

Detailed Information and Preliminary Planning Analysis

**Owner: The Erin Mills Development Corporation on behalf of St. Mark and
 St. Demiana Coptic Orthodox Church**

0 Ninth Line

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1. Proposed Development

The applicant proposes to develop the subject property with a place of religious assembly (Phase 1) and a three storey private community services centre (Phase 2) on a private sanitary system with municipal water servicing. The place of religious assembly will contain ancillary uses such as a cafeteria, banquet/multi-purpose hall, Sunday school classrooms and gymnasium facilities. The private community services centre will contain support uses for the place of religious assembly including counselling, tutoring and career development. A rezoning application is required to permit the proposed development (refer to Section 4 for details concerning the proposed amendments).

Development Proposal		
Application submitted:	Received: September 18, 2020 Deemed complete: December 9, 2020	
Developer/ Owner:	The Erin Mills Development Corporation on behalf of St. Mark and St. Demiana Coptic Orthodox Church	
Applicant:	John D. Rogers & Associates Inc.	
Proposed Gross Floor Area:	11,592 m ² (124,775.3 ft ²) (place of religious assembly) 10,830 m ² (116,573.2 ft ²) (private community services centre)	
Height:	32 m (105 ft.)	
Lot Coverage:	21.2%	
Floor Space Index:	0.60 FSI	
Landscaped Area:	To be confirmed	
Road Type:	Public road (Ninth Line)	
Parking:	Required	Provided
Place of assembly	535	427

Development Proposal		
Private com. serv. centre	190	96
Future	-	65
Total	725	588
Green Initiatives:	<ul style="list-style-type: none"> • High efficiency LED lighting • High efficiency, low consumption fixtures • Permeable paving surfaces where feasible • On-site stormwater management 	

Supporting Studies and Plans

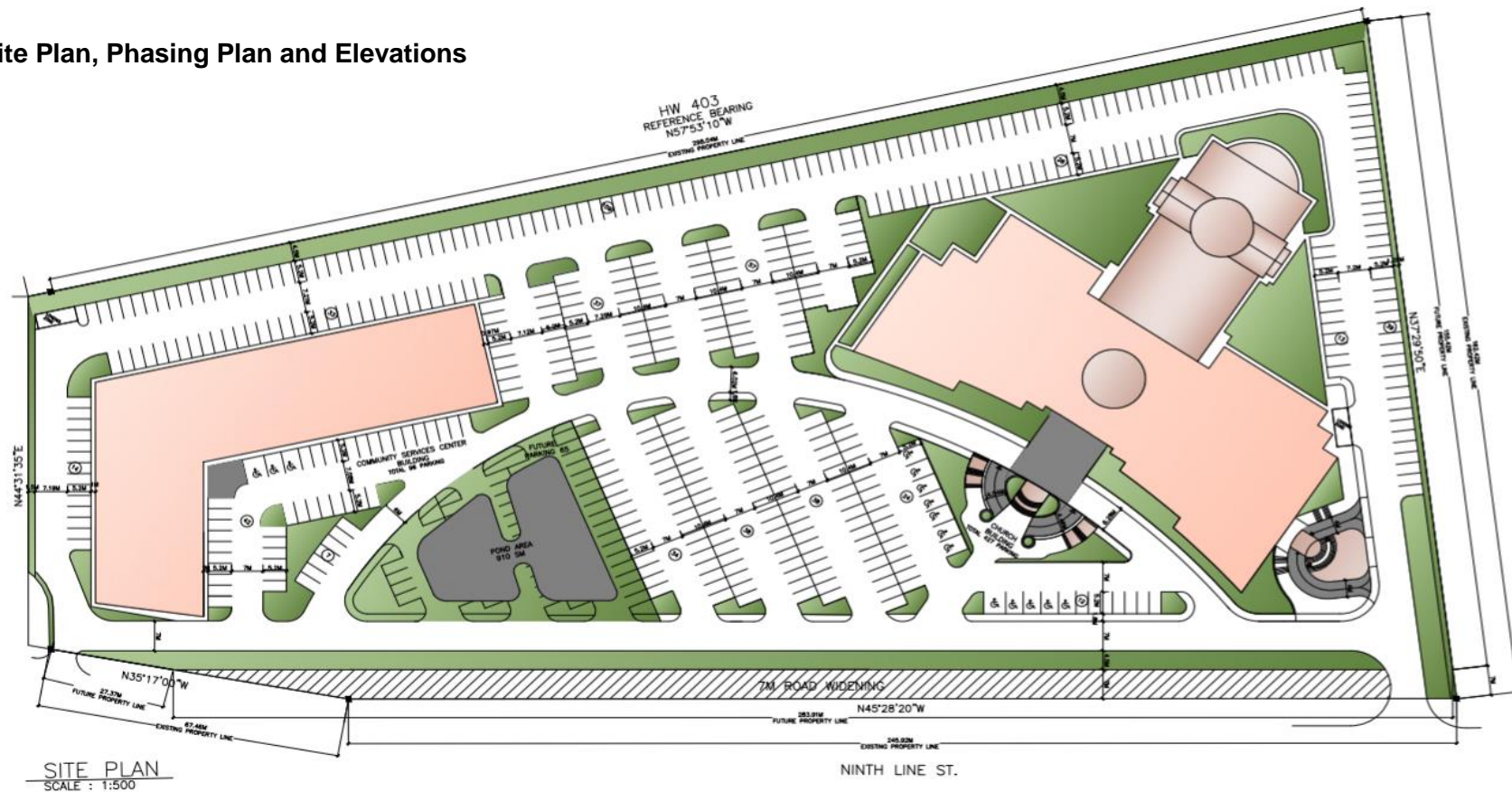
The applicant has submitted the following information in support of the application which can be viewed at <http://www.mississauga.ca/portal/residents/development-applications>:

- Planning Justification Report
- Concept Site Plan, Phasing Plan and Elevations
- Context Map and Survey
- Draft Zoning By-law Amendment
- Acoustical Feasibility Study
- Arborist Report and Tree Preservation Plan
- Archaeological Assessment and Letter from Ministry of Heritage, Sport, Tourism, Culture Industries
- Hydrogeological Assessment
- Zoning By-law Amendment
- Functional Servicing/Stormwater Management Report
- Phase I Environmental Report
- Parking Utilization Study
- Grading, Drainage and Servicing Plans
- Low Impact Design Summary

Application Status

Upon deeming the application complete, the supporting studies and plans were circulated to City departments and external agencies for review and comment. These comments are summarized in Section 6 of this appendix and are to be addressed in future resubmissions of the application.

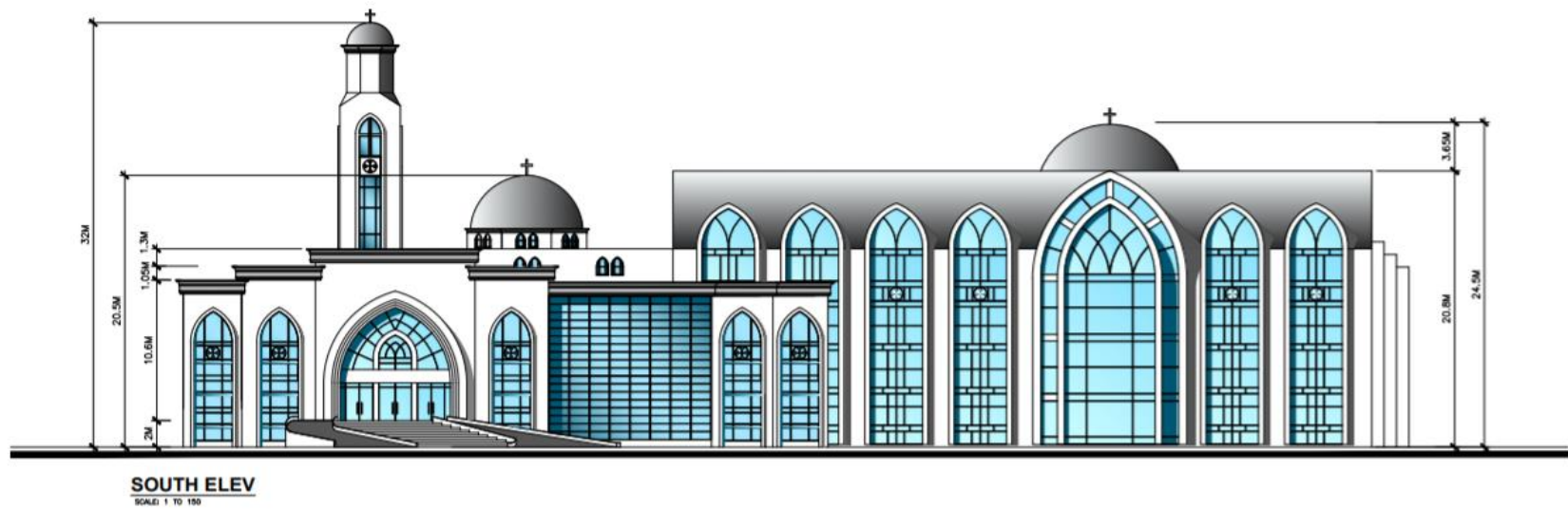
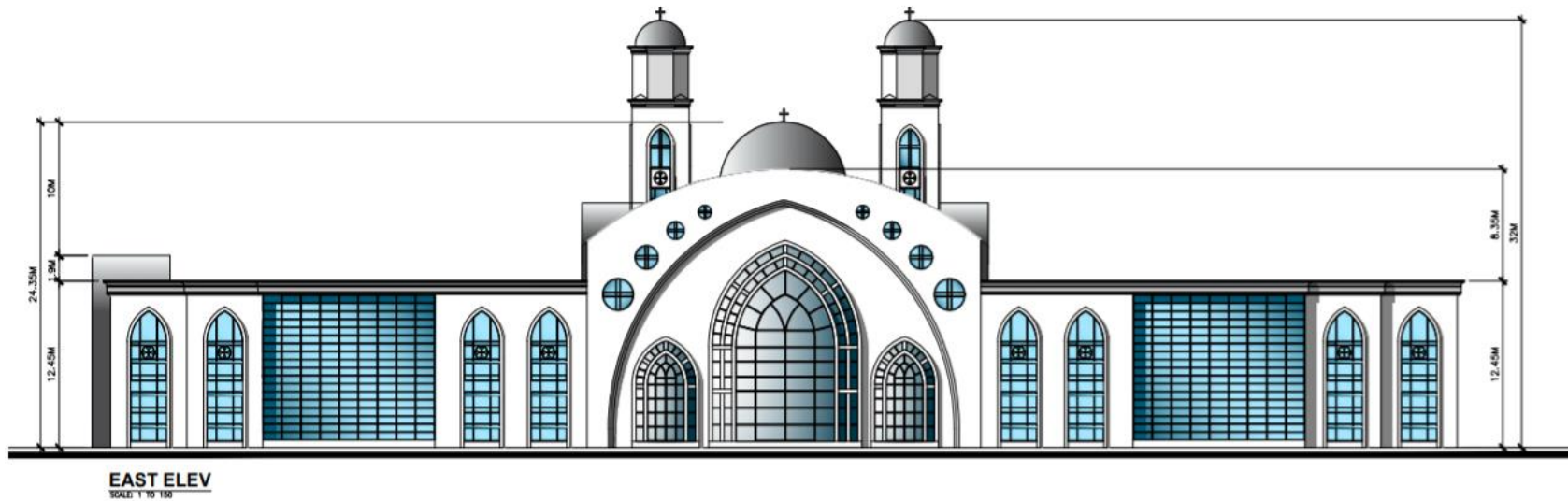
Site Plan, Phasing Plan and Elevations



Site Plan







Elevations

2. Site Description

Site Information

The subject property is located on the east side of Ninth Line (arterial road), south of Burnhamthorpe Road West within the Western Business Park Employment Area. The property is undeveloped with the southern portion being used by a tree care company for vehicle and equipment storage and the remainder for agricultural purposes. The land has a generally flat topography with the exception of a berm located adjacent to the Highway 403 corridor.



Aerial photo of 0 Ninth Line

The property is not connected to municipal sanitary or water servicing, and there are no immediate plans for the Region of

Peel to extend the necessary sanitary servicing. Connection to municipal water service can be made available.

Property Size and Use	
Frontage:	318 m (1,043.3 ft.)
Average Depth:	122 m (400.3 ft.)
Gross Lot Area:	3.9 ha (9.6 ac)
Existing Uses:	Vacant (outdoor storage and agriculture)



Image of existing conditions facing northeast from Ninth Line

Site History

- May 12, 2003 – Submission of Site Plan application under SP 03/204 W8 to permit a new ball hockey facility. The application was subsequently cancelled
- June 20, 2007 – Amendment Number 25 to Mississauga Plan is adopted by Council which establishes a policy

framework for the new zoning by-law (Zoning By-law 0225-2007), including special site policies permitting general commercial and cemetery uses on the subject property

- June 20, 2007 – Zoning By-law 0225-2007 came into force. The subject property is zoned **E2-93** (Employment - Exception) which restricts permitted uses to active recreational uses and a cemetery
- November 14, 2012 – Mississauga Official Plan came into force except for policies which have been appealed. The subject property is designated **Business Employment** in the Western Business Park Employment Area and is subject to a special site policy which permits a cemetery
- September 18, 2020 – Submission of rezoning application under OZ 20/016 W8 to permit a place of religious assembly (Phase 1) and a three-storey private community services centre (Phase 2)

3. Site Context

Surrounding Land Uses

The surrounding area generally remains undeveloped with the exception of employment lands to the immediate north and to the east. Immediately north of the subject property is a four storey self-storage facility. To the east of the subject property is the Highway 403 corridor and an industrial complex comprised of industrial and commercial uses, with surface parking. The surrounding area on the west side of the subject property and

Ninth Line is comprised of agricultural lands, outdoor storage and detached dwellings within the Town of Oakville. To the south of the subject property is undeveloped employment lands and a Provincially Significant Wetland (PSW).

The surrounding land uses are:

North: Self-storage facility (Access Storage)

East: Industrial complex across Highway 403

South: Vacant employment lands

West: Vacant agricultural lands and a detached dwelling across Ninth Line

Neighbourhood Context

The subject property is located along the western boundary of the Western Business Park Employment Area. The subject property, and the surrounding lands within the Western Business Park Employment Area, are designated primarily for employment purposes as well as to accommodate uses permitted by the *Parkway Belt West Plan*.

The lands located east of Highway 403 are developed and fully connected to municipal servicing. The majority of lands located between Highway 403 and Ninth Line, and between Dundas Street West and Burnhamthorpe Road West, remain undeveloped. This is most likely due to the lack of municipal services. Despite the absence of servicing, these lands are slowly redeveloping in the form of low intensity uses such as the existing self-storage facility (Access Storage), the sports facility (Ninth Line Sports Park) and a tennis facility (The Tennis School).



Aerial photo of 0 Ninth Line

Demographics

According to the 2019 Mississauga Employment database, the Western Business Park Employment Area contains 1,087 businesses (5% of city total) which employ 14,454 people. Employment sectors in the Western Business Park Employment Area include manufacturing, wholesale, retail, construction, professional services, accommodation and transportation. The Western Business Park Employment Area does not have a reported population.

Other Development Applications

There is one active development application in the general vicinity of the subject property being processed:

- OZ 18/018 W8 – 3855 Dundas Street West (northeast quadrant of Ninth Line and Dundas Street West) – application in process for a five storey self-storage facility and 2, two storey industrial condominium buildings

The following development application was approved in the immediate vicinity of the subject property:

- OZ 12/011 W8 – 3995 Ninth Line (southeast corner of Ninth Line and Burnhamthorpe Road West) – application approved for a four storey self-storage facility on October 28, 2015

Community and Transportation Services

Community and transportation services are limited in the surrounding area of the subject property. Ninth Line Sports Park is located approximately 1.2 kilometres (3,937 ft.) south of the subject property and provides two baseball diamonds. Access to the closest MiWay Transit Route, being Transit Route 36, requires an approximate 700 metres (2,296.6 ft.) walk east. Multi-use trails and bike lanes are not currently provided on the portion of Ninth Line between Dundas Street West and Burnhamthorpe Road West. This application is anticipated to have minimal impact on existing services in the community.

4. Summary of Applicable Policies, Regulations and Proposed Amendments

The *Planning Act* requires that Mississauga Official Plan be consistent with the *Provincial Policy Statement* and conform with the applicable provincial plans and Regional Official Plan. The policy and regulatory documents that affect this application have been reviewed and summarized in the table below. Only key policies relevant to the application have been included. The

table should be considered a general summary of the intent of the policies and should not be considered exhaustive. In the sub-section that follows, the relevant policies of Mississauga Official Plan are summarized. The development application will be evaluated based on these policies in the subsequent recommendation report.

Policy Document	Legislative Authority/Applicability	Key Policies
Provincial Policy Statement (PPS)	<p>The fundamental principles set out in the PPS apply throughout Ontario. (PPS Part IV)</p> <p>Decisions of the council of a municipality shall be consistent with PPS. (PPS 4.1)</p> <p>The Official Plan is the most important vehicle for implementation of the Provincial Policy Statement (PPS 4.6)</p>	<p>Settlement areas shall be the focus of growth and development. (PPS 1.1.3.1)</p> <p>Land use patterns within settlement areas will achieve densities and a mix of uses that efficiently use land, resources, infrastructure, public service facilities and transit. (PPS 1.1.3.2.a)</p> <p>Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment. (PPS 1.1.3.3)</p> <p>Where municipal sewage services and municipal water services or private communal sewage services and private communal water services are not available, planned or feasible, individual on-site sewage services and individual on-site water services may be used provided that site conditions are suitable for the long-term provision of such services with no negative impacts. In settlement areas, individual on-site sewage services and individual on-site water services may be used for infilling and minor rounding out of existing development.</p> <p>At the time of the official plan review or update, planning authorities should assess the long-term impacts of individual on-site sewage services and individual on-site water services on the environmental health and the character of rural settlement areas. Where planning is conducted by an upper-tier municipality, the upper-tier municipality should work with lower-tier municipalities at the time of the official plan review or update to assess the long-term impacts of individual on-site sewage services and individual on-site water services on the environmental health and the desired character of rural settlement areas and the feasibility of other forms of servicing set out in policies 1.6.6.2 and 1.6.6.3. (PPS 1.6.6.4)</p>

Policy Document	Legislative Authority/Applicability	Key Policies
		<p>Partial services shall only be permitted in the following circumstances:</p> <ul style="list-style-type: none"> a) where they are necessary to address failed individual on-site sewage services and individual on-site water services in existing development; or b) within settlement areas, to allow for infilling and minor rounding out of existing development on partial services provided that site conditions are suitable for the long-term provision of such services with no negative impacts (PPS 1.6.6.5) <p>Natural features and areas shall be protected for the long term. (PPS 2.1.1)</p>
<p><i>Growth Plan for the Greater Golden Horseshoe (Growth Plan)</i></p>	<p>The Growth Plan applies to the area designated as the Greater Golden Horseshoe growth plan area. All decisions made on or after May 16, 2019 in respect of the exercise of any authority that affects a planning matter will conform with this Plan, subject to any legislative or regulatory provisions providing otherwise. (Growth Plan 1.2.2)</p>	<p>Within settlement areas, growth will be focused in delineated built-up areas; strategic growth areas; locations with existing or planned transit; and, areas with existing or planned public service facilities. (Growth Plan 2.2.1.2 c)</p> <p>Complete communities will feature a diverse mix of land uses; improve social equity and quality of life; provide a range and mix of housing options; provide convenient access to a range of transportation options, public service facilities, open spaces and parks, and healthy, local and affordable food options; provide a more compact built form; mitigate and adapt to climate change impacts; and, integrate green infrastructure. (Growth Plan 2.2.1.4)</p> <p>Economic development and competitiveness in the GGH will be promoted by: a) making more efficient use of existing employment areas and vacant and underutilized employment lands and increasing employment densities; b) ensuring the availability of sufficient land, in appropriate locations, for a variety of employment to accommodate forecasted employment growth to the horizon of this Plan; c) planning to better connect areas with high employment densities to transit; and d) integrating and aligning land use planning and economic development goals and strategies to retain and attract investment and employment. (Growth Plan 2.2.5.1)</p> <p>Municipal water and wastewater systems and private communal water and wastewater systems will be planned, designed, constructed, or expanded in accordance with the following:</p> <ul style="list-style-type: none"> a) opportunities for optimization and improved efficiency within existing systems will be prioritized and supported by strategies for energy and water conservation and water demand management; b) the system will serve growth in a manner that supports achievement of the minimum intensification and density targets in this Plan; c) a comprehensive water or wastewater master plan or equivalent, informed by watershed planning or equivalent has been prepared to:

Policy Document	Legislative Authority/Applicability	Key Policies
		<p>i. demonstrate that the effluent discharges and water takings associated with the system will not negatively impact the quality and quantity of water;</p> <p>ii. identify the preferred option for servicing growth and development, subject to the hierarchy of services provided in policies 1.6.6.2, 1.6.6.3, 1.6.6.4 and 1.6.6.5 of the PPS, 2020, which must not exceed the assimilative capacity of the effluent receivers and sustainable water supply for servicing, ecological, and other needs; and</p> <p>iii. identify the full life cycle costs of the system and develop options to pay for these costs over the long-term.</p> <p>d) in the case of large subsurface sewage disposal systems, the proponent has demonstrated attenuation capacity; and</p> <p>e) plans have been considered in the context of applicable interprovincial, national, bi-national, or state-provincial Great Lakes Basin agreements or provincial legislation or strategies (Growth Plan 3.2.6.2)</p> <p>Municipalities will continue to protect any natural heritage features and areas in a manner that is consistent with the PPS and may continue to identify new systems in a manner that is consistent with the PPS. (Growth Plan 4.2.2.6)</p> <p>To achieve minimum intensification and density targets, municipalities will develop and implement urban design and site design official plan policies and other supporting documents that direct the development of high quality public realm and compact built form. (Growth Plan 5.2.5.6)</p>
Region of Peel Official Plan (ROP)	<p>The Region of Peel approved MOP on September 22, 2011, which is the primary instrument used to evaluate development applications. The proposed development applications were circulated to the Region who has advised that in its current state, the applications meet the requirements for exemption from Regional approval. Local official plan amendments are generally exempt from approval where they have had regard for the <i>Provincial Policy Statement</i> and applicable Provincial Plans, where the City Clerk has certified that processing was completed in accordance with the <i>Planning Act</i> and where the Region has advised that no Regional official plan amendment is required to accommodate the local official plan amendment. The Region provided additional comments which are discussed in Section 8 of this Appendix.</p>	<p>The ROP identifies the subject property as being located within Peel's Urban System.</p> <p>General objectives of ROP, as outlined in Section 5.3, include conserving the environment, achieving sustainable development, establishing healthy complete communities, achieving intensified and compact form and mix of land uses in appropriate areas that efficiently use land, services, infrastructure and public finances, while taking into account the characteristics of existing communities and services, and achieving an urban form and densities that are pedestrian-friendly and transit supportive.</p> <p>To establish healthy complete urban communities that contain living, working and recreational opportunities, which respect the natural environment, resources and the characteristics of existing communities (ROP 5.3.1.3)</p> <p>To achieve intensified and compact form and a mix of land uses in appropriate areas that efficiently use land, services, infrastructure and public finances while taking into account the characteristics of existing communities and services (ROP 5.3.1.4)</p>

Policy Document	Legislative Authority/Applicability	Key Policies
		<p>To support planning for complete communities in Peel that are compact, well-designed, transit-oriented, offer transportation choices, include a diverse mix of land uses, accommodate people at all stages of life and have an appropriate mix of housing, a good range of jobs, high quality open space, and easy access to retail and services to meet daily needs (ROP 5.5.1.6)</p> <p>To provide sufficient lands in employment area in Peel to support a vibrant and sustainable regional economy, to further the economic development goals of the area municipalities and to contribute to complete communities, in accordance with the policies in the provincial Growth Plan and in accordance with the policies of this Plan including the forecasts set out in Table 3 (ROP 5.6.1.1)</p> <p>To provide infrastructure and services that are required for the development of employment areas to facilitate economic development (ROP 5.6.1.2)</p> <p>Protect and support employment areas for employment uses, as defined and designated in area municipal official plans (ROP 5.6.2.6)</p> <p>To provide water supply and sanitary sewer services to appropriate areas of the region in an adequate, efficient, planned and cost-effective manner consistent with public needs and financial realities (ROP 6.4.1)</p> <p>Require and provide full municipal sewage and water services to accommodate growth in the Urban System to the year 2031, and the three Rural Services Centres to the year 2021. The provision of full municipal sewage and water services in the Urban System and the three Rural Services Centres will be subject to the Regional financial and physical capabilities (ROP 6.4.2.3)</p> <p>Continue to have water and/or sewer services provided by private or communal systems where appropriate, for existing and committed development as designated in the area municipal official plans (ROP 6.4.2.4)</p> <p>Ensure that the planning, construction, expansion, extension, operation and maintenance of water and sanitary sewer services protects the environmental system and natural resources of peel in a manner consistent with the objective and policies in this Plan, the Niagara Escarpment Plan, the Oak Ridges Moraine Conservation Plan and the Greenbelt Plan (ROP 6.4.2.7)</p>

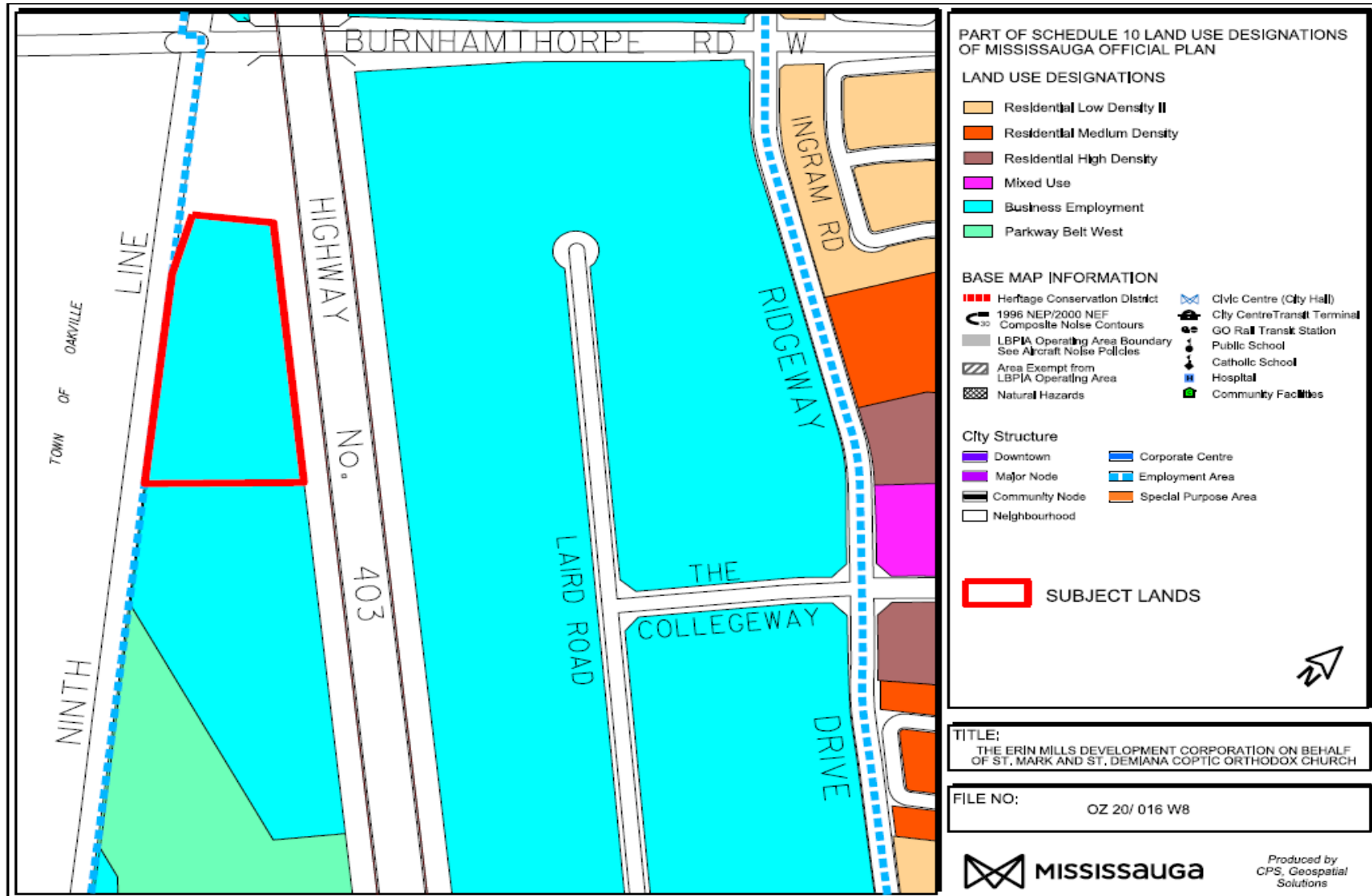
Mississauga Official Plan

The policies of Mississauga Official Plan (MOP) implement provincial directions for growth. MOP is generally consistent with the PPS and conforms with the Growth Plan, Greenbelt Plan, PBWP and ROP. An update to MOP is currently underway to ensure MOP is consistent with and conform to changes resulting from the recently released Growth Plan, 2019 and Amendment No. 1 (2020).

Existing Designation

The lands are located within the Western Business Park Employment Area and are designated **Business Employment**. The **Business Employment** designation permits a variety of commercial and industrial uses including a self-storage facility, commercial school, warehousing, distributing and wholesaling. Community infrastructure, which includes a place of religious assembly, is permitted in all designations of MOP. The subject property is also subject to Special Site Policy 3 which permits a cemetery use.

The subject property is not located within a Major Transit Station Area (MTSA).



Excerpt of Western Business Park Employment Area

Relevant Mississauga Official Plan Policies

The following policies are applicable in the review of this application. In some cases the description of the general intent summarizes multiple policies.

MOP Chapter	General Intent
Chapter 4 Vision	Mississauga will plan for a wide range of housing, jobs and community infrastructure resources so that they are available to meet the daily needs of the community through all stages of life (Section 4.4.6)
Chapter 5 Direct Growth	<p>Mississauga will protect employment lands to allow for a diversity of employment uses. (Section 5.1.8)</p> <p>New development will not exceed the capacity of existing and planned engineering services, transit services and community infrastructure. Development proposals may be refused if existing or planned servicing and/or infrastructure are inadequate to support the additional population and employment growth that would be generated or be phased to coordinate with the provision of services and infrastructure. (Section 5.1.9)</p> <p>Mississauga will maintain an adequate supply of lands for a variety of employment uses to accommodate existing and future employment needs. (Section 5.3.6.1)</p> <p>Mississauga will provide the necessary infrastructure to support existing and planned employment uses. (Section 5.3.6.4)</p> <p>Where feasible and appropriate, development will be encouraged to be transit supportive and minimize surface parking. (Section 5.3.6.7)</p> <p>Land uses serving the residential population of the city will be discouraged and only permitted where a use is beneficial to the city and cannot locate in a residential area due to land use conflicts or the unavailability of a suitable site. Acceptable locations for these uses will be identified through a municipal comprehensive review and local area plans. (Section 5.3.6.8)</p> <p>Additional development within Employment Areas will be permitted where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan. (Section 5.3.6.10)</p>
Chapter 6 Value The Environment	<p>Mississauga will require development proposals to address the management of stormwater using stormwater best management practices. (Section 6.2.7)</p> <p>Mississauga will encourage the use of green technologies and design to assist in minimizing the impacts of development on the health of the environment. (Section 6.2.8)</p> <p>Mississauga will use a water balance approach in the management of stormwater by encouraging and supporting measures and activities that reduce stormwater runoff, improve water quality, promote evapotranspiration and infiltration, and reduce erosion using stormwater best management practice. (Section 6.4.2.1)</p> <p>Mississauga will require that development applications be supported by stormwater best management practices in accordance with relevant plans, studies, development standards and policies. Additional measures may be specified by the City based on known concerns</p>

MOP Chapter	General Intent
	<p>related to storm sewer capacity, pollution prevention, flood risk and erosion, and protection of the city's Natural Heritage System, including its ecological function. Stormwater best management practices must be approved by the city, appropriate conservation authority and Provincial Government, where applicable. (Section 6.4.2.2)</p> <p>Surface drainage and stormwater management facilities will be installed for the safety of residents and to protect infrastructure and property. (Section 6.4.2.4)</p> <p>The design of storm drainage and stormwater management facilities will consider interim and ultimate development conditions. (Section 6.4.2.5)</p> <p>The design of stormwater management facilities and surface drainage facilities must conform to City standards, policies and guidelines. A buffer may be required as determined by the City. (Section 6.4.2.6)</p> <p>At-source controls should be provided to reduce the need for new stormwater infrastructure. All efforts to this effect should be guided by the appropriate environmental agencies, according to all Provincial Government, Regional Government and municipal policies, guidelines and regulations. (Section 6.4.2.7)</p> <p>Proposals for development and site alteration will incorporate appropriate buffers adjacent to watercourses, Natural areas and parks to protect against soil erosion and sediment impacts. (Section 6.6.2)</p> <p>Mississauga will establish site design standards that allow adequate flexibility in waste handling for development proposals. Standards will address a range of waste management options including on-site material separation, multiple waste streams and composting. (Section 6.9.3)</p>
Chapter 7 Complete Communities	<p>Mississauga will encourage the provision of services, facilities and housing that support the population living and working in Mississauga. (Section 7.1.1)</p> <p>Community infrastructure will support the creation of complete communities (Section 7.3.1)</p> <p>The preferred location for community infrastructure will be within the Downtown, Major Nodes, Community Nodes and Corridors. Where appropriate, community infrastructure may also be located within Neighbourhoods and Corporate Centres. Community infrastructure will generally not be located within Employment Areas. Where permitted within Employment Areas, these uses will be located along the periphery of Employment Areas. (Section 7.3.2)</p> <p>Community infrastructure that generates large amounts of traffic will be located to minimize impacts on the transportation system. (Section 7.3.4)</p> <p>Community infrastructure will generally be:</p> <ul style="list-style-type: none"> a. in proximity to transit facilities; b. on Corridors, major and minor collector roads, preferably at intersections; c. connected to trails, cycling facilities, where possible; d. in proximity to other community infrastructure and places of gathering, where possible; and e. accessible to persons with disabilities. (Section 7.3.5)

MOP Chapter	General Intent
	<p>The type of community infrastructure as well as its scale, design, layout and configuration permitted at any location, may be limited to ensure visual and functional compatibility with surrounding development. (Section 7.3.7)</p> <p>Where possible, community infrastructure will be encouraged to develop shared parking facilities. (Section 7.3.8)</p> <p>The preferred locations for places of religious assembly will be the Downtown, Major Nodes and Community Nodes. Otherwise, places of religious assembly will be encouraged to locate in Corridors, preferably at their intersections. (Section 7.3.11)</p>
Chapter 9 Build A Desirable Urban Form	<p>Mississauga will develop an urban form based on the urban system and the hierarchy identified in the city structure as shown on Schedule 1: Urban System. (Section 9.1.1)</p> <p>Development within Employment Areas and Special Purpose Areas will promote good urban design that respects the function of the area. (Section 9.1.4)</p> <p>Mississauga will transform the public realm to create a strong sense of place and civic pride. (Section 9.1.8)</p> <p>The city vision will be supported by site development that:</p> <ul style="list-style-type: none"> a. respects the urban hierarchy; b. utilizes best sustainable practices; c. demonstrates context sensitivity, including the public realm; d. promotes universal accessibility and public safety; and e. employs design excellence. (Section 9.1.10) <p>Development will contribute to pedestrian oriented streetscapes and have an urban built form that is attractive, compact and transit supportive. (Section 9.2.1.21)</p> <p>Development will face the street (9.2.1.24)</p> <p>Buildings should have active façades characterized by features such as lobbies, entrances and display windows. Blank building walls will not be permitted facing principal street frontages and intersections.(Section 9.2.1.25)</p> <p>For non-residential uses, at grade windows will be required facing major streets and must be transparent. (Section 9.2.1.26)</p> <p>Built form will relate to and be integrated with the streetline, with minimal building setbacks where spatial enclosure and street related activity is desired. (Section 9.2.1.28)</p> <p>Development will have a compatible bulk, massing and scale of built form to provide an integrated streetscape. (Section 9.2.1.29)</p> <p>Buildings and streetscapes will be situated and designed so as to encourage pedestrian circulation. (Section 9.2.1.35)</p> <p>Streetscape improvements including trees, pedestrian scale lighting, special paving and street furniture in sidewalks, boulevards, open</p>

MOP Chapter	General Intent
	<p>spaces and walkways, will be coordinated and well designed. (Section 9.2.1.36)</p> <p>Developments should minimize the use of surface parking in favour of underground or aboveground structured parking. All surface parking should be screened from the street and be designed to ensure for natural surveillance from public areas. Aboveground structured parking should be lined with residential, commercial or office uses. (Section 9.2.1.37)</p> <p>Heights in excess of four storeys will be required to demonstrate that an appropriate transition in height and built form that respects the surrounding context will be achieved. (Section 9.2.2.1)</p> <p>Employment Areas adjacent to residential areas, sensitive land uses and major roads will be required to meet higher standards of design and to mitigate adverse impacts on adjacent uses. (Section 9.2.2.4)</p> <p>The design of developments at intersections and along major streets should be of a highly attractive urban quality, recognizing that streets are important civic spaces and linkages. (Section 9.3.1.8)</p> <p>Sites with prominence, high visibility and access should be considered as a priority for civic buildings and community infrastructure. (Section 9.3.3.3)</p> <p>Buildings that serve the community such as places of religious assembly, colleges and hospitals, should be designed to be the focus of the community, highly visible, universally accessible and attractive and serve as landmarks for future generations. (Section 9.3.3.4)</p> <p>The design of all development will foster the improvement of connections and accessibility for transit users and promote active transportation modes. (Section 9.4.1.1)</p> <p>Development will support transit and active transportation by:</p> <ul style="list-style-type: none"> a. locating buildings at the street edge, where appropriate; b. requiring front doors that open to the public street; c. ensuring active/animated building façades and high quality architecture; d. ensuring buildings respect the scale of the street; e. ensuring appropriate massing for the context; f. providing pedestrian safety and comfort; and g. providing bicycle destination amenities such as bicycle parking, shower facilities and clothing lockers, where appropriate. (Section 9.4.1.3) <p>All development will be consistent with the Mississauga Accessibility Design Handbook. (Section 9.4.3.2)</p> <p>Buildings and site design will be compatible with site conditions, the surrounding context and surrounding landscape of the existing or planned character of the area. (Section 9.5.1.1)</p> <p>Site designs and buildings will create a sense of enclosure along the street edge with heights appropriate to the surrounding context. (Section 9.5.1.3)</p>

MOP Chapter	General Intent
	<p>Buildings with exposure to Provincial Highways or public streets in areas of site plan control will be subject to a higher standard of design to achieve upgraded building elevations and landscaping, including principal doors and window fenestration. (Section 9.5.1.13)</p> <p>Sites that have exposure to parks or double exposure to both Provincial Highways and public streets will be required to be designed with upgraded building elevations and landscaping facing all parks, public highways and public streets. (Section 9.5.1.14)</p> <p>Developments will be sited and massed to contribute to a safe and comfortable environment for pedestrians by:</p> <ul style="list-style-type: none"> a. providing walkways that are connected to the public sidewalk, are well lit, attractive and safe; b. fronting walkways and sidewalks with doors and windows and having visible active uses inside; c. avoiding blank walls facing pedestrian areas; and d. providing opportunities for weather protection, including awnings and trees. (Section 9.5.2.2) <p>Development proponents may be required to upgrade the public boulevard and contribute to the quality and character of streets and open spaces by providing:</p> <ul style="list-style-type: none"> a. street trees and landscaping, and relocating utilities, if required; b. lighting; c. weather protection elements; d. screening of parking areas; e. bicycle parking; f. public art; and g. street furniture. (Section 9.5.2.5) <p>Site development will be required to:</p> <ul style="list-style-type: none"> a. incorporate stormwater best management practices; b. provide enhanced streetscape; c. provide landscaping that complements the public realm; d. include the use of native non-invasive plant material; e. protect and enhance habitat; f. preserve significant trees on public and private lands; g. incorporate techniques to minimize urban heat island effects such as providing planting and appropriate surface treatment; and h. provide landscaping that beautifies the site and complements the building form. (Section 9.5.2.11) <p>Development on a site may be phased provided that the location of buildings and services allow for future development. For projects that will be phased, applications shall be accompanied by a detailed phasing plan. (Section 9.5.2.14)</p> <p>Buildings will be designed to create a sense of identity through the site layout, massing, forms, orientation, scale, architectural features, landscaping and signage. (Section 9.5.3.1)</p> <p>Street facing façades should have the highest design quality. Materials used for the front façade should be carried around the building where any façades are exposed to the public view at the side or rear. (Section 9.5.3.6)</p>

MOP Chapter	General Intent
	<p>Where appropriate, development should be designed to incorporate measures that minimize urban heat island effects. (Section 9.5.3.13) Buildings should be designed to conserve energy and incorporate sustainable material. (Section 9.5.3.14)</p> <p>Buildings should be designed to minimize the consumption of water and to utilize stormwater best management practices. (Section 9.5.3.15)</p> <p>Buildings should coordinate and integrate vehicular and servicing access to minimize their visual prominence. (Section 9.5.3.16)</p> <p>Mechanical equipment, vents and metering devices will be integrated into the building design and will not be visible from the public realm. (Section 9.5.3.17)</p> <p>Rooftop mechanicals and appurtenances will be integrated into building design and will not be visible from the public realm and residential developments. (Section 9.5.3.18)</p> <p>Parking should be located underground, internal to the building or to the rear of buildings. (Section 9.5.5.1)</p> <p>Where surface parking is permitted, the following will apply. Parking should:</p> <ul style="list-style-type: none"> a. not be located between the building and the street; b. incorporate stormwater best management practices, such as, permeable paving, bioretention areas and tree clusters; c. provide safe and legible raised walkways, with curb ramps, within parking areas to buildings and streets; d. incorporate universal design principles; e. be configured to permit future development; f. have appropriate landscape treatment including trees and lighting, throughout parking lots; g. provide appropriate landscape treatment to provide shading of parking areas; and h. provide landscape buffering at the street edge. (Section 9.5.5.3)
Chapter 10 Foster a Strong Economy	<p>Mississauga will ensure that full Regional and municipal services are available to serve all development. (Section 10.6.5)</p> <p>Mississauga expects that the Region of Peel will provide appropriate and timely water, and wastewater facilities to serve the city's development. (Section 10.6.9)</p> <p>Mississauga encourages appropriate land use and development patterns that are transit supportive, site and building designs that conserve energy and opportunities for district energy. (Section 10.7.5)</p> <p>Mississauga encourages the creation of innovative strategies such as green site design and green buildings, which utilize technology such as green roofs, white roofs and the use of the urban tree canopy to achieve energy efficiencies. (Section 10.7.6)</p>
Chapter 11 General Land Use Designations	<p>Permitted uses in all designations include community infrastructure, community gardening and parkland. (Section 11.2.1.1)</p> <p>In addition to the Uses Permitted in all Designations, lands designated Business Employment will also permit the following uses:</p> <ul style="list-style-type: none"> • Banquet hall; • Commercial school; and • Secondary office. (Section 11.2.11)

MOP Chapter	General Intent
	<p>Permitted uses will operate mainly within enclosed buildings. (Section 11.2.11.3)</p> <p>Accessory uses will generally be limited to a maximum of 20% of the total Gross Floor Area. (Section 11.2.11.4)</p> <p>All accessory uses should be on the same lot and clearly subordinate to and directly related to the functioning of the permitted use (Section 11.2.11.5)</p>
Chapter 17 Employment Areas	<p>For lands with direct exposure to Provincial Highway 403, the following design policies will be used to evaluate the design aspects of development proposals:</p> <ul style="list-style-type: none"> a. limited parking will be permitted between the building and Provincial Highway 403 property line; and b. upgraded building elevations will be required for lots abutting Provincial Highway 403, Ridgeway Drive and Ninth Line. (Section 17.9.1.1) <p>Loading bays and waste collection areas should not face onto Provincial Highway 403, Ridgeway Drive, Winston Churchill Boulevard, Dundas Street West or Ninth Line. (Section 17.9.1.2)</p> <p>The lands identified as Special Site 3 are located on the east side of Ninth Line, south off Burnhamthorpe Road West. (Section 17.9.3.3.1)</p> <p>Notwithstanding the provisions of the Business Employment designation, cemetery use will be permitted. (Section 17.9.3.3.2)</p>
Chapter 19 Implementation	<p>To provide consistent application of planning and urban design principles, all development applications will address, among other matters:</p> <ul style="list-style-type: none"> a. the compatibility of the proposed development to existing or planned land uses and forms, including the transition in height, density, and built form; b. conformity with the policies in this Plan; [...] e. the adequacy of engineering services; [...] (Section 19.4.3) <p>A holding provision may be used in conjunction with any zoning category to specify the use to which lands, buildings or structures may be put at such time in the future as the holding provision is removed by amendment to the zoning by-law. (Section 19.7.1)</p> <p>A holding provision will be used to implement this Plan for staging of development and additionally may be used to implement specific requirements, such as, but not limited to:</p> <ul style="list-style-type: none"> a. the adequacy of engineering services; [...] (Section 19.7.2)

Mississauga Zoning By-law

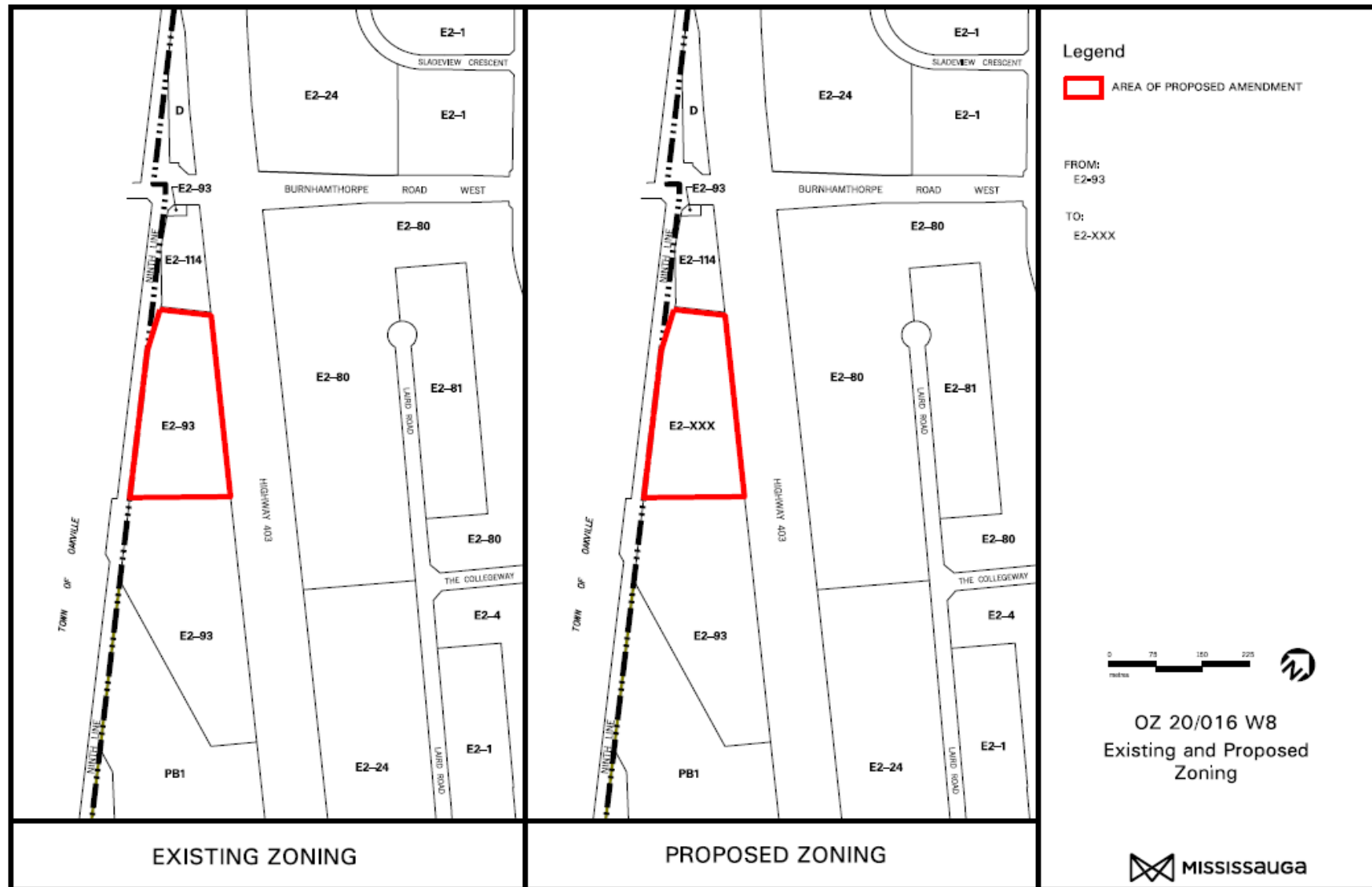
Existing Zoning

The subject property is currently zoned **E2-93** (Employment - Exception), which only permits active recreational uses and a cemetery.

Proposed Zoning

The applicant is proposing to zone the property to **E2-Exception** (Employment - Exception) to permit the place of religious assembly (Phase 1) and a three storey private community services centre (Phase 2) and set forth a maximum gross floor area, a minimum number of parking spaces and other technical performance standards. An "H" holding provision is also proposed to be added to the Phase 2 portion of the subject property, and subsequently removed when arrangements are made to the satisfaction of the appropriate agencies with respect to the provision of adequate services and connection to the municipal system being provided.

Through the processing of the application staff may recommend a more appropriate zone category for the development in the Recommendation Report.



Excerpt of Zoning Map 59
Proposed Zoning Regulations

Zone Regulations	Existing Zone Regulations	Proposed Amended Zone Regulations
Permitted Uses	Active recreational uses and cemetery	Place of religious assembly, office, medical office, commercial school, food bank and other community supportive uses to be confirmed
Maximum Gross Floor Area	No maximum	11,600 m ² (124,861 ft ²) for a place of religious assembly 10,850 m ² (116,788 ft ²) for E2 uses
Required Parking Spaces	Place of religious assembly (Phase 1): 535 spaces Private community services centre (Phase 2): 190 spaces	Place of religious assembly (Phase 1): 427 spaces Private community services centre (Phase 2): 96 spaces
Holding Provision	No "H" holding provision	"H" holding provision to be added to the Phase 2 lands, and subsequently removed when arrangements are made to the satisfaction of the appropriate agencies with respect to the provision of adequate services and connection to the municipal system being provided
	Note: The provisions listed are based on information provided by the applicant, which is subject to revisions as the application is further refined. In addition to the regulations listed, other minor and technical variations to the implementing by-law may also apply, including changes that may take place before Council adoption of the by-law, should the application be approved.	

5. Community Questions and Comments

No community meetings were held and no written comments were received by the Planning and Building Department.

6. Development Issues

The following is a summary of comments from agencies and departments regarding the application:

Agency / Comment Date	Comments
Region of Peel (January 20, 2021)	<p><u>Servicing</u></p> <p>Before the Region can provide comments on the water servicing proposal, the applicant and the City need to collectively confirm partial servicing will be permitted to service the subject property.</p> <p><u>Waste</u></p> <p>For the private community services centre, on-site waste collection will be required through a private waste hauler. For the place of worship, the Region will provide front-end collection of garbage and semi-automated collection of recyclable materials. A waste management plan providing specific criteria for maneuvering, collection points and storage must be submitted prior to approval of the application.</p> <p><u>Public Health</u></p> <p>The Region has no objections to the application but has offered comments for consideration during the site plan stage including the provision of car share parking spaces and bicycle parking, use of permeable paving and inclusion of pedestrian amenities such as benches and lighting along pathways.</p>
Halton Region (February 23, 2021)	<p>Halton Region requests that the City ensure that the matters of interest identified and detailed below are resolved. These are generally summarized as follows:</p> <p><u>Transportation</u></p> <p>A Transportation Impact Study to the satisfaction of the Region of Halton should be provided and any related matters be addressed to the Region's satisfaction.</p> <p>The concept plan should be revised to reflect any road widening requirements that are known at this time.</p>

Agency / Comment Date	Comments
	<p><u>Servicing</u></p> <p>Matters related to partial servicing and potential impacts on groundwater and environmental receptors in Halton, should be addressed. The Region requests confirmation through an MECP approval letter and relevant hydrogeology reports, and the functional servicing report that lands/uses within Halton (in particular private wells and environmental lands) are not impacted and also that the proposed system is able to handle expected flows.</p> <p>The Region does not support post development storm drainage from the site draining to the Region's storm drainage system. The Functional Servicing Report and any related plans should be revised to show a post development stormwater drainage scheme that does not have an outfall to the Ninth Line drainage swale.</p> <p>Any ground development infrastructure should be located outside of the Regional right-of-way.</p> <p><u>Environment</u></p> <p>The Planning Justification Report should identify the RNHS features/Provincially Significant Wetland located across from the subject site in Halton Region and address how Provincial Policy under the PPS and ROP policy is satisfied.</p>
Dufferin-Peel Catholic District School Board and the Peel District School Board	No comments received on the application.
City Community Services Department – Park Planning Section (July 2021)	<p>In the event that the application is approved, the Community Services Department - Park Planning note the following conditions.</p> <p>In comments dated July, 2021, this Department requires the payment of street tree cash contributions for the Ninth Line public street frontage. This Department also notes that prior to the issuance of building permits for each lot or block cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the Planning Act (R.S.O. 1990, c.P. 13, as amended) and in accordance with City's Policies and By-laws.</p>
City Community Services Department - Arborist – Private Property Section (February 23, 2021)	Submission of a Tree Removal application is required for the proposed injury and removal of trees on the subject property. The approval of the Tree Permission application is required prior to the earliest of the Demolition Permit/the Erosion and Sediment Control Permit/Site Plan approval. The Tree Removal application is to be submitted to Urban Forestry, and will be issued when the drawings are approved, securities provided and the protective hoarding is installed, inspected and approved by an Urban Forestry representative.
Economic Development Office (December 18, 2020)	Economic Development have no comments or concerns on the application.
City Transportation and Works Department (July, 2021)	<p>Technical reports and drawings have been submitted and are under review to ensure that engineering matters related to noise, grading, servicing, stormwater management, traffic and environmental compliance can be satisfactorily addressed to confirm the feasibility of the project, in accordance with City requirements.</p> <p>Based on a review of the materials submitted to date, the owner has been requested to provide additional technical details</p>

Agency / Comment Date	Comments
	<p>and revisions prior to the City making a recommendation on the application, as follows:</p> <p><u>Stormwater</u></p> <p>A Functional Servicing Report (FSR), prepared by R. J. Burnside dated March 2020, was submitted in support of the proposed development. The purpose of the report is to evaluate the proposed development impact on the municipal drainage system (e.g. storm sewers, etc.) and to mitigate the quality and quantity impacts of stormwater run-off generated from the site. Mitigation measures may include improvements to existing stormwater servicing infrastructure, new infrastructure and/or on-site stormwater management controls. The applicant is proposing to connect to the existing ditches on Ninth Line to service the development lands and on-site stormwater management controls for the post-development discharge.</p> <p>The applicant is required to provide further technical information to:</p> <ul style="list-style-type: none"> • demonstrate the feasibility of the storm outlet ; • develop an acceptable strategy to accommodate external drainage from the adjacent property, if any; • demonstrate that there will be no impact on the City's existing drainage system including how groundwater will be managed on-site; and • approval from other agencies will be required (Town of Oakville and MTO). <p><u>Traffic</u></p> <p>The applicant is required to provide the following information as part of subsequent submissions, to the satisfaction of the Transportation and Works Department:</p> <ul style="list-style-type: none"> • Provide a Traffic Impact Study (prior to the preparation of a Recommendation Report); • Provide turning movement diagrams to evaluate the internal site circulation and access points; • Provide accessible and secure short term (outdoor) and long term (indoor) bicycle storage facilities on site; • Confirmation of the approval from Halton Region. This section of Ninth Line is under the jurisdiction of Halton Region; • Confirmation of the approval from the Ministry of Transportation (MTO), due to the proximity to the Highway. <p><u>Environmental Compliance</u></p> <p>Phase One ESA dated April 2020, prepared by R.J. Burnside & Associates Limited has been received. The results of the Phase One ESA indicate that further investigation is required.</p> <p>A Record of Site Conditions (RSC) is required to be filed for the property in accordance with MECP regulations.</p> <p>In addition, the applicant is required to provide the following information as part of the next submission:</p> <ul style="list-style-type: none"> • Reliance for the Phase One ESA • Phase Two ESA

Agency / Comment Date	Comments
	<ul style="list-style-type: none"> • Specific references for land dedication • Clarification regarding removal/reuse of the fill (berm) material on the property • The Temporary Discharge to Storm Sewer Commitment Letter <p>Noise</p> <p>The Noise Study evaluates the potential impact to and from the development, and recommends mitigation measures to reduce any negative impacts. Noise sources that may have an impact on this development include road traffic and stationary sources from this site with the potential to impact noise sensitive land uses in the vicinity. Noise mitigation will be required.</p> <p>Engineering Plans/Drawings</p> <p>The applicant has submitted a number of technical plans/drawings (i.e. Grading and Servicing Plans), which need to be revised in accordance with City Standards and as part of subsequent submissions.</p>
Fire Prevention – Fire Prevention Plan Examination (January 25, 2021)	Additional information regarding connection to the 300 mm (0.9 feet) watermain, OBC compliance, fire department access and fire hydrant coverage, and sprinkler/standpipe protection is required.
Conservation Halton (February 3, 2021)	<p>The subject property is not regulated by Conservation Halton and so permission from Conservation Halton is not required. Staff have reviewed the Functional Servicing and Stormwater Management Report, prepared by R.J. Burnside & Associates Ltd., dated March 2020, and provide the following advisory comments:</p> <p>Stormwater</p> <p>In accordance with the Stormwater Management Planning and Design Manual (MOE, 2003), wet ponds require a minimum drainage area of 5 hectares (538,195.5 square feet) to sustain the permanent pool. Considering the proposed drainage area is 3.75 hectares (403,646.6 square feet), other stormwater management (SWM) types should be considered.</p> <p>Coordination with the City of Mississauga is recommended to discuss feasibility of providing stormwater management treatment on site for external areas from Ninth Line right-of-way, including the site area that will be dedicated to the City for future road widening of Ninth Line.</p> <p>Based on the above, Conservation Halton staff has no objection to the approval of this application and defers further review to municipal staff unless additional review from Conservation Halton is requested.</p>
Enbridge (February 25, 2021)	<p>The subject property is contained in a Legacy Enbridge Gas area, however, will be serviced via a Legacy Union Gas Pipeline.</p> <p>The subject property will require a road crossing, and possibly a Conservation Halton permit. A request for gas must be submitted.</p> <p>Enbridge also owns and operates a high pressure crude oil pipeline in the surrounding area. Ground disturbance activities in the prescribed area (CER) which extends 30 metres (98.4 feet) from each side of the centerline of the pipeline requires notification to Enbridge. All proposed facilities crossing the pipeline right-of-way require approval from Enbridge in the form</p>

Agency / Comment Date	Comments
Union Gas (January 18, 2021)	<p>of a crossing agreement between Enbridge and the facility owner.</p> <p>Enbridge Gas Inc., operating as Union Gas, does have service lines running within the area which may or may not be affected by the proposed development. Should the proposed development impact these services, it may be necessary to terminate the gas service and relocate the line according to the new property boundaries. Any service relocation required would be at the cost of the property owner. If there is any work (i.e. underground infrastructure rebuild or grading changes...) at the easement and on/near any of our existing facilities, please contact Union as early as possible so we can exercise engineering assessment of your work. The purpose is to ensure the integrity of our main is maintained and protected.</p>
Ministry of Transportation (February 23, 2021)	<p>The subject property is located within the permit control area and an MTO Building and Land Use Permit is required prior to the start of construction.</p> <p>The MTO has no objection to the application however, request that the applicant address the site plan and FSR comments below:</p> <p><u>Site Plan</u></p> <p>The site plan shows a significant number of required parking spaces within the 14 metre (45.9 feet) setback limits. All above and below grade structures, including required parking spaces and loading areas must be located beyond the 14 metre (45.9 feet) setback limits.</p> <p>A site lighting plan and traffic brief will be required at the site plan stage for MTO's review.</p> <p><u>Servicing and Stormwater</u></p> <p>Detailed site servicing, grading and erosion and sediment control plans should be provided at the detailed design stage for MTO's review.</p> <p>Indicate MTO's 14 metre (45.9 feet) setback line on all drawings as a swale from the leaching bed, retaining wall and parking spots are identified towards the Highway 403 corridor.</p> <p>Provide 5, 10, 25, 50 and 100 year post and pre development flows to MTO's drainage system and their outlet.</p> <p>It is noted that the stormwater management facility will be converted to a parking lot in Phase 2. MTO doesn't allow chamber system storage and if the proponent insists on providing such a storage system then calculations should be provided by taking out such storage from the model and confirming that MTO's drainage system will not be impacted.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> - Canada Post Corporation - Alectra Utilities - Union Gas <p>The following City Departments and external agencies were circulated the applications but provided no comments:</p>

Agency / Comment Date	Comments
	<ul style="list-style-type: none"> - Realty Services - Town of Oakville - Community Services - Heritage Planning - Rogers Cable - Greater Toronto Airport Authority - TransCanada Pipeline - Trillium Health Partners - Community Services - Arborist – City Property - Community Services - Public Art Coordinator - Arborist - Streetscape

Development Requirements

There are engineering matters including: grading, environmental, engineering, servicing and stormwater management that will require the applicant to enter into agreements with the City. Prior to any development proceeding on-site, the City will require the submission and review of an application for site plan approval.

7. Section 37 Community Benefits (Bonus Zoning)

Section 37 community benefits (bonus zoning) is not considered applicable for the current proposal as no official plan amendment is required and the net increase in height and density above existing zoning permissions does not meet the eligibility requirements of Corporate Policy 07-03-01 – Bonus Zoning.

8. Next Steps

Based on the comments received and the applicable Mississauga Official Plan policies, the following matters will have to be addressed:

- Is the proposed partial servicing of the subject property, which entails a private sanitary system and a municipal water connection, appropriate?
- Is the proposed development premature given the absence of full municipal servicing?
- Have technical matters including but not limited to stormwater management been addressed to the satisfaction of the City, and relevant authorities including Peel Region and Halton Region?
- Is the proposed development compatible with the existing

and planned character of the area given the proposed massing, setbacks, building configuration and building height?

- Are the policies and principles of Mississauga Official Plan maintained by the proposed development?
- Are the proposed zoning by-law exception standards, including reduced parking standards, appropriate?

Upon satisfying the requirements of various City departments and external agencies, the Planning and Building Department will bring forward a recommendation report to a future Planning and Development Committee meeting. It is at this meeting that the members of the Committee will make a decision on the application.