An easement will thus be required.

Existing hydro pole (south end of the property) fronting Clarkson Road North will be removed.

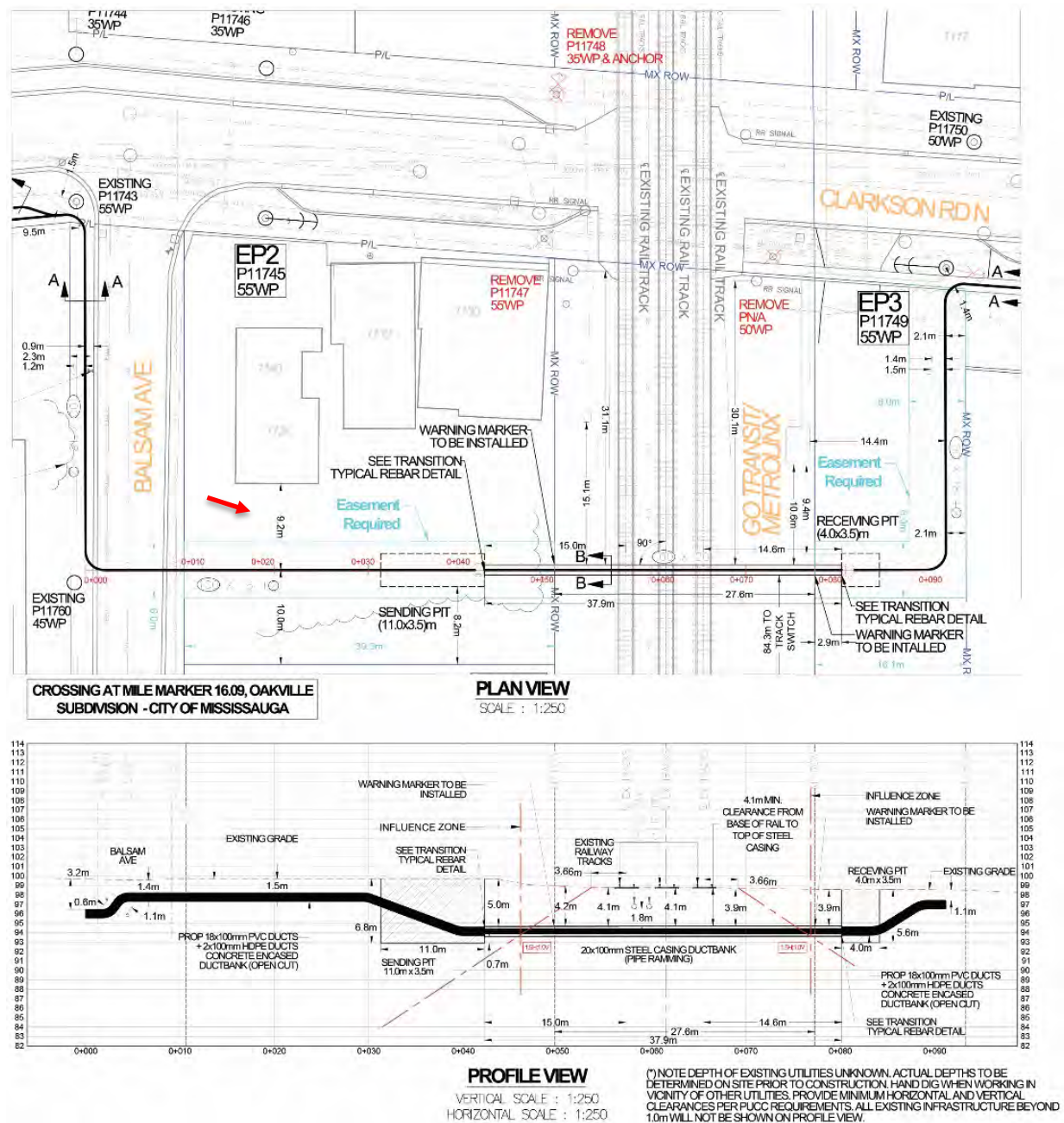


Figure 56: Profile view of the proposed ductbank within the subject site. Distance buffer of the William Clarkson house from the ductback annotated by AREA
(Source: Metrolinx GO Elec. LWL – CONFLICT #LW26 Clarkson Road North-Lakeshore Road West to Truscott Drive, Trenching 2, Sheet No. D07-396295-003)

5.6 IMPACT ASSESSMENT

Impacts can be described as ‘direct’ when cultural heritage landscapes and/or built heritage resources will be removed or significantly altered by a proposed development activity or ‘indirect’ when cultural heritage resources are disrupted by the introduction of physical, visual, audible or atmospheric elements that are not in keeping with their character and, or setting.

5.6.1 ASSESSMENT OF ADVERSE IMPACTS

When determining the effects, a development or site alteration may have on known or identified built heritage resources or cultural heritage landscapes, the MTCS Heritage Resources in the Land Use Planning Process advises that the following direct and indirect adverse impacts be considered:

A. DIRECT IMPACTS

- *Destruction* of any, or part of any, significant heritage attributes, or features; and
- *Alteration* that is not sympathetic or is incompatible, with the historic fabric and appearance.

B. INDIRECT IMPACTS

- *Shadows* created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;
- *Isolation* of a heritage attribute from its surrounding environment, context or a significant relationship; or
- *A change in land use* such as a rezoning an open space to residential use, allowing new development or site alteration to fill in the formerly open spaces.

Other potential impacts associated with the undertaking may also be considered. Historic structures, particularly those built in masonry, are susceptible to damage from vibration caused during the construction phase. Like any other structure, they are also threatened by collisions with heavy machinery or subsidence from utility line failures (Randl 2001:3-6).

The following is an assessment of the potential direct or indirect adverse impacts on the property’s heritage attributes resulting from the proposed development, based on those identified in Ontario Heritage Tool Kit, Info Sheet #5.

TABLE 2: ASSESSMENT OF DIRECT AND INDIRECT ADVERSE IMPACTS

Potential direct and indirect adverse impact	Assessment	Summary of Impact with Mitigation
1. <i>Destruction</i> of any, or part of any	The proposed development includes the retention and protection of the property’s-	Minor Impact. If controls are followed during construction,

significant heritage attributes of features.	built heritage resource in its entirety. No alterations are proposed for the CHR.	impact will be limited and monitored. No heritage attributes are at risk if the recommended mitigation measures are implemented (see section 6).
2. Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance.	<p>On the east (front) elevation, an existing hydro pole will be removed.</p> <p>A concrete encased ductbank (18x100mm PVC Ducts & 2x100mm HDPE Ducts) is proposed 9.2m west of the William Clarkson house and 1.4m below existing grade level (Figure 56)</p> <p>The proposed development has the potential to negatively impact heritage attributes during the construction phase through accident or faulty procedure. Construction activity and potential excavation adjacent to the property has potential to cause limited and temporary impacts to the CHR.</p> <p>These impacts can be mitigated through construction controls, protection plans and retention of a structural engineer to avoid any damage to the property's heritage attributes.</p>	<p>Minor Impact.</p> <p>If controls are followed during construction, impact will be limited and monitored. No heritage attributes are at risk if the recommended mitigation measures are implemented (see section 6).</p> <p>Positive impact will be the removal of the existing wood hydro pole which obscured view from and to the east facade of the original former Clarkson General Store.</p>
3. Shadows created that alter the appearance of the heritage attribute or change the viability of an associated natural feature or plantings, such as a garden.	The proposed development will not result in shadows on the CHR.	No impact.

<p>4. Isolation of a heritage attribute from its surrounding environment, context or a significant relationship.</p>	<p>The Clarkson General Store and the William Clarkson House will be retained at their original location dating to c. 1835. The buildings will continue to front on Clarkson Road North and will remain within its original lot.</p> <p>The rear underground cabling will not block the existing structure from surrounding properties or views.</p>	<p>No impact.</p> <p>Retention is within the original site and will maintain visual relationship with Clarkson Road North & Balsam Avenue, and potentially draw new interest and appreciation for the former Clarkson Corners and the history of the Township of Clarkson.</p>
<p>5. Direct or indirect obstruction of significant views or vistas with, from, or of built and natural features.</p>	<p>N/A - No significant views or vistas have been identified within, to, or from the Subject Property.</p> <p>2 new anchors to be installed on the existing pole in front of the property to provide structural stability, overlayed on the current conditions at 1140 Clarkson Rd N (Figure 55).</p>	<p>No impact.</p> <p>The impact of the proposed development will be minimal, as the addition is located underground, to the rear of the property and the existing heritage structure will be retained.</p> <p>Visual impact by the 2 new anchors will be minimal and reversible. The cables will be thin and will have a yellow protector to protect pedestrians (Figure 55).</p> <p>Positive impact will be the removal of the existing wood hydro pole which obscured view from and to the east facade of the original former Clarkson General Store.</p>

<p>6. A change in land use (such as rezoning a church to a multi-unity residence) where the change in use affects the property's cultural heritage value.</p>	<p>A change in land use is not proposed for the Subject Property.</p> <p>The mixed-use character of the area will be preserved. The subject property will be preserved and will be utilized as a mixed-use building associated for the owners of the building which will provide stewardship of the CHR in the long-term.</p>	<p>No Impact.</p>
<p>7. Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect a cultural heritage resource, including archeological resources.</p>	<p>Construction of underground ductbanks.</p> <p>Land disturbances during construction phase can be monitored if mitigation measures such as standard drainage, site grading and vibration monitoring are implemented. There are no anticipated changes in grade that would negatively impact the building.</p>	<p>No impact.</p> <p>If mitigation measures are followed during construction, impact will be limited and monitored.</p>

5.6.2 Results of Impact Assessment

The preceding assessment has determined that with the proposed conservation strategy, the proposed development will not result in significant direct and indirect impacts to the heritage attributes of the CHR. Such impact will be limited and monitored through proper mitigation measures. The following section provides an analysis of potential alternative mitigation strategies.

5.7 CONSIDERED ALTERNATIVES AND MITIGATION STRATEGIES

Various mitigation options are evaluated in this section, to determine how the proposed development can lessen its impacts on the subject property. This proposed conservation strategy implements "mitigative measures and/or alternative development approaches" supported by the PPS. This PPS provision is incorporated municipally through the Official Plan, which states that, (Section 7.4.1.13) 'Cultural heritage resources must be maintained in situ and in a manner that prevents deterioration and protects the heritage qualities of the resource.'³³

The proposed development proposes the in-situ preservation and protection of the CHR. This HIA thus evaluates the following mitigation options while maintaining the CHR in-situ in the following order of priority:

³³ Heritage Planning, Cultural Heritage Resources, Mississauga Official Plan (Office Consolidation September 2020). Retrieved from: <http://www6.mississauga.ca/online/maps/planbldg/MOP/Chapter7-CompleteCommunities-July13-2016.pdf>

- (i) On-site retention of the CHR in the original use and maintaining the existing overhead system as is;
- (ii) On-site retention of the CHR in the original use and replacing the existing overhead system with Rear Lot overhead system; and,
- (iii) On-site retention of the CHR in the original use and replacing the existing overhead system with full underground infrastructure.

The following table provides analysis of each mitigation option:

TABLE 3: ANALYSIS OF OPTIONS FOR CONSIDERED ALTERNATIVES

OPTION	ADVANTAGES	DISADVANTAGES	FEASIBILITY
(i) On-site retention of the CHR in the original use and maintaining the existing overhead system as is	<ul style="list-style-type: none"> - Minimal intervention and no investment. - Potential for retaining all heritage attributes of the property. - Assets will only be replaced when they are obsolete. 	<ul style="list-style-type: none"> - Customers will be exposed to prolonged reliability impacts due to the operational and accessibility issues associated with the lot. - Wood poles will have to be replaced manually with field crews accessing private properties in order to execute the work. - Both crew and customers will be exposed to safety risks due to minimal proximity between the customer and overhead lines as well as the non-standard and non-ergonomic work procedures that field crews would have to continue to execute to sufficiently maintain, inspect and service the assets.³⁴ 	Alectra Utilities rejected this approach due to inherent issues with maintenance, reliability, safety concerns and resulting operational inefficiencies. ³⁵
(ii) On-site retention of the CHR in the original use and	- All heritage attributes retained.	- The rear lot overhead infrastructure would remain on private property.	This option is not feasible because it would perpetuate

³⁴ Ibid.

³⁵ Alectra Utilities 2020-24 Distribution System Plan, EB-2019-0018 Alectra Utilities Corporation 2020 EDR Application Exhibit 04 Tab 01 Schedule 01 Appendix A07 — Rear Lot Conversion, Pg. 18 of 21

replacing the existing overhead system with Rear Lot overhead system;		<ul style="list-style-type: none"> - The existing infrastructure would have to be reconfigured to make it accessible from the street in case the crew wants to access the plant during an outage event. - This approach would require easements, access private property. 	operating constraints and safety concerns. Alectra Utilities rejected this approach.
(iii) On-site retention of the CHR in the original use and replacing the existing overhead system with full underground infrastructure.	<ul style="list-style-type: none"> - All heritage attributes retained. - The Clarkson House will maintain a visual relationship to Clarkson Road North. - Through preservation, the heritage building can be conserved. - This scenario proposes the complete replacement of the existing overhead system with underground infrastructure that can be accessed from Clarkson Road North. - The new assets will be installed as per the current design practices. - This proposal also ensures efficiencies for the utilities as activities like tree cutting/trimming can be eliminated. 	<ul style="list-style-type: none"> - Stabilization and monitoring measures will be required. If controls are followed during construction, impact will be limited and monitored. No heritage attributes are at risk if the recommended mitigation measures are implemented (see section 6). 	<p>This option is feasible as this approach will completely mitigate the reliability and safety issues associated with overhead distribution as well as the operational constraints associated with the existing infrastructure.</p> <ul style="list-style-type: none"> -The subject property has sufficient lands to incorporate the proposed construction. -The CHR is in overall good condition with high integrity; and - It will assist in meeting the objectives for Protection of Cultural Heritage Resources as identified in the City of Mississauga's <i>Official Plan</i> under section 7.4 Heritage Planning.

Based on the above evaluation, the on-site retention of the CHR in the original use, on its original location and replacing the existing overhead system with full underground infrastructure is the most feasible and most appropriate form of conservation strategy.

6.0 CONSERVATION AND IMPLEMENTATION STRATEGIES

6.1 PRESERVATION APPROACH

Based on the evaluation from the previous section, preservation of the Clarkson House and General store is the most appropriate form of conservation strategy among those listed in the Official Plan. Preservation involves protecting, maintaining and stabilizing the existing form, material and integrity of a historic place, or of an individual component, while protecting its heritage value. There are nine Standards relating to Preservation, and they must all be applied to a Preservation project. Since protection, maintenance and stabilization are at the core of all conservation projects, all nine Preservation Standards must be applied to any conservation project.³⁶ In conclusion, the proposed preservation of the Clarkson House & General Store will accommodate a balance between conservation and development, and is therefore recommended by this HIA. This approach will allow the recovery of the building's historic Gothic Revival form, while also permitting viability for its integration within the proposed development. The process of preservation can be achieved through the processes described in the following sub-sections.

6.1.1 Planning for Stabilization and Monitoring

The CHR should be stabilized as part of a mothballing plan that involves correcting deficiencies to slow down the deterioration of the building while it is vacant. The building's windows and doors are currently boarded; however, it is essential to provide adequate air exchange throughout the building to prevent mould formulation, rot and insect infestation. A structural engineer should visit the site and determine if structural stabilization measures will also be required to ensure that the vibration caused by the underground digging to lay the ductbank will not harm the building or its finishes. Once the property is stabilized, a regular schedule for surveillance, maintenance and monitoring should be established. The local fire and police departments should be notified that the buildings are vacant. For details related to pre-construction, construction and post construction monitoring and contingency measures, please refer to Appendix D - drawing Metrolinx GO Elec. Sheet No. D07-396295-007.

6.1.2 Recommendations and Approvals

To undertake the proposed construction work, a Heritage Permit Application (HPA) is typically submitted to the City's Heritage Planning, Culture Division. Typically, a submission for HPA would be required, however in this case it is recommended and requested of Heritage Planning staff that this HIA be approved and represent the only required submittal to allow for the underground ductbank construction. It is commonplace for municipalities to delegate authority to Heritage Planning staff to approve such infrastructure related projects which do not impact the CHR. It is thus requested that the heritage approval be delegated to Heritage Planning staff in order to expedite the approval of this infrastructure project. This heritage staff approval therefore would entail communicating with the Building Division to provide clearance and sign off of the building permit for the ductbank work.

³⁶ Page1,2, Standards for conservation — Applying the Standards, Standards and Guidelines for the Conservation of Historic Places in Canada, 2010

7.0 SUMMARY STATEMENT & RECOMMENDATIONS

Based on the results of archival research, site investigation and existing conditions and an assessment of impacts of the proposed undertaking, this HIA concludes that the proposed adjacent development will have no impact on the cultural heritage value of the heritage property. The site is subject to the City's Property Standards By-law.

To ensure the long-term sustainability and use of the Clarkson House and General Store as a valued built heritage resource, *AREA* recommends to:

- preserve and protect the Clarkson House and General Store at its original location within the property in the proposed development. This operation will require the following short-term actions:

Short-term Conservation Actions:

- Ensure that the vacant heritage resource is protected in accordance with the City of Mississauga *Property Standards By-Law 654-98 Section 14.2 BOARDED BUILDINGS ON HERITAGE PROPERTIES (368-06)* and to the satisfaction of the City of Mississauga.

8.0 APPENDICES

Appendix A: Bibliography & References

Appendix B: Site Photos

Appendix C: City of Mississauga's Cultural Heritage Impact Assessment, Terms of Reference ("HIA-ToR") 2017

Appendix D: Metrolinx Go Electrical Lwl. – Conflict #LW26, Clarkson Rd. N - Lakeshore Rd. W to Truscott Drive, Alectra Utilities Corporation, ISSUED JULY 2020

Appendix E: Clarkson family file, William Perkins Bull fonds, Region of Peel Archives

Appendix F: Archival Photos and Historical Plan

Appendix G: The Corporation of the City of Mississauga By-Law 0057-2017

Appendix H: Resume

APPENDIX A: BIBLIOGRAPHY & REFERENCES

BIBLIOGRAPHY

Alectra Utilities Corporation

2019 Alectra Utilities 2020-24 Distribution System Plan, Exhibit 4, Tab 1, Schedule 1.

Accessed from:

https://alectrautilities.com/sites/default/files/assets/pdf/AlectraUtilities_APPL_Ex-4-Apx-A_PUBLIC-VERSION_-v.-2-of-7.pdf

CIMA+

Alectra Utilities Corporation, Geotechnical Investigation Area 5-Lakeshore West Corridor
Metrolinx Utility Crossing, October 9, 2020

Hicks, Kathleen A

2003 *Clarkson and its Many Corners*. Mississauga: Mississauga Library System. Accessed from: http://www5.mississauga.ca/library/EBooks/8147_ClarksonBook.pdf

Mississaugas of the Credit First Nation. *Ajetance Treaty No. 19*. <http://mncfn.ca/treaty19/>

Peel Art Gallery, Museum and Archives (PAMA). *The History of Peel Region, Ontario, Canada*.

<https://peelarchivesblog.com/about-peel/>

The Corporation of the City of Mississauga Property Standards “By-law 654-98.” (Amended by 357-00, 162-03, 89-04, 76-06, 368-06, 348-07, 211-08). Accessed from:

<http://184.150.237.247/file/COM/propertystandardsheritage.pdf>

References

ASI

2017 Heritage Impact Assessment 1141 Clarkson Road North City of Mississauga, Ontario

Accessed from:

https://www7.mississauga.ca/documents/committees/heritage/2018/2018_06_05_HAC_Agenda.pdf

Beers, J.H

1877 *Illustrated Historical Atlas of the County of Ontario, Ont.* Toronto: J.H. Beers & Co.

Canada's Historic Places

2010 *Standards and Guidelines for the Conservation of Historic Places in Canada*. Second Addition. Canada's Historic Places, Ottawa

CIMA+

Metrolinx Electrical Utility Crossings. Accessed from: <https://www.cima.ca/en/project/metrolinx-electrical-utility-crossings/>

City of Mississauga

2021 Planning Information Hub. Accessed from: <https://mississauga.maps.arcgis.com/apps/MapSeries/index.html?appid=2103d1a57cf044a3ab801950bc174686&entry=3>

2021 *Property Information Report (2021). Electronic Document.*
<https://ext.maps.mississauga.ca/Geocortex/Essentials/REST/TempFiles/Property%20Report.pdf?guid=c62ba40a-bf35-42bf-b8f6-12cc35ec9097&contentType=application%2Fpdf>

2020 *Official Plan 2017 (Office Consolidation September 2020). Electronic Document:*
<https://www.mississauga.ca/projects-and-strategies/strategies-and-plans/mississauga-official-plan/>

2020 Official Plan Schedule 10 Land Use Designations. Electronic Resource:
http://www6.mississauga.ca/onlinemaps/planbldg/MOP/Schedules/LandUse10_MOP_Appeals_V30.006.pdf

2020 Official Plan Schedule 9. Character Areas. Electronic Resource:
http://www6.mississauga.ca/onlinemaps/planbldg/MOP/Schedules/Sched_9_CharacterAreas.pdf

2018 Heritage Registrar for Mississauga. Accessed from: https://www.mississauga.ca/wp-content/uploads/2020/06/10135918/2018-07-01_Mississauga_Heritage_Register_Web.pdf

2017 *Terms of Reference Heritage Impact Assessment. Electronic Document:*
https://www7.mississauga.ca/documents/culture/heritage/HeritageImpactAssessment_TermsOfReference2017.pdf

2007 Zoning By-Law 0225-2007 - D ZONE (DEVELOPMENT). Accessed from: <https://www.mississauga.ca/apps/zoningbylaw/#/show/12,3248>

Government of Canada

Library and Archives Canada

Clarkson, Record Information. Accessed from: <https://www.bac-lac.gc.ca/eng/CollectionSearch/Pages/record.aspx?app=posoffposmas&IdNumber=15501&new=-8585880098273607412>

Census of 1861

Digitized page of Census of 1861 (Canada East, Canada West, Prince Edward Island, New Brunswick and Nova Scotia) for Image No.: 4391950_00438. Accessed from:

https://central.bac-lac.gc.ca/.item/?app=Census1861&op=&img&id=4391950_00438

Government of Ontario

2020 *Provincial Planning Statement*. Electronic Document: <https://files.ontario.ca/mmah-provincial-policy-statement-2020-accessible-final-en-2020-02-14.pdf>

2018 Region of Peel Official Plan. Electronic Document: https://www.peelregion.ca/planning/officialplan/pdfs/ropdec18/ROPConsolidationDec2018_TextSchedules_Final_TEXT.pdf

1990 *Ontario Heritage Act*. Electronic Document: <https://www.ontario.ca/laws/statute/90o18?search=heritage+act>

Heritage Mississauga

2019 Clarkson Tour Brochure. Accessed from: <https://heritagemississauga.com/wp-content/uploads/2020/04/Clarkson-Tour-Brochure-August-2019.pdf>

2009 Clarkson. Accessed from: <http://www.heritagemississauga.com/page/Clarkson>

Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI)

2015 *Criteria for Evaluating Potential for Built Heritage Resources and Cultural Landscapes: A Checklist for the Non-Specialist*. Ministry of Tourism, Culture and Sport, Toronto.

2014 *Standards and Guidelines for the Conservation of Provincial Heritage Properties – Heritage Identification and Evaluation Process*. Ministry of Tourism, Culture and Sport, Toronto.

2007 *Infosheet: Eight Guiding Principles in the Conservation of Built Heritage Properties*. Ministry of Tourism, Culture and Sport, Toronto.

2006 *Ontario Heritage Tool Kit: Heritage Property Evaluation – A Guide to Listing, Researching, and Evaluating Cultural Heritage Property in Ontario Communities*. Ministry of Tourism, Culture and Sport, Toronto.

Ontario Heritage Tool Kit: Designating Heritage Properties: A Guide to Municipal Designation of Individual Properties under the Ontario Heritage Act. Ministry of Tourism, Culture and Sport, Toronto.

Mississauga Library

Clarkson, Historic Image Gallery. Accessed from: <https://www.mississauga.ca/apps/mediagallery/#/agency/Library>

Ontario Architecture

Gothic Revival (1750-1900). Accessed from:

<http://www.ontarioarchitecture.com/gothicrevival.html>

Ontario Land Property Records Portal

Concession 2 SDS, Lot 28 Retrieved from: Ontario Land Registry Access – Historical Books

<https://www.onland.ca/ui/43/books/42302/viewer/706285865?page=787>

Peel Art Gallery Museum Archives (PAMA)

Clarkson family file, William Perkins Bull fonds, Region of Peel Archives

Orthoimagery Aerial, 1967, Region of Peel Archives

Technical Preservation Services

Preservation Briefs, 31 Mothballing Historic Buildings. Accessed from:

<https://www.nps.gov/tps/how-to-preserve/briefs/31-mothballing.htm#stable>

The Canadian County Atlas Digital Project

Map of Toronto South Township, Peel County, II SDS, Lot 28. Accessed from:

<https://digital.library.mcgill.ca/CountyAtlas/showmap.php?Township=Toronto+South&plotX=2500&plotY=3346&getMap.x=82&getMap.y=22>

Tremaine, George C.

1860 *Tremaine's Map of Ontario County, Upper Canada*. Toronto, Ontario

Underwriter's Survey Bureau

1936 *Clarkson Ont. Township of Toronto, County of Peel: Fire Insurance Plan dated April 1936*.

Unterman McPhail Associates Heritage Resource Management Consultants

2015 Cultural Heritage Evaluation Report: 1130 -1140 Clarkson Road North, City of Mississauga, Ontario. Accessed from:

http://www7.mississauga.ca/documents/agendas/committees/heritage/2016/1_-_01_12_16_-_HAC_Agenda.pdf

APPENDIX B: SITE PHOTOS (FEBRUARY 2021)



IMG_4601.JPG



IMG_4602.JPG



IMG_4603.JPG



IMG_4604.JPG



IMG_4605.JPG



IMG_4606.JPG



IMG_4607.JPG



IMG_4608.JPG



IMG_4609.JPG



IMG_4610.JPG



IMG_4611.JPG



IMG_4612.JPG



IMG_4613.JPG



IMG_4614.JPG



IMG_4615.JPG



IMG_4616.JPG



IMG_4617.JPG



IMG_4618.JPG



IMG_4619.JPG



IMG_4620.JPG



IMG_4621.JPG



IMG_4622.JPG



IMG_4623.JPG



IMG_4624.JPG



IMG_4625.JPG



IMG_4626.JPG



IMG_4627.JPG



IMG_4628.JPG



IMG_4629.JPG



IMG_4630.JPG



IMG_4631.JPG



IMG_4632.JPG



IMG_4633.JPG



IMG_4634.JPG



IMG_4635.JPG



IMG_4636.JPG



IMG_4637.JPG



IMG_4638.JPG



IMG_4639.JPG



IMG_4640.JPG



IMG_4641.JPG



IMG_4642.JPG



IMG_4643.JPG



IMG_4644.JPG



IMG_4645.JPG



IMG_4646.JPG



IMG_4647.JPG



IMG_4648.JPG



IMG_4649.JPG



IMG_4650.JPG



IMG_4651.JPG



IMG_4652.JPG



IMG_4653.JPG



IMG_4654.JPG



IMG_4655.JPG



IMG_4656.JPG



IMG_4657.JPG



IMG_4658.JPG



IMG_4659.JPG



IMG_4660.JPG



IMG_4661.JPG



IMG_4662.JPG



IMG_4663.JPG



IMG_4664.JPG



IMG_4665.JPG



IMG_4666.JPG



IMG_4667.JPG



IMG_4581.JPG



IMG_4582.JPG



IMG_4583.JPG



IMG_4584.JPG



IMG_4585.JPG



IMG_4586.JPG



IMG_4587.JPG



IMG_4588.JPG



IMG_4589.JPG



IMG_4590.JPG



IMG_4591.JPG



IMG_4592.JPG



IMG_4593.JPG



IMG_4594.JPG



IMG_4595.JPG



IMG_4596.JPG



IMG_4597.JPG



IMG_4598.JPG



IMG_4599.JPG



IMG_4600.JPG

APPENDIX C: CITY OF MISSISSAUGA'S CULTURAL HERITAGE IMPACT ASSESSMENT, TERMS OF REFERENCE ("HIA-TOR")

Culture Division
Community Services Department
City of Mississauga
201 City Centre Dr, Suite 202
MISSISSAUGA, ON L5B 2T4
www.mississauga.ca



CITY OF MISSISSAUGA HERITAGE IMPACT ASSESSMENT

TERMS OF REFERENCE

1. Background: The Mississauga Official Plan

The City's Official Plan introduces cultural heritage resources in the following manner:

Mississauga's cultural heritage resources reflect the social, cultural and ethnic heritage of the city and, as such, are imperative to conserve and protect. Cultural heritage resources are structures, sites, environments, artifacts and traditions that are of cultural, historical, architectural, or archaeological value, significance or interest.

In compliance with the City's policy 7.4.1.12, as stated below, the City of Mississauga seeks to conserve, record, and protect its heritage resources:

*7.4.1.12: The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a **Heritage Impact Assessment**, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.*

A Heritage Impact Assessment is a study to determine the impacts to known and potential heritage resources within a defined area proposed for future development. The study would include an inventory of all heritage resources within the planning application area. The study results in a report which identifies all known heritage resources, an evaluation of the significance of the resources, and makes recommendations toward mitigation measures that would minimize negative impacts to those resources. A Heritage Impact Assessment may be required on a Designated or individually Listed property on the City's Heritage Register or where development is proposed adjacent to a known heritage resource. The requirement may also apply to unknown or recorded heritage resources which are discovered during the development application stage or construction.¹

¹ For the definition of "development," please refer to the Mississauga Official Plan.

Mississauga Heritage Impact Assessment Terms of Reference, June 2017

The City's Heritage Register includes properties that comprise cultural landscapes. Cultural landscapes include neighbourhoods, roadways and waterways. Individual properties within these landscapes may or may not have cultural heritage value independent of the landscape. Heritage Impact Assessments are required to ascertain the property's cultural heritage value and to ensure that any development maintains the cultural landscape criteria, available at http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf

To determine the specific heritage status of a particular property visit <http://www.mississauga.ca/portal/services/property>. Submit the desired address and click on the "Heritage" tab. Further information is available by clicking the underlined "INV#." This last tab explains the reason why the property is listed or designated.

2. The following minimum requirements will be requested in a Heritage Impact Assessment:

- 2.1 A detailed site history to include a listing of owners from the Land Registry Office, and a history of the site use(s). However, please note that due to the Freedom of Information and Protection of Privacy Act, current property owner information must not be included. As such, Heritage Planning will request that current property owner personal information be redacted to ensure the reports comply with the Act.
- 2.2 A complete listing and full written description of all existing structures, natural or man-made, on the property. Specific mention must be made of all the heritage resources on the subject property which include, but are not limited to: structures, buildings, building elements (like fences and gates), building materials, architectural and interior finishes, natural heritage elements, landscaping, and archaeological resources. The description will also include a chronological history of the structure(s) developments, such as additions, removals, conversions, alterations etc.

The report will include a clear statement of the conclusions regarding the significance and heritage attributes of the cultural heritage resource.

A location map must be provided, with indications of existing land use, zoning, as well as the zoning and land use of adjacent properties.

- 2.3 Documentation of the existing conditions related to the heritage resource will include:
 - Current legible internal photographs, external photographs from each elevation. Please note that due to the Freedom of Information and Protection of Privacy Act, photographs should not contain people or highlight personal possessions. The purpose of the photographs is to capture architectural features and building materials.
 - Measured drawings, including elevations, floor plans, and a site plan or survey, at an appropriate scale for the given application, indicating the context in which the heritage resource is situated
 - Historical photos, drawings, or other archival material that may be available or relevant

2 of 6

Mississauga Heritage Impact Assessment Terms of Reference, June 2017

The applicant must provide a description of all relevant municipal or agency requirements which will be applied to the subject property, and when implemented may supplement, supersede and/or affect the conservation of heritage resources (i.e. Building Code requirements, Zoning requirements, Transportation and Works requirements.)

- 2.4 An outline of the proposed development, its context and how it will impact the heritage resource and neighbouring properties will be provided. This may include such issues as the pattern of lots, roadways, setbacks, massing, relationship to natural and built heritage features, recommended building materials, etc. The outline should address the influence of the development on the setting, character and use of lands on the subject property and adjacent lands. If the property forms part of a Heritage Conservation District, the proposal must be analysed in terms of its compliance with the Heritage Conservation District Plan.

Note: An architectural drawing indicating the subject property streetscape with properties to either side of the subject lands must be provided. The purpose of this drawing is to provide a schematic view of how the new construction is oriented and integrates with the adjacent properties from a streetscape perspective. The drawing must therefore show, within the limits of defined property lines, an outline of the building mass of the subject property and the existing neighbouring properties, along with significant trees or any other landscape or landform features. A composite photograph may accomplish the same purpose with a schematic of the proposed building drawn in.

- 2.5 Full architectural drawings, by a licensed architect or accredited architectural designer, showing all four elevations of the proposed development must be included for major alterations and new construction.
- 2.6 An assessment of alternative development options and mitigation measures that should be considered in order to avoid or limit the negative impact on the cultural heritage resources. Methods of minimizing or avoiding negative impact on a cultural heritage resource as stated in the Ontario Heritage Tool Kit (InfoSheet #5, Ministry of Culture) include, but are not limited to:
- Alternative development approaches
 - Isolating development and site alteration from the significant built and natural heritage features and vistas
 - Design guidelines that harmonize mass, setback, setting and materials
 - Limiting height and density
 - Allowing only compatible infill and additions
 - Reversible alterations

These alternate forms of development options presented in the Heritage Impact Assessment must be evaluated and assessed by the heritage consultant writing the report as to the best option to proceed with and the reasons why that particular option has been chosen.

- 2.7 A summary of conservation principles and how they will be used must be included. The conservation principles may be found in publications such as: Parks Canada – *Standards*

3 of 6

Mississauga Heritage Impact Assessment Terms of Reference, June 2017

and Guidelines for the Conservation of Historic Places in Canada; Eight Guiding Principles in the Conservation of Historic Properties, Ontario Ministry of Culture. (Both publications are available online.)

- 2.8 Proposed demolition/alterations must be explained as to the loss of cultural heritage value interests in the site and the impact on the streetscape and sense of place.
- 2.9 When a property cannot be conserved, alternatives will be considered for salvage mitigation. Only when other options can be demonstrated not to be viable will options such as relocation, ruinification, or symbolic conservation be considered.

Relocation of a heritage resource may indicate a move within or beyond the subject property. The appropriate context of the resource must be considered in relocation. Ruinification allows for the exterior only of a structure to be maintained on a site. Symbolic conservation refers to the recovery of unique heritage resources and incorporating those components into new development, or using a symbolic design method to depict a theme or remembrance of the past.

All recommendations shall be as specific as possible indicating the exact location of the preferred option, site plan, building elevations, materials, landscaping, and any impact on neighbouring properties, if relevant.

3. Summary Statement and Conservation Recommendations

The summary should provide a full description of:

- The significance and heritage attributes of the cultural heritage resource, including the reference to a listing on the Heritage Register, or designation by-law if it is applicable
- The identification of any impact that the proposed development will have on the cultural heritage resource
- An explanation of what conservation or mitigative measures, or alternative development, or site alteration approaches are recommended
- Clarification as to why conservation or mitigative measures, or alternative development or site alteration approaches are not appropriate

4. Mandatory Recommendation

The consultant must write a recommendation as to whether the subject property is worthy of heritage designation in accordance with the heritage designation criteria per Regulation 9/06, *Ontario Heritage Act*. Should the consultant not support heritage designation then it must be clearly stated as to why the subject property does not meet the criteria as stated in Regulation 9/06.

The following questions **must** be answered in the final recommendation of the report:

- Does the property meet the criteria for heritage designation under the Ontario Regulation 9/06, *Ontario Heritage Act*?
- If the subject property does not meet the criteria for heritage designation then it

4 of 6

Mississauga Heritage Impact Assessment Terms of Reference, June 2017

- must be clearly stated as to why it does not
- Regardless of the failure to meet criteria for heritage designation, does the property warrant conservation as per the definition in the Provincial Policy Statement:

Conserved: means the identification, protection, use and/or management of cultural heritage and archaeological resources in such a way that their heritage values, attributes and integrity are retained. This may be addressed through a conservation plan or heritage impact assessment.

Please note that failure to provide a clear recommendation as per the significance and direction of the identified cultural heritage resource will result in the rejection of the Heritage Impact Assessment.

5. Qualifications

The qualifications and background of the person completing the Heritage Impact Assessment will be included in the report. The author must be a qualified heritage consultant by having Professional standing with the Canadian Association of Heritage Professionals (CAHP) and/or clearly demonstrate, through a Curriculum Vitae, his/her experience in writing such Assessments or experience in the conservation of heritage places. The Assessment will also include a reference for any literature cited, and a list of people contacted during the study and referenced in the report.

6. Approval Process

Two hard copies of the Heritage Impact Assessment, along with a PDF version, will be provided to the Heritage Planning unit. Hard copies must be no larger than 11 x 17 inches. Staff will ensure that copies are distributed to the Planning and Building Department and relevant staff and stakeholders within the Corporation. The Heritage Impact Assessment will be reviewed by City staff to determine whether all requirements have been met and, if relevant, to evaluate the recommendations presented by the Heritage Consultant on the alternative development options. The applicant will be notified of Staff's comments and acceptance, or rejection of the report. The Heritage Impact Assessment may be subject to a peer review by a qualified heritage consultant at the owner's expense.

All Heritage Impact Assessments will be sent to the City's Heritage Advisory Committee for information or review. Reports will be published online.

An accepted Heritage Impact Assessment will become part of the further processing of a development application under the direction of the Planning and Building Department. The recommendations within the final approved version of the Heritage Impact Assessment will be incorporated into development related legal agreements between the City and the proponent at the discretion of the municipality.

7. References

5 of 6

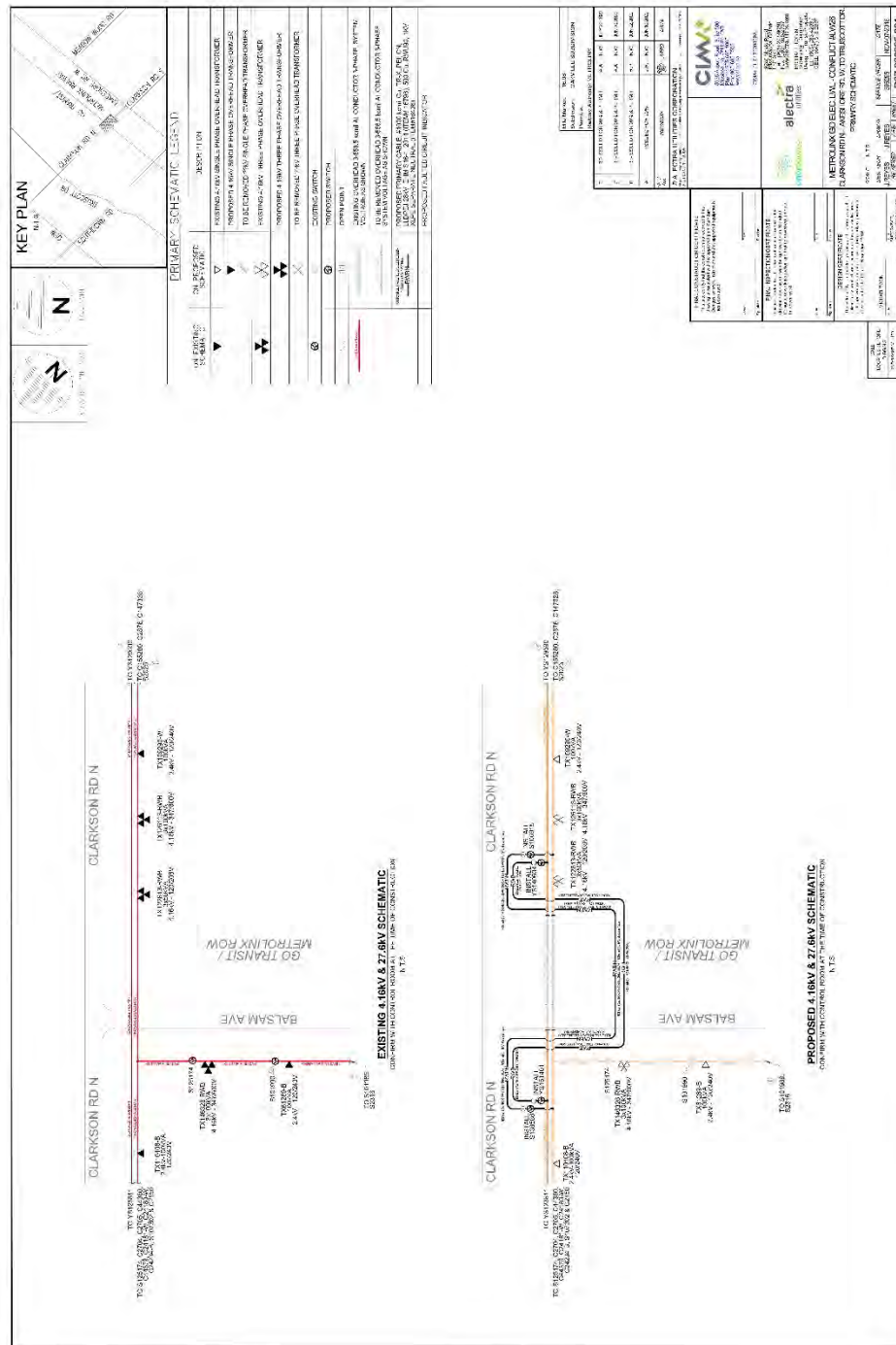
Mississauga Heritage Impact Assessment Terms of Reference, June 2017

Applicants looking for professional assistance may wish to refer to the Canadian Association of Heritage Professionals. website: <http://www.cahp-acecp.ca/>

For more information on Heritage Planning at the City of Mississauga, visit us online at [http:// www.mississauga.ca/heritageplanning](http://www.mississauga.ca/heritageplanning)

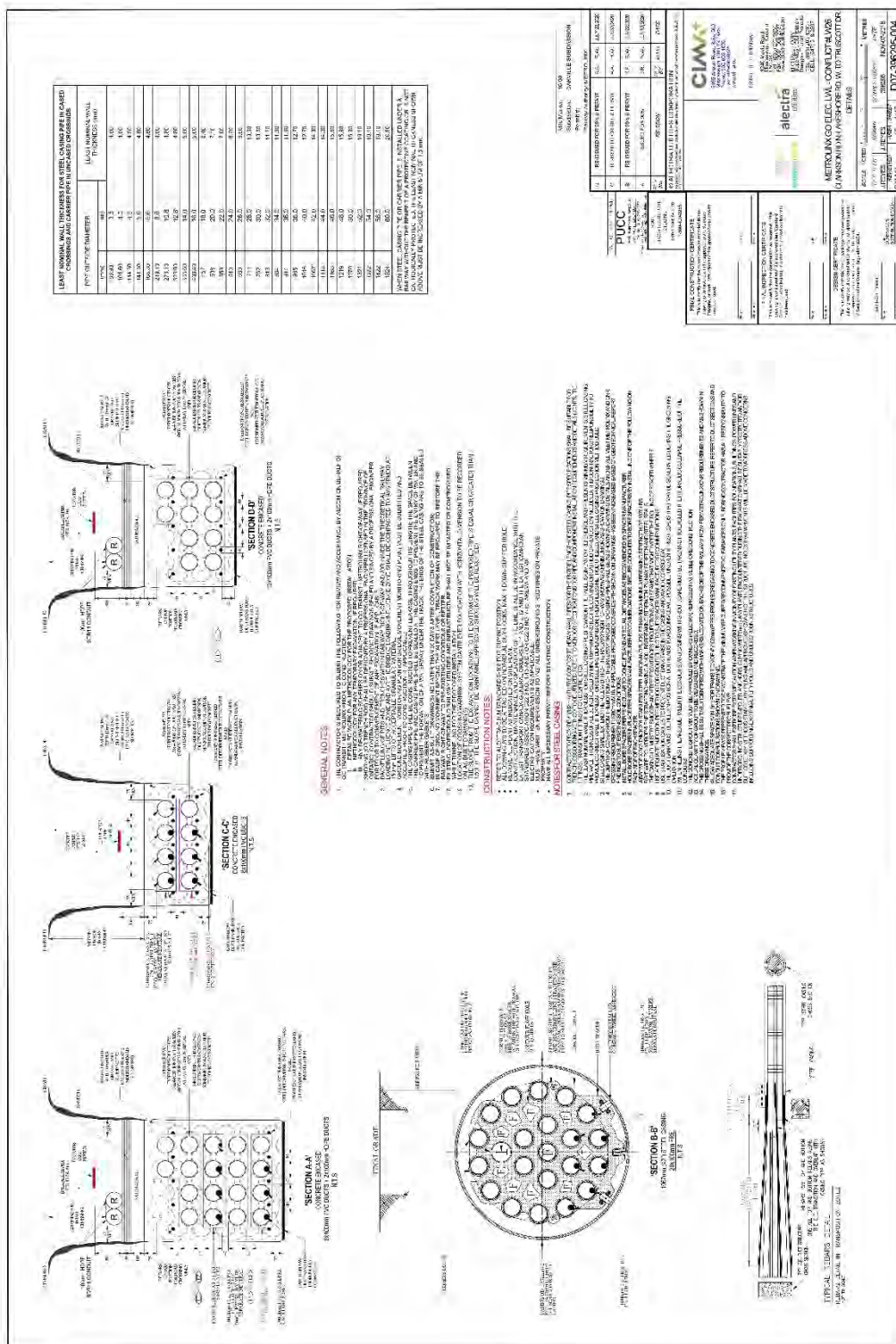
Interpretation Services: <http://www.mississauga.ca/portal/cityhall/languages>

APPENDIX D: METROLINX GO ELECTRICAL LWL. – CONFLICT #LW26, CLARKSON RD. N - LAKESHORE RD. W TO TRUSCOTT DRIVE, ALECTRA UTILITIES CORPORATION, ISSUED JULY 2020

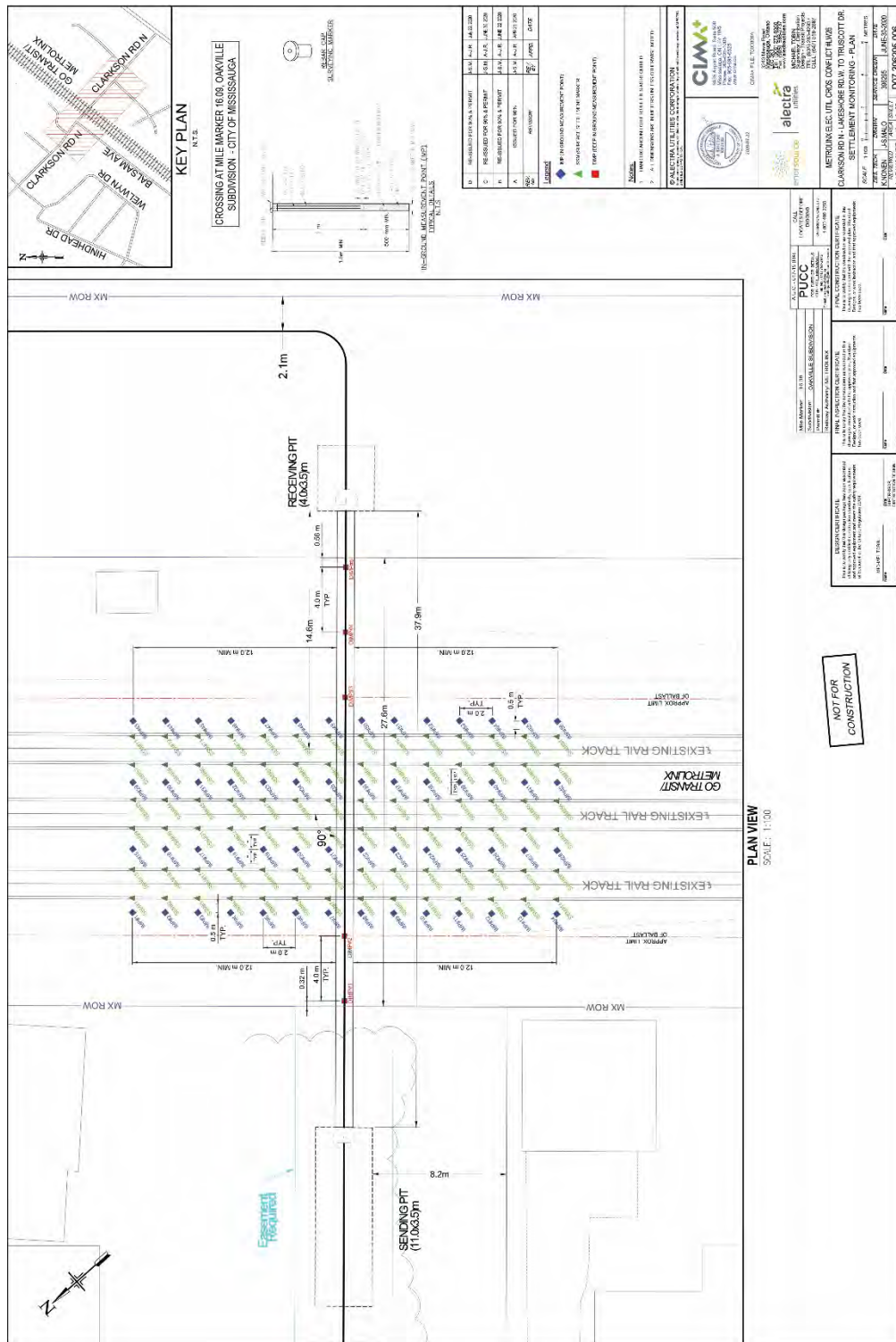


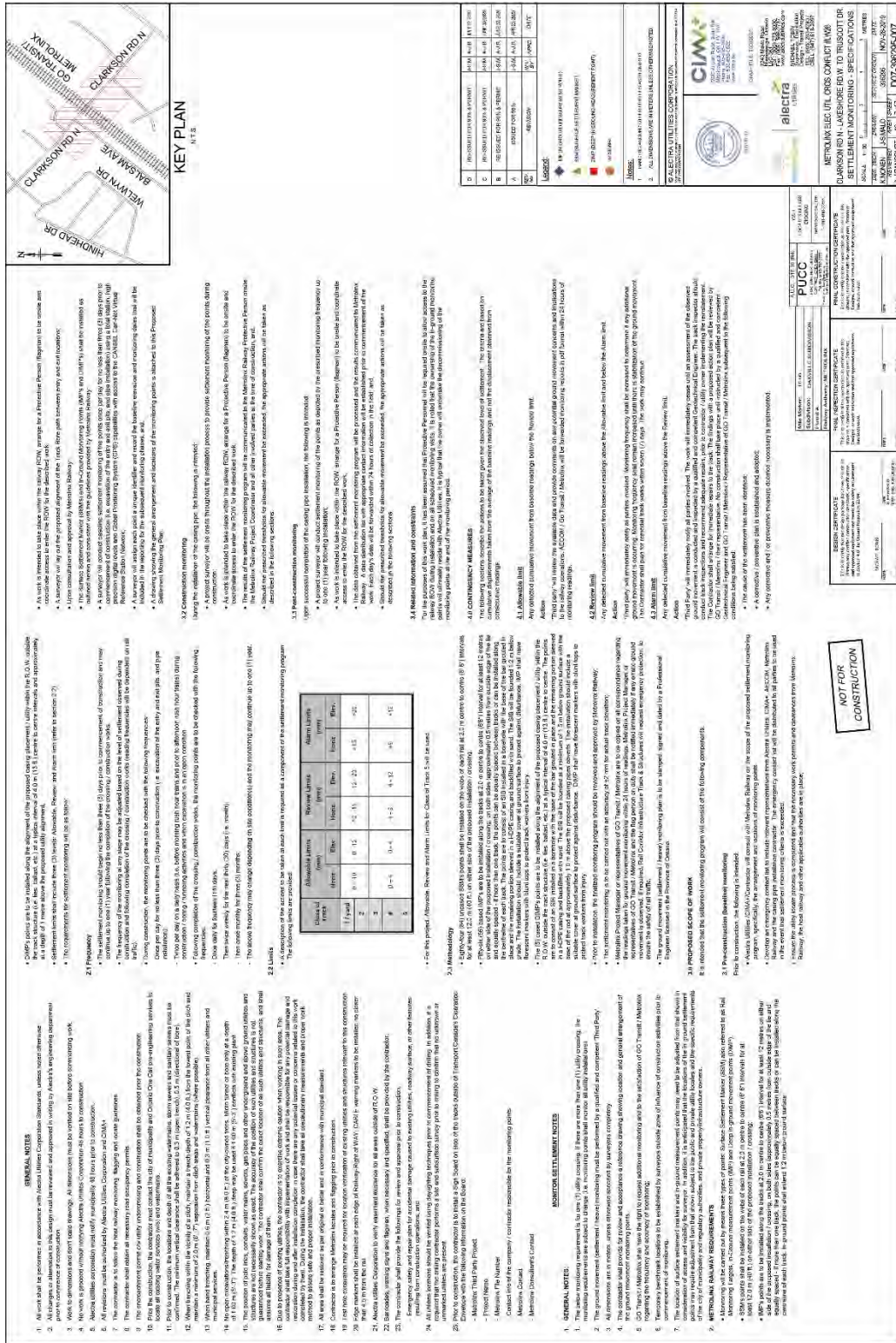












**APPENDIX E: CLARKSON FAMILY FILE, WILLIAM PERKINS BULL FONDS,
REGION OF PEEL ARCHIVES**

CLARKSON

CD

FAMILY HISTORY.

22 Jan. 1933

Name? Warren Clarkson 11800

Lot & Concession? Toronto S.D.S. Con. 2. lot 28 S pt. 50 ac. 1835

Date of birth? 1793 Date of death? 1882

Place of burial? Clarkson (Springcreek) Cemetery

Parents?

Where did they live?

From where did they come and when?

Wife's name? Susan

Wife's parent's names?

Wife's father's place of residence?

Date of wife's birth? 1797 Date of wife's death? 1853

Place of wife's burial? Clarkson (Springcreek) Cemetery

.....

What was Warren Clarkson's... Occupation? Farming

Religion? C. of E.

Politics?

What offices did he hold in the Community, Township, County, Province, School and Church?

Road Commissioner Toronto Twp. 1846

Lay Delegate from St. Peter's, Erindale to Synod.

To what Societies or Military Organizations did he belong?

Did he raise any pure-bred stock? If so, what breed?

-2-

11801 CD

Name? Warren Clarkson

Lot & Con. Tor. SDS Con. 2, lot 28

CHILDREN: Please give names, showing date of birth, where lived, to whom married, date of death, where buried.

William W. b. 1830 Lived Toronto SDS Con. 2, lot 29

Lourenda

m. Loretta Hemphill d. 1894 Buried Clarkson Cem.

Henry S. b. 1834 Lived Toronto SDS Con, 2, lot 28

m. Sarah Moseley d. 1918 Buried Clarkson Cem.

Lovina S. b. 1827 Lived Toronto SDS Con. 2, lot 28

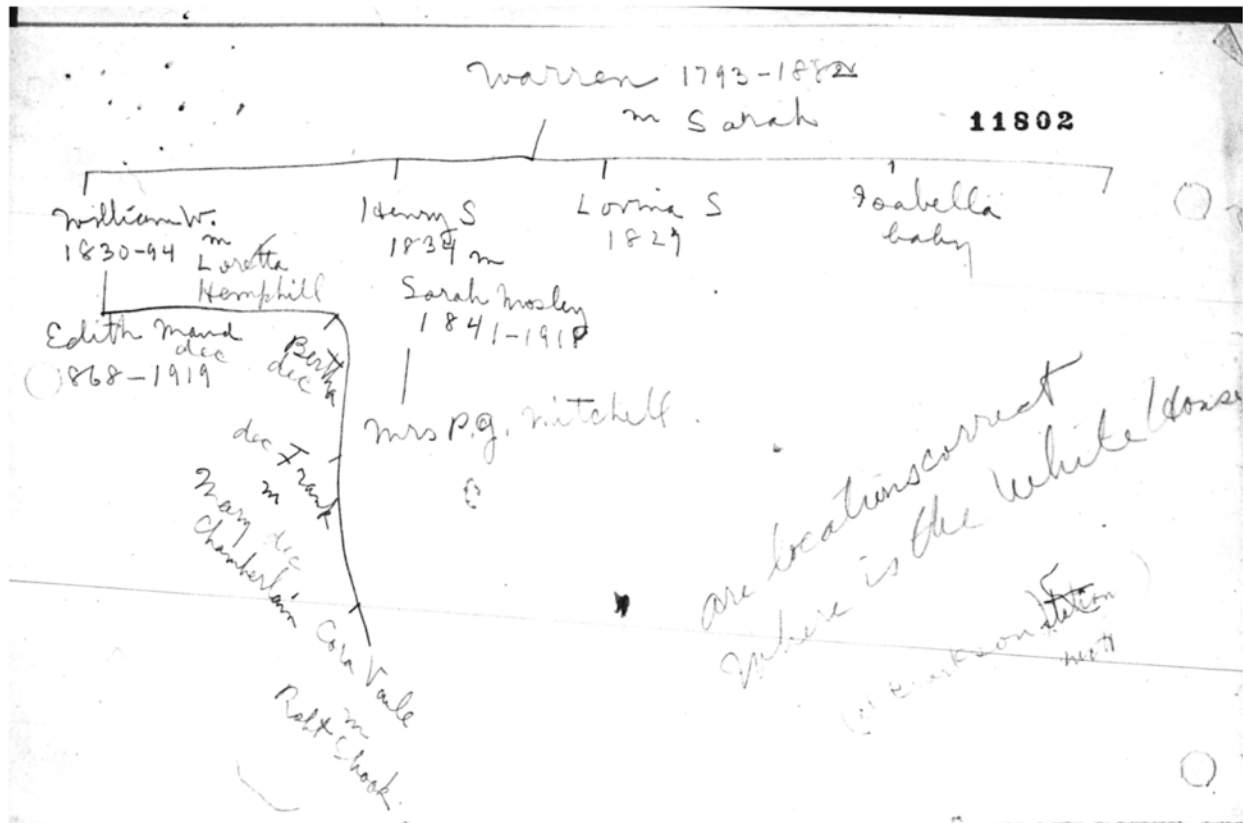
Unmarried d. 1850 Buried Clarkson Cem.

ALSO OWNED: Chinguacousy West Con. 1, lot 11 100 ac. PATENT 1831
Toronto S.D.S. Con. 2, lot 30 pt. 1819

Any further information:

Warren Clarkson was one of the early settlers in this locality. The old cottage on the corner of the Lake Shore Highway and the Clarkson Road was the original Warren Clarkson home. Mrs. Warren Clarkson later built a larger house nearer the station, now known as the Clarkson homestead.

Warren Clarkson gave the land for a road running from the C.N. Ry. tracks to the Lake Shore Rd. in about 1845. When the Great Western railway went through his property in 1856, the point where it crossed the above mentioned road was known as Clarkson's Corner's. It was only a flag stop, but gave the name to the present village.



11803

Warren Clarkson

Minutes of the Home District Municipal Council.

TORONTO Township, 1846.

That \$45 be expended on the Credit Hills on the Lake Road, and on other roads south of Dundas Street in the old survey, and that Capt. J.B. Harris, Warren Clarkson, and Jacob Cook, be co missioners to expend the same.

11804

TT

THE TORONTO MIRROR, Vol. XIX. No. 13. "Delirant
Reges, Plectuntur Achivi."--Horace. Toronto, Canada,
Friday, November 23, 1855.

[one word]

MARRIED--At Richmondhill, C.W., on Tuesday, the 20th
November instant, at the residence of M. Teefy
Esquire, by the Rev. James Dick, Mr. Warren Clarkson,
of Springfield, Toronto Township, to Mrs. Mary Ann
Clarkson, of Richmondhill.

(Pioneer)

11805

WARREN CLARKSON

(M. Miceli)

Village of Clarkson named after this family.

born 1793

died 1882 - buried Clarkson (Springfield)
Cemetery.

Emigrated from Cheshire, England, to Pennsylvania
and from there to Peel in 1819.

Married Susan ----

born 1797

died 1853 - buried Clarkson (Springfield) Cem.

1819 - purchased part of Lot 30 Con.2 Toronto S.D.S. £100

1831 - Patent 100 acres Lot 11 Con.1 - Chinguacousy West
under Regulations of 1804

1835 - purchased Lot 28 Con.2 South part 50 acres,
Toronto S.D.S. - £162.10

1837 - Directory shows Warren Clarkson as Householder
of Lot 29, Con.2 Toronto S.D.S.

1850 - Directory shows him as Householder of Lot 29 also.

In 1846 shown as Commissioner of Roads for Toronto twp.

(According to Questionnaire from family)

Warren was a radical and when ~~the~~ Mackenzie was
being sought by soldiers he hid him in his farmhouse.
Six British soldiers through out hay, grain and thoroughly
searched the barns. For 3 days and nights they occupied
the kitchen and slept in front of the fire place of the

Warren Clarkson

11806 (2)

old Dutch oven (which still exists), while Mackenzie was hiding over their heads.

The old homestead or farmhouse was built by Warren and still has the retreat of Mackenzie. It is known as the White House. Henry Clarkson (son of Warren) added two rooms to the portion built by a Mr. Neath, a former owner.

CHILDREN

William W.

Henry Shook

Lovina S. - born 1827 - died 1850 - buried Clarkson Cem.

Isabella.

WILLIAM W. CLARKSON

(son of Warren)

born 1830

died 1894 - buried Clarkson Cemetery.

William W. lived on Lot 28, Con.2 S.D.S. Toronto twp.

William's father started him up in a store at Port Credit which was then a trading post but William failed here. (From family's own questionnaire).

Was a J.P. " " " "

~~CHURCHMAN - Mrs. P.O. Mitchell, of Clarkson.~~

Married Loretta Hemphill, dtr. of Nathaniel Hemphill and Hannah Vaile.

Clarkson

11807 (3)

Children of William W.

Bertha - unmarried

Frank - married Emma Chamberlain (she later married
Oliver Gable, son
of Sylvester Gable
and Mary Ann Kelley
of Lot 12 Range 2,
Toronto S.D.S.
SEE GABLE PIONEER.)

Edith Maud - born 1868 - died 1919 - buried Clarkson Cem.
Unmarried. In 1907 purchased 25 acres
Lot 28, Con.2 for \$2,800.

Cora Vaile - married Robert Shook, son of Henry Shook
and Alicia Starret, Toronto S.D.S. Con.2
Lot 30.

HENRY SHOOK (CLARKSON)

(son of Warren)

born 1834,

died 1901 - buried Clarkson Cemetery.

1855 - purchased 42½ acres South Pt. Lot 28, Con.2 for £200

(From Family Questionnaire)

Henry was on the United Church Board.

He was a civil engineer and worked with J.J. Hill on
American railroad construction. Henry started with Hill
when the latter had little money, and had to take his
pay in notes and doubtful railroad bonds. When he could
not get any cash from Hill and his creditors would not
accept payment of any more bonds, Henry returned to
Clarkson, bringing with him a nigger who was very loyal.

Clarkson

11808 (A)

Henry brought several hundred strawb/erry plants which he planted in the field directly opposite where the Clarkson Post Office now is. He and his faithful nigger ~~xxxxx~~ tended the plants very carefully and produced the first berries in Clarkson.

Married - Sarah ~~Mosley~~ Moseley

born 1841

died 1918

(Sarah's grandfather was an auctioneer who owned land just east of the Don. He kept 2 nigger servants. Later, Sarah with her family lived at Dixie and were neighbours of Colonel Kennedy's family).

1 child only --

Mildred Daymar -- married P. Geo. Michell.
now in Clarkson. (1936).

11810

(Pionner) *File*

WARREN CLARKSON (M. Miceli)

Village of Clarkson named after this family.

born 1793

died 1882 - buried Clarkson (Springfield) Cemetery.

Emigrated from Cheshire, England, to Pennsylvania and from there to Peel in 1819.

Married Susan ----

born 1797

died 1853 - buried Clarkson (Springfield) Cem.

1819 - purchased part of Lot 30 Con.2 Toronto S.D.S. \$100

1831 - Patent 100 acres Lot 11 Con.1 - Chinguacousy West under Regulations of 1804

1835 - purchased Lot 28 Con.2 South part 50 acres, Toronto S.D.S. - \$162.10

1837 - Directory shows Warren Clarkson as Householder of Lot 29, Con.2 Toronto S.D.S.

1850 - Directory shows him as Householder of Lot 29 also.

In 1846 shown as Commissioner of Roads for Toronto twp.

(According to Questionnaire from family)

Warren was a radical and when ~~the~~ Mackenzie was being sought by soldiers he hid him in his farmhouse. Six British soldiers through out hay, grain and thoroughly searched the barns. For 3 days and nights they occupied the kitchen and slept in front of the fire place of the

11811

Warren Clarkson

(2)

old Dutch overn (which still exists), while Mackenzie was hiding over their heads.

The old homestead or farmhouse was built by Warren and still has the retreat of Mackenzie. It is known as the White House. Henry Clarkson (son of Warren) added two rooms to the portion built by a Mr. Heath, a former owner.

CHILDREN

William W.

Henry Shook

Lovina S. - born 1827 - died 1850 - buried Clarkson Cem.
Isabella.

WILLIAM W. CLARKSON

(son of Warren)

born 1830

died 1894 - buried Clarkson Cemetery.

William W. lived on Lot 28, Con.2 S.D.S. Toronto twp.

William's father started him up in a store at Port Credit which was then a trading post but William failed here. (From family's own questionnaire).

Was a J.P. " " " "

~~Married Loretta Hemphill, dtr. of Nathaniel Hemphill and Hannah Vaile.~~

Married Loretta Hemphill, dtr. of Nathaniel Hemphill and Hannah Vaile.

Clarkson

11812 (3)

Children of William W.

Bertha - unmarried

Frank - married Emma Chamberlain (she later married
Oliver Gable, son
of Sylvester Gable
and Mary Ann Kelley
of Lot 12 Range 2,
Toronto S.D.S.
SEE GABLE PIONEER.)

Edith Maud - born 1868 - died 1919 - buried Clarkson Cem.
Unmarried. In 1907 purchased 25 acres
Lot 28, Con.2 for \$2,800.

Cora Vaile - married Robert Shook, son of Henry Shook
and Alicia Starret, Toronto S.D.S. Con.2
Lot 30.

HENRY SHOOK CLARKSON

(son of Warren)

born 1834

died 1901 - buried Clarkson Cemetery.

1855 - purchased 42½ acres South Pt. Lot 28, Con.2 for \$200

(From Family Questionnaire)

Henry was on the United Church Board.

He was a civil engineer and worked with J.J. Hill on
American railroad construction. Henry started with Hill
when the latter had little money, and had to take his
pay in notes and doubtful railroad bonds. When he could
not get any cash from Hill and his creditors would not
accept payment of any more bonds, Henry returned to
Clarkson, bringing with him a nigger who was very loyal.

11813

Clarkson

(4)

Henry brought several hundred strawberrry plants which he planted in the field directly opposite where the Clarkson Post Office now is. He and his faithful nigger ~~man~~ tended the plants very carefully and produced the first berries in Clarkson.

Married - Sarah ~~Mosley~~ Moseley

born 1841

died 1918

(Sarah's grandfather was an auctioneer who owned land just east of the Don. He kept 2 nigger servants. Later, Sarah with her family lived at Dixie and were neighbours of Colonel Kennedy's family).

1 child only --

Mildred Daymar -- married P. Geo. Michell.
now in Clarkson. (1936).

Mr Wm Perkins Bull K.C.
Meridiths Court.
Toronto.

- Clarkson Ont.
Feb. 3rd '36

11815

ack'd
Feb 9/36
MHW

Dear Sir, Your letter re Clarkson family came to me as my wife the last of the Clarksons is not well and the information I am submitting is what has been told me.

Warren Clarkson came from Cheshire to Pennsylvania. During the unsettled days there he became one of the U. E. Loyalists. Coming to Canada he joined the British army and for his services received a grant of land. This land included what Brampton now is built on. He bought Lot 29 and 30 Con 2 S. D. S. for \$100. I still have the deed dated 1819. The Clarksons are related to the Shooks. This is not very clear but one Pennsylvanian Dutch name that I believe is a great-grand mother of my wife (formerly Mildred Dapmar Clarkson). Her name was Rosana Boomis Schuckers. That name was altered to Shook.

When Warren Clarkson died he left 22 Acres to William and 115 to Henry. The 22 acres were north of the G. W. railway and west of the Clarkson Travelled Road. The reason for this division of land was due

to Warren ²Clarkson. Having started ¹¹⁸¹⁶William
in a store a Post Office which was then a
trading post. William failed here. Henry
who was a Civil Engineer and worked with
J. J. Hill on American railroad construction
had not received any help from his father.
Henry started with J. J. Hill when Hill
had little money and had to take his pay
in notes & doubtful railroad bonds.

Warren Clarkson was a radical
and when the 'little rebel' was being hounded
by soldiers his Mackenzie in the farmhouse.
Six British soldiers threw out hay, grain
& thoroughly searched the barn. For three
days & nights they occupied the kitchen &
slept in front of the fire place of the old
Dutch oven - (still there) while the Little
Rebel was hiding just over their heads.

Returning to Henry I took
Clarkson. When he could not get any
cash out of J. J. Hill and his creditors
would not accept payment of any more
bonds Henry returned to Clarkson
bringing with him a big black
nigger who was excessively loyal.
Henry also brought several hundred
strawberry plants. He planted them
in the field directly opposite where
the Clarkson Post Office now is.

He & the nisse³ tended these plants¹¹⁸¹⁷
very carefully & produced the first
berries in Clarkson.

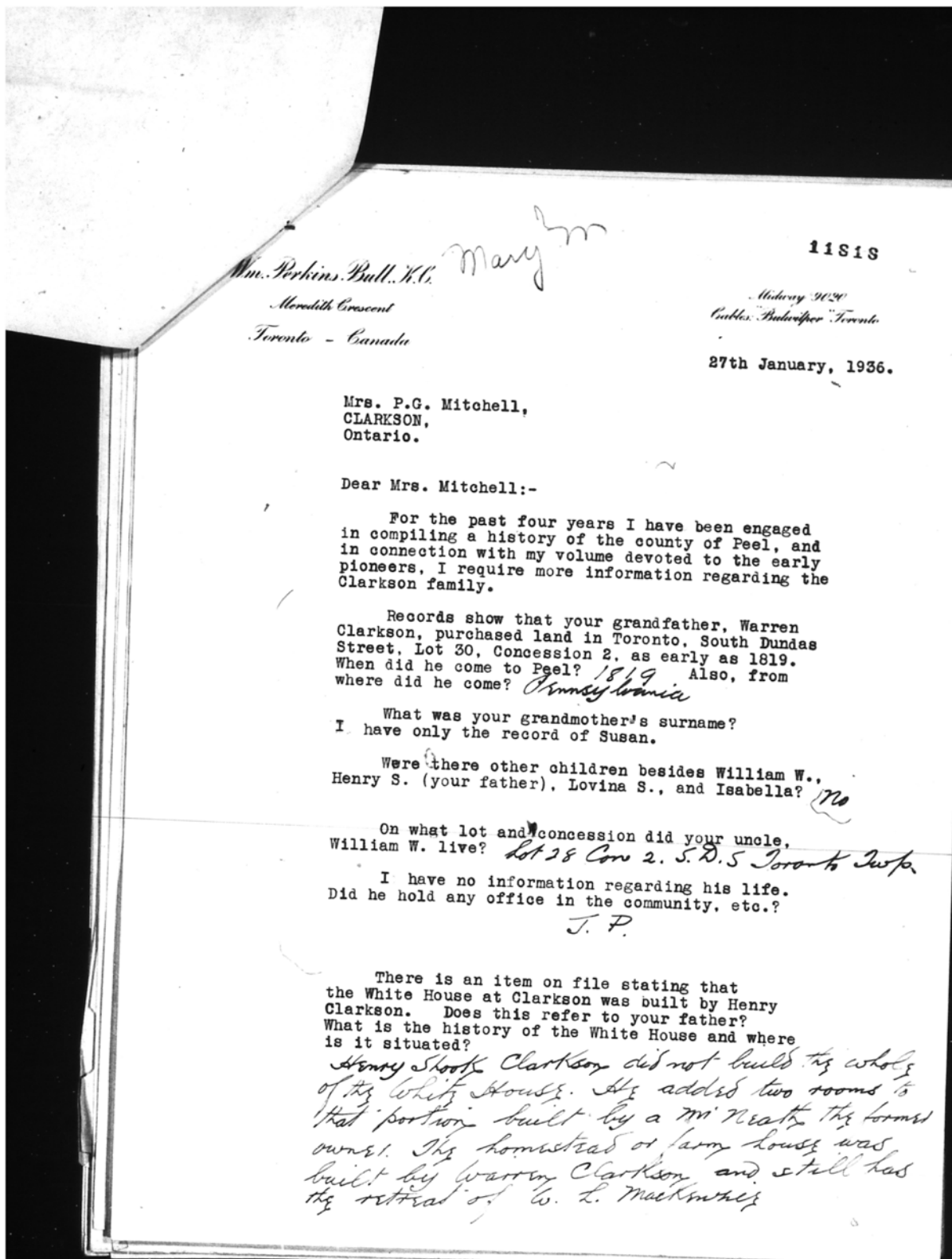
Henry Phook Clarkson
married Sarah Mosley. Her
grandfather Mosley was an auctioneer
and owned the farm just east of the
Don. The Don was the western
boundary. Oddly enough this Mosley
had two nisse³ servants. Sarah
Mosley with her family lived
at Dixie later neighbours of
Mrs Kennedy - mother of Jack &
Tom Kennedy - when she operated
a hotel at Dixie.

My wife Mildred Dajmar
Clarkson was their only child.

Any other information you
need I may be able to supply.

Yours truly

C. E. Mosley



11819

Mrs. P.G. Mitchell

(2)

Did your father hold any offices in the township, school or church?

on board of United Church

Did he raise any pure-bred stock? *No.*

brought Strawberry plants to Clarkson and produced the first berries grown in Clarkson.

Were there other children in your family besides yourself? *No.* Who are they?

There is a record of Joshua Clarkson, a native of the United States who, in 1819, purchased part of Lot 29, Concession 2, Toronto S.D.S. Is he a connection? *The same*

Any assistance you can render will be greatly appreciated, and as I am seriously held up for this data I shall be very glad to hear from you at your very earliest convenience.

Yours very faithfully,

Wm. Perkins Bull.

Per. *R*

MMH.

P.S.

Have you any old photographs or documents that you could loan me for reproduction? These of course would be returned in good order.

Was the village of Clarkson called after your family? *Yes.*

FAMILY HISTORY.

11820

Name? ^{Shook} Henry ~~Mc~~ Clarkson

Lot & Concession? Toronto S.D.S. Con. 2, lot 28 S. pt. 1855

Date of birth? 1834 Date of death? 1901

Place of burial? Clarkson (Springcreek) Cemetery

.....

Parents? Warren Clarkson and Susan

Where did they live? Toronto S.D.S. Con. 2, lot 28

From where did they come and when?

.....

Wife's name? Sarah Mosley

Wife's parent's names?

Wife's father's place of residence?

Date of wife's birth? 1841 Date of wife's death? 1918

Place of wife's burial? Clarkson (Springcreek) Cem.

.....

What was Henry S. Clarkson's Occupation?

Religion?

Politics?

What offices did he hold in the Community, Township, County, Province, School and Church?

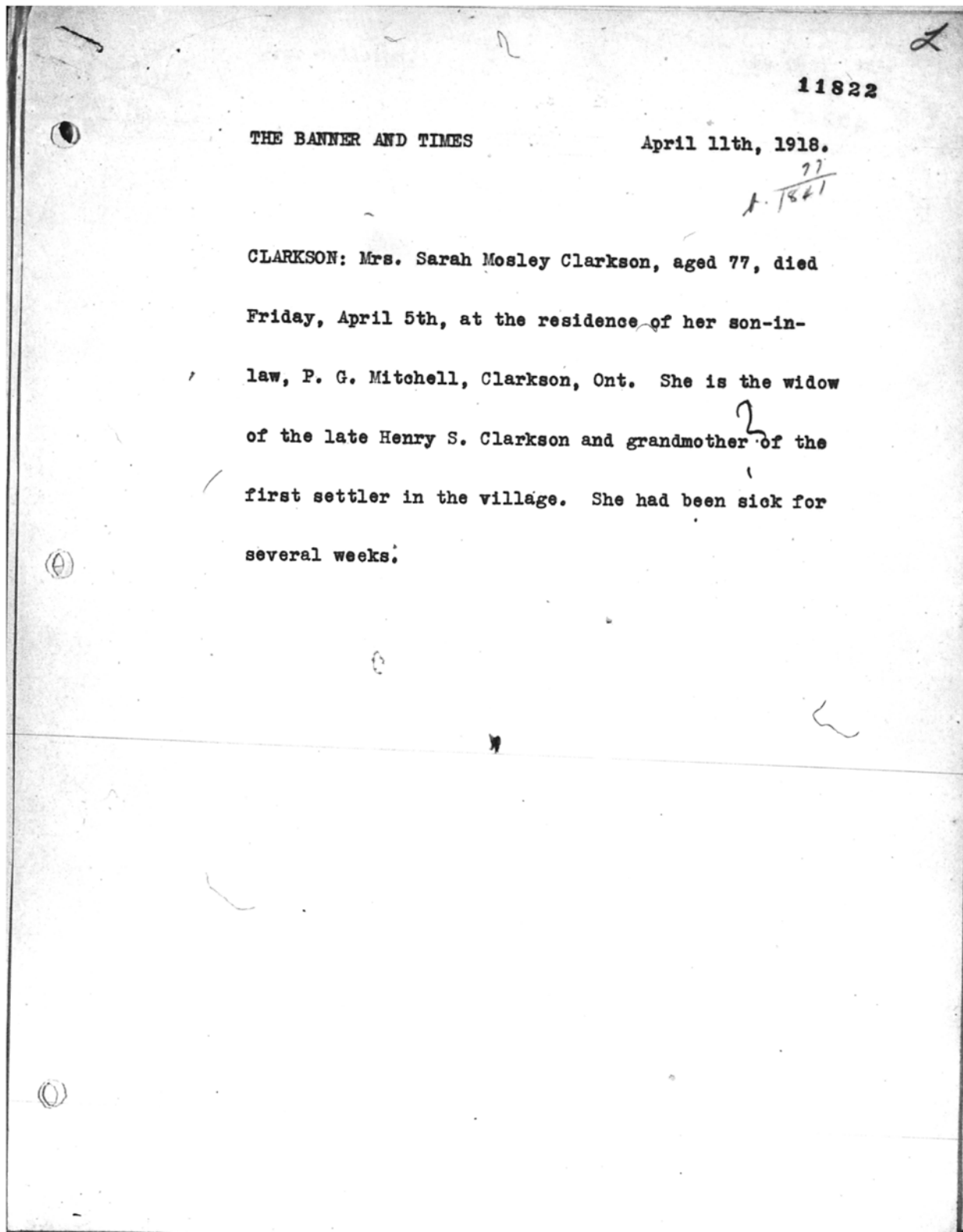
J.P. 1894

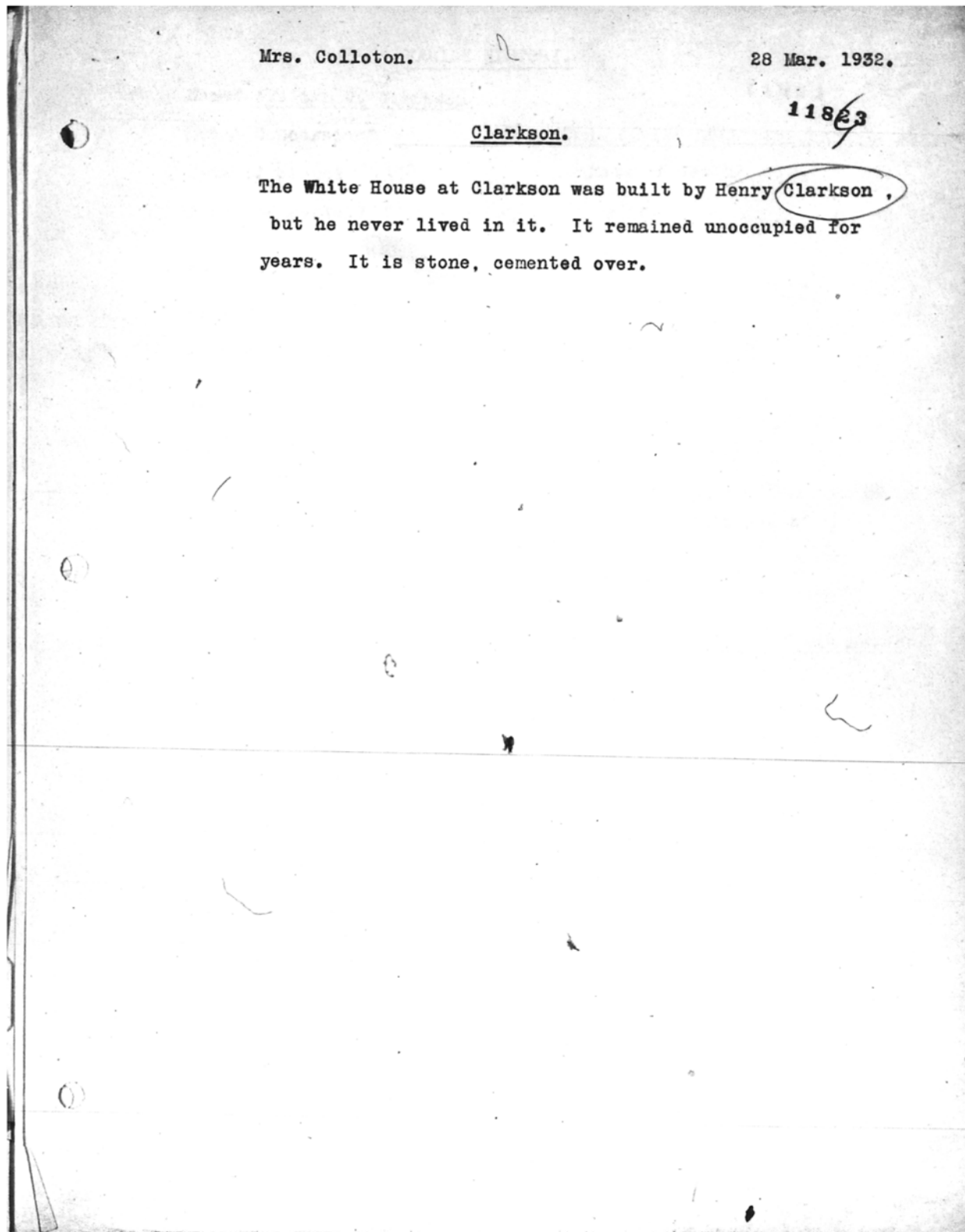
Auditor S.S. #5 - 1860

Trustee S.S. #5 - 1876, 77, 78.

To what Societies or Military Organizations did he belong?

Did he raise any pure-bred stock? If so, what breed?





FAMILY HISTORY.

CD
22 Jan. 1933

Name? William W. Clarkson 11824

Lot & Concession? Toronto S.D.S. Con. 2. lot 29 (Living here in 1877)

Date of birth? 1830 Date of death? 1894

Place of burial? Clarkson (Springcreek) Cemetery

Parents? Warren Clarkson and Susan

Where did they live? Toronto S.D.S. Con. 2, lot 28

From where did they come and when?

Wife's name? Louenda
Loretta Hemphill

Wife's parent's names? Nathaniel Hemphill and Hannah Vaile

Wife's father's place of residence? Toronto S.D.S. Con. 2, lot 31

Date of wife's birth? 1836 Date of wife's death? 1897

Place of wife's burial? Clarkson (Springcreek) Cemetery

.....

What was William W. Clarkson's Occupation? Merchant & Post-master

Religion?

Politics?

What offices did he hold in the Community, Township, County, Province, School and Church?

To what Societies or Military Organizations did he belong?

Did he raise any pure-bred stock? If so, what breed?

NOTES TAKEN FROM MR. AND MRS. GEORGE VAILE.

Mar 17 1885
11825

CLARKSON.

Lourence Lemphill, daughter of Nathaniel Kemp-
hill, married William Clarkson. 4 children.

Lertha - dead, buried at Clarkson.

Frank - dead, M. Emma Chamberlain.

Edith -

Hand - Dead.

Dora -

Vaile - rs. Robert Chook - dead. Called
after her grandmother.

CD

-2- 11826

CHILDREN: Please give names, showing date of birth, where lived,
to whom married, when died, and where buried.

<u>Bertha</u>	b.	Lived	d.	
	Unmarried		d.	Buried Clarkson Cem.
<u>Frank</u>	b.	Lived	d.	
	m. Emma Chamberlain		d.	Buried
<u>Edith Maud</u>	b. 1868	Lived Toronto	S.D.S. Con. 2, lot 28	
	Unmarried		d. 1919	Buried Clarkson Cem.
<u>Cora Vaile</u>	b.	Lived	d.	
	m. Robert Shook		d.	Buried

ALSO OWNED:

Any further information:

Wm. Clarkson started store and post-office at Clarkson, this
being for years the only waiting room for travellers.

CD 5

FAMILY HISTORY

22 Jan. 1933

11827

Name? Edith Maud Clarkson

Lot & Concession? Toronto S.D.S. Con. 2. lot 28 25 ac. 1907

Date of birth? 1868 Date of death? 1919

Place of burial? Clarkson (Springcreek) Cemetery

Parents? William W. Clarkson and ~~Leta~~ Loretta Hemphill

Where did they live? Toronto S.D.S. Con. 2. lot 29

From where did they come and when? Toronto S.D.S.

Any further information?

In 1906, Miss Edith M. Clarkson, who succeeded her father as post-mistress at Clarkson, enlarged the old building and built the new store.

Mrs. Maud Clarkson
11828

From Mrs. Charles Cordingley's Scrap Book.

CLARKSON On Sunday, Nov. 30th, at Clarkson, Edith
Maud Clarkson, daughter of the late William and Lorenda
Clarkson died (The year is not given). She was buried
in Clarkson Cemetery.

NS

11834

Clarkson (Springcreek)

CLARKSON, Warren F., d. 15 Feb. 1904, aged 42.
$$\begin{array}{r} 42 \\ 1862 \\ \hline 1904 \end{array}$$
Marie, w. of E. M. South, d. Sept. 1922,
aged 22.
$$\begin{array}{r} 22 \\ 1900 \\ \hline 1922 \end{array}$$
✓ William W., d. 15 Mar. 1894, aged 64.
$$\begin{array}{r} 64 \\ 1830 \\ \hline 1894 \end{array}$$
✓ Loretta Hemphill, his wife, d. 24 Mar. 1897, aged
61.
$$\begin{array}{r} 61 \\ 1836 \\ \hline 1897 \end{array}$$
Edith Maud, b. 29 Mar. 1868, d. 30 Nov. 1919.✓ Henry S., b. 1834. d. 1901.x H. Isabella, b. 1837, d. 1840.✓ Lovina S., b. 1827, d. 1850.✓ Warren, b. 1793, d. 1882.✓ Susan, b. 1797, d. 1853.

APPENDIX F: ARCHIVAL PHOTOS AND HISTORICAL PLAN



Figure 57: Edith and Cora Clarkson

(Source: Image BA0156, Historical Image Gallery, Mississauga Library System, undated)
Edith and Cora were the daughters of William Clarkson. Cora Clarkson married Robert Shook.



Figure 58: Edith Clarkson House 1160 Clarkson Road North, Plan G-13, Lot 1

(Source: Image BA0153, Historical Image Gallery, Mississauga Library System, undated)

PAGE TWO THE PORT CREDIT NEWS WEDNESDAY, AUGUST 29, 1934

Clarkson's Newest Shopping Centre **OPENS** **THURSDAY, AUGUST 30TH** **AT 2 P.M.**

The increasing popularity of the "grocery type" store and the economies effected by this system of merchandising have resulted in changing our store (W. T. McCord) from a service grocery to a CARLOAD GROCERIA. We are confident that the residents of Clarkson and district will approve of this change and find both pleasure and profit in shopping at our new Groceria.

Community Weiner Roast & Dance Sept. 5th

To celebrate the opening of this new CARLOAD GROCERIA a community Weiner Roast and Dance is to be held on WEDNESDAY EVENING, SEPTEMBER 5th. Come and bring your friends.

TICKETS FREE TO OPENING DAY CUSTOMERS AUGUST 30th

Tickets for dancing and refreshments will be given to all opening day customers. Be on hand early, THURSDAY, August 30th, and secure your tickets. Store opens at 2 p.m.

DELIVERY SERVICE: Delivery is available on all orders for the sum of 5c per order.

SHOPPING BAGS — FREE SAMPLES and GIFTS

Some of Our Many GROCERY SPECIALS

Sugar, 10 lbs.....	53c
Lard, per lb.....	11c
White Sponge Flour, 7's.....	23c
24's.....	67c
Pure Light Honey, 2 1/2's.....	25c
Hillcrest Peas, No. 4's.....	2 for 19c
C. & B. Boned Chicken.....	23c
Chef Ready Meals.....	2 for 21c
Maxwell House Coffee, 1's.....	35c
1/2's.....	20c
Vinegar, Spirit or Cider, per gal.....	35c
Kipper Snacks.....	6 for 25c
Waxed Paper, 15 sheet roll.....	2 for 5c
Waxed Paper, 40 ft. roll.....	10c

MEAT PRICES MEATS AT CARLOAD PRICES PRICES CUT BUT QUALITY MAINTAINED

BEEF

Rump Roast of Beef.....lb.	11c
Round Steak Roast or Sliced.....lb.	14c
Prime Rib Roast of Beef.....lb.	12c
Shoulder Roast of Beef.....lb.	10c
Pot Roast.....lb.	8c
Brisket.....lb.	5c
Lean Hamburg Steak.....3 lbs.	19c
Stewing Beef.....3 lbs.	19c

LAMB

Choice Spring Lamb Legs.....lb.	18c
Short Loins.....lb.	19c
Fronts.....lb.	12c
Stewing Lamb.....lb.	10c

VEAL

Leg of Veal.....lb.	19c
Veal Chops.....lb.	17c
Roll Fillet of Veal.....lb.	13c
Home-made Sausage.....3 lbs.	25c

Shortening, 2 lbs.....	17c
Black Fly Insecticide, 8 oz. tin.....	29c
Whole Mixed Pickling Spice, 1 lb.....	19c
Olives, 8 oz. Queen.....	15c
11 oz. Stuffed.....	25c
Olives, 11 oz. Queen.....	19c
8 oz. Stuffed.....	19c
Sugar Crisp Corn Flakes.....3 for	19c
Scribbles (Carload) 180 pages.....	3c
Lead Pencils.....3 for	5c
Kirk's Castile Soap.....4 for	19c
St. William's Plum Jam, 32 oz.....	21c
Salada Tea, 1/2 lb.....	30c

WATCH THURSDAY'S PAPER FOR BUTTER PRICES

Figure 59: An opening announcement of the Carload Groceteria in the local newspaper
(Source: Port Credit News, August 29, 1934)

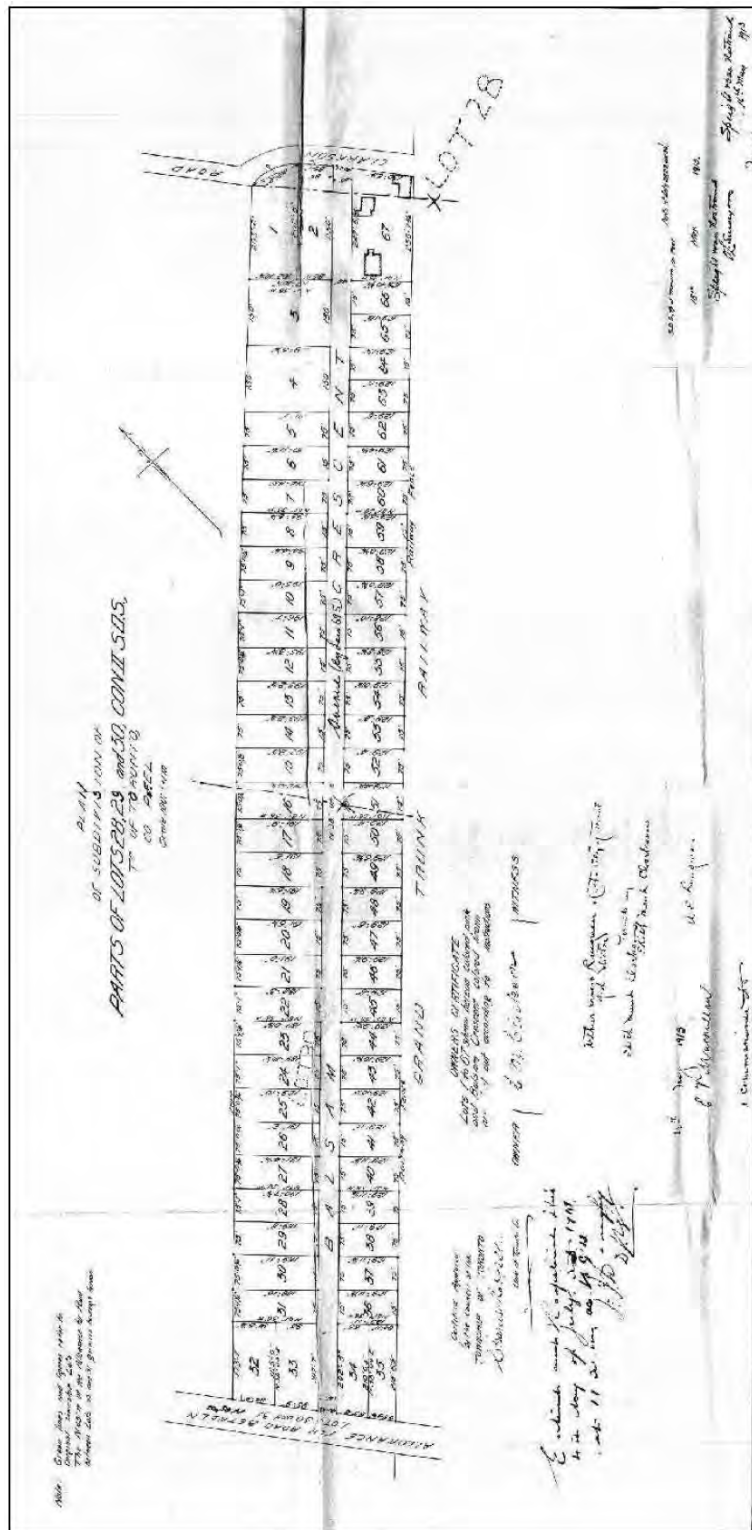


Figure 60: Plan G-13, Plan of Subdivision of Parts Lot 28, 29 and 30, Concession 2, SDS, Township of Toronto, Co. Peel, Speights and Van Nostrand, O.L. Surveyors Toronto, May 15, 1913.

**APPENDIX G: THE CORPORATION OF THE CITY OF MISSISSAUGA BY-LAW
0057-2017**

CERTIFIED A TRUE COPY
[Signature]
DIANA RUSNOV DEPUTY CLERK
 CITY OF MISSISSAUGA

THE CORPORATION OF THE CITY OF MISSISSAUGA

BY-LAW NUMBER *0057-2017*

A By-law to designate the Clarkson Property located
at 1130 - 1140 Clarkson Road North as being of
cultural heritage value or interest

WHEREAS the *Ontario Heritage Act*, R.S.O. 1990, Chapter O.18, as amended (the "*Heritage Act*"), authorizes the Council of a municipality to enact by-laws to designate real property, including all the buildings and structures thereon, to be of cultural heritage value or interest;

AND WHEREAS Council for The Corporation of the City of Mississauga approved the designation of the property known as the Clarkson Property, located at 1130-40 Clarkson Road North in the city of Mississauga (the "Property") as being of cultural heritage value or interest through Resolution 0078-2016;

AND WHEREAS in accordance with the requirements of the *Heritage Act*, a Notice of Intention to designate the Property was duly published and served, and no notice of objection to such designation was received by the Clerk of The Corporation of the City of Mississauga;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. That the property, including all the buildings and structures thereon, known as the Clarkson Property located at 1130 - 1140 Clarkson Road North in the City of Mississauga and legally described in Schedule 'A' attached hereto, is hereby designated as being of cultural heritage value or interest under Part IV of the *Ontario Heritage Act*, R.S.O. 1990, Chapter O.18, as amended.
2. That the reasons for designating the Property are duly set out in Schedule 'B', attached hereto.
3. That the City Clerk is hereby authorized to cause a copy of this By-law to be served upon the owner of the Property and upon the Ontario Heritage Trust and to cause notice of this By-law to be published in a newspaper having general circulation in the city of Mississauga.
4. That Schedules 'A' and 'B' form an integral part of this By-law.
5. That the City Solicitor is hereby directed to register a copy of this By-law against the property located at 1130 - 1140 Clarkson Road North as described in Schedule 'A' in the proper land registry office.

ENACTED AND PASSED this *26* day of *April*, 2017.

APPROVED AS TO FORM City Solicitor MISSISSAUGA			
<i>[Signature]</i>			
Date	2017	04	20

Bonnie Crombie
MAYOR

Cynthia Green
CLERK

SCHEDULE 'A' TO BY-LAW 0057-2017

Summary: Part of Lot 67, Registered Plan G-13
(To be designated under the Ontario Heritage Act)
(Ward 2, City Zone 10, in the vicinity of Clarkson Road North and Balsam Avenue)

Legal Description: In the City of Mississauga, Regional Municipality of Peel, Province of Ontario and being composed of part of Lot 67, Registered Plan G-13, designated as Parts 1 and 2, Plan 43R-37471.

Alnashir Jeraj
Ontario Land Surveyor

Page 1 of 1

SCHEDULE 'B' TO BY-LAW NO. 0057-2007**DESIGNATION STATEMENT**
Clarkson Property, 1130 - 1140 Clarkson Road North**Description of Property**

The subject property is located on Part of Lot 67, Registered Plan G-13, designated as Parts 1 and 2, Plan 43R-37471, formerly part of Lot 28, Concession 2 SDS, Toronto Township at the municipal address of at 1130 - 1140 Clarkson Road North, City of Mississauga.

The subject property, in its entirety, functions as a physical, historical and contextual reminder this was once the principal location of the Clarkson's village commercial core from the 19th century onwards.

Statement of Cultural Heritage Value or InterestDesign/Physical Value

The subject property, located at 1130-1140 Clarkson Road North in the City of Mississauga is an important and significant cultural heritage property that has design, associative and contextual values.

The design/physical value of the subject property relates specifically to the Clarkson Store and the William Clarkson House.

The Clarkson Store was substantially altered from its 19th century form c. 1906-1910. These early 20th century alterations are considered to be of sufficient age as to form an integral part of the history of the building. Although more recent exterior alterations have been completed, the store building still retains its 1906-1910 height, scale, shape, form and massing and a commercial storefront of an early 20th century rural store. Examples of rural commercial stores of this age are considered to be a diminished resource in the City.

The William Clarkson House is a vernacular, mid-19th century, wood frame residence with characteristics of the Gothic Revival style. When built, it was one and one-half storeys high with a front/cross gable roof, rectangular floor plan, symmetrical arrangement of openings on the principal facades and a wrap-around verandah. Exterior details included clapboard siding with corner boards and door and window openings with triangular shaped heads on the front elevation. Despite later alterations to the exterior including the installation of new siding, the addition of two roof dormers, removal of the original chimneys and some modifications to the door and window openings, the building retains its dominant form and exterior elements of its original design character.

Historical/Associative Value

The property has historical value for its associations with an important settlement family that contributed significantly to the settlement of Clarkson and Toronto Township in the 19th century. In recognition of the significance of the Clarkson store to the community, the road passing in front of the general store, and later post office, the train station, and the village were all named Clarkson. The local GO Transit train station still bears the family name.

The Clarkson family was fundamental to the development of the local community. Warren Clarkson was one of the first settlers in this area of Toronto Township. His decision to locate the first general store in the area, the construction of the railway beside the store, which ensured the community's economic viability, and the location of the first postal service in the store represent and illustrate important social and development periods of the community's history.

The property was the site of the first post office in Clarkson, an event that meant local residents no longer had to travel to Erindale to retrieve their mail. The post office was located in the Clarkson Store, the focal point of the community. It was located on the subject property for 80 years from 1875 to 1955. A new, separate post office building was constructed in the late 1940s beside the store and house.

Contextual Value

The subject property, in its entirety, has been identified as having contextual value relating to the historical, visual, physical and functional links between the property, its buildings and the surrounding neighbourhood that once formed the nucleus of Clarkson's Corners, later Clarkson,

Page 1 of 2

people and activities that were significant to the community and its ability to yield information that contributes to an understanding of the community. The property was the site of Warren Clarkson's store in 1835. The store became the nucleus of the historic settlement community and in 1875, the location of the first post office to serve Clarkson.

The three buildings on the subject property are positioned on the north side of the railway tracks facing onto and set close to Clarkson Road North, an important transportation north to south route both historically and currently. This setting of the subject property is valued for maintaining the 19th and early 20th century character of the historic village core of Clarkson village. The original Clarkson land holdings were subdivided as Plan G-13 in 1913 and the buildings were placed on Lot 67. The configuration of Lot 67 has not been subdivided since 1913 and is clearly delineated in the environment between the railway tracks to the south and Balsam Avenue to the north.

The property with its buildings is the pivot point for historically, visually and contextually linking the adjacent properties associated with the Clarkson family and the village. The Clarkson Store on the south end beside the railway track and the William Clarkson House on the north end of the property at Balsam Avenue are clearly visible in the streetscape. The subject property at 1130-1140 Clarkson Road North is considered to be a physical landmark in the community.

The heritage attributes of the subject property located at 1130-1140 Clarkson Road North include, but are not limited to the following listed.

Heritage Attributes

- The placement and orientation of the buildings, the Clarkson Store, the former Post Office Building and the William Clarkson House, in their original positions on the west side of Clarkson Road North to the north at the railway tracks with the principal elevations facing onto the said road.
- The integrity of Lot 67, Plan G-13 on which the three buildings are located.
- Its pivotal position in the historic core of Clarkson as an historical, visual and contextual link adjacent to the neighbourhood properties associated with the Clarkson family, former commercial, industrial and residential buildings that were clustered around the store and the location of the former Clarkson's railway station.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings and the commercial storefront of the Clarkson Store.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings, and verandah of the William Clarkson House.
- The north and south views to the subject property and its three buildings from Clarkson Road North.

APPENDIX H: RESUME

A. DAVID ECKLER, AREA



FIRM PROFILE | HERITAGE & MUSEUM WORK

AREA is a full service firm, based in Toronto and operating across Canada, which specializes in the restoration and adaptive re-use of historic buildings, urban design for heritage streetscapes and approvals under the Ontario Heritage Act. The firm has a history extending over 30 years of practice, and is managed by 2 principals and 8 technical staff – including intern architects, interior designers and architectural technologists – with experience in the documentation and restoration of historic buildings and sites. Although we are qualified for heritage and museum projects, the members of our firm have also undertaken a wide range of institutional and commercial projects often involving the integration of historic components into new developments.

AREA and its staff are members of various heritage associations and advisory boards across Canada. David Eckler, B.E.S., B.Arch., OAA, MRAIC is an active member in many heritage associations including the Architectural Conservancy of Ontario Advisory Board. He is a former Vice-Chair of Heritage Toronto, which advised Toronto City Council on heritage matters as a LACAC under the Heritage Act. Bernard Rasch, B.Arch., PPOAA, FRAIC, ARIBA has served on a number of heritage committees and boards including the Markham District Historical Society and City of York Historical Committee and the Metro Board of Management for The Guild from 1984 to 1998 where he served in many positions including Chair & Vice-Chair of the Board.

Historical Façade Improvement Guidelines & Heritage Districts

- Stouffville Main Street Revitalization, 1998, DEA was presenter at workshop
- Yonge Street Commercial Façade Improvement Program, 1996, received City approval of grant
- Woodstock Façade Improvement Program, 1995, DEA initiated program for City
- Hazelton-Yorkville Area Heritage Conservation District Study, City-sponsored study
- Fergus Downtown Community Masterplan & Design Guidelines

Historic Museums, Institutional & Cultural Buildings

- Officers' Quarters (1830), Military & Naval Establishment, Discovery Harbour, Penetanguishene
- Spence Half-Way House Restoration (c. 1850), Muskoka Pioneer Village, Huntsville
- Sharon Temple Compound (1821), Sharon, York Region
- Heliconian Hall (first Olivat Sunday Schoolhouse, circa 1876), (Yorkville)
- Cedar Ridge Studio Gallery (1918), 225 Confederation Drive, (Scarborough)
- Aurora Historical Society Museum (1886 school), 22 Church Street, Aurora
- The Niagara Institute (early 20th.c), 9 Weatherstone Crt., Niagara on the Lake
- St. Lawrence Hall (1840) - renovations of town hall to accommodate National Ballet School

Historic House Restorations

- Jacob Ross House Restoration (1852), 108 Stayner Ave.
- William Wonch House Restoration (1840), 2777 Woodbine Ave., Markham
- Robert Milroy House Restoration (c. 1833), 7111 Reesor Rd., Markham
- McDougall Farmhouse (1893) Heritage Assessment, James Snow Parkway, Milton, ON
- Devonian House Restoration & Addition (circa 1923), 144 John St. E., Niagara on the Lake
- Savage House & Blacksmith Shop (c.1840), 1480 Derry Rd. E., Mississauga

Converted Historic Residences

- Old Post Inn (c. 1830), 367 Kingston Road East, Ajax
- Valley Halla Villa (Jackson Residence, 1922), Toronto Zoo, Rouge Valley, Scarborough
- Armour Heights Officer's Mess (1913, 'Strathrobyn'), Canadian Forces College, 215 Yonge Blvd.
- Bellevue Daycare Centre (1887), 95 Bellevue Ave.
- Gerrard & Bay Historic Houses (1860-1890), 68-84 Gerrard St. W.
- Toronto French School Restoration (Silton Estate, 1923), 294 - 318 Lawrence Ave. E.

AREA

EDUCATION

University of Waterloo
B.Arch (1985)
B.E.S. (1982)

MEMBERSHIPS

Ontario Association of Architects
(Former Councillor & Chair Awards Committee)

Royal Architectural Institute of Canada

Canadian Standards Association (CSA)

Architectural Conservancy of Ontario Advisory Board

Society for the Study of Architecture in Canada

Heritage Canada Foundation

Ontario Historical Society

CAREER SUMMARY

AREA Architects Rasch Eckler Associates Ltd.
President
2001 to Present

David Eckler Architect
1991 – 2001

Page & Steele Architects
1989 – 1991

Arthur Erickson Architects
1986 – 1989

DAVID ECKLER BES, B.Arch., OAA, MRAIC
AREA, Architects Rasch Eckler Associates Ltd.
President & Principal – Senior Heritage Architect

David Eckler is the firm's principal and is responsible for the design, construction drawings, specifications and construction administration of all heritage projects in the office. Mr. Eckler has over 25 years experience in the conservation, restoration and adaptive reuse of heritage structures for government, non-profit agencies and private sector owners and developers. Mr. Eckler directs the Concept Design, Design Development and Contract Documents phases of heritage projects and authors many of the firm's heritage assessment reports.

Mr. Eckler established a specialization in heritage conservation beginning in 1992 with his previous firm *David Eckler Architect (DEA)* and continuing in his current practice, *AREA Architects*. His architectural heritage services include feasibility studies, preservation planning, infill projects within historic districts, adaptive reuse and building restoration. David is an active member in many architectural and heritage associations including the *Architectural Conservancy of Ontario Advisory Board*. He is a past member of the *Canadian Association of Professional Heritage Consultants* and is a former Vice-Chair of *Heritage Toronto*, which advised Toronto City Council on heritage matters under the Heritage Act and as an advisory board for the city's museums.

Mr. Eckler has particular experience in the restoration of heritage properties within public parks and cultural landscapes. An example of a heritage attraction in a park setting is the restoration of the Officers' Quarters within the *Discovery Harbour* museum in Penetanguishene. He has most recently worked on the restoration of the historic site of the *1910 Allan Gardens Conservatory*.

RELEVANT EXPERIENCE: Toronto location unless indicated

Heritage Adaptive Re-use

- Goldring Student Centre (Wymilwood, 1954) – 150 Charles St. W., Toronto
- Warwick Office Building (1905) – 401-409 King St. W.
- Church of Christ, Scientist (1928), Condominium Redevelopment, 70 High Park
- Eglinton Hunt Club (1929) – Condominiums, 1355 Kingston Rd.
- Hutton House (1853) – Community Centre, Ardmore Park, St. Marys
- Bellevue Daycare Centre (1887) – 95 Bellevue Avenue

Restoration of Institutional Historic Buildings

- Allan Gardens Conservatory Complex (1910) – 160 Gerrard St. E.
- Aurora Historical Society Museum (Church Street School, 1886)
- Toronto French School (Sifton Estate, 1924) – 306 Lawrence Ave. E.
- Armour Heights Officers' Mess ('Strathrobyn' 1913) – 215 Yonge Blvd.
- Medical Arts Building Restoration (circa 1929)
- Officers' Barracks (1830) – Discovery Harbour, Penetanguishene
- Heliconian Hall (first Olivet Church, 1876) – 35 Hazelton Ave.

Heritage Planning, Parks & Streetscape Design

- Cookstown Heritage Conservation District – Innisfil, ON
- Old Pickering Village Planning & Heritage Study, Ajax
- Yorkville-Hazelton Avenue Heritage Conservation District
- Limehouse Kilns Heritage Masterplan, Halton Hills
- Confederation Commemorative Park, Charlottetown, PEI
- Gerrard & Bay Historic Houses (1860-1890)