

# 167 Lakeshore Road West

Scoped Heritage Impact Assessment- July 12, 2021, Revised



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## Executive Summary

This *Scoped Heritage Impact Assessment* has been undertaken to determine the impacts of the proposed exterior alterations to 167 Lakeshore Road West. The intention of this review is to determine if the alterations will have any negative impacts on the heritage resource and neighbouring properties identified by the City of Mississauga as:

1. 167 Lakeshore is designated as “Other” within the Old Port Credit Village Heritage Conservation District Plan (HCD).

2. The adjacent property to the east, identified as Clarke Hall, is designated under Part IV of the Ontario Heritage Act.

The assessment included the following undertakings by the W.E. Oughtred & Associates Inc.:

- field review of the subject property
- review of existing historical information
- review of existing heritage evaluations
- review of relevant heritage policies
- consultation with Alexandra Conibear, designer for Carlo’s Bake Shop.
- consultation with Matthew Wilkinson, Heritage Mississauga
- evaluation of the property against the Old Port Credit Heritage Conservation District Plan.

Based on this assessment, it has been determined that the proposed alterations will have no negative impact on the subject property or the adjacent property, Clarke Hall. Further, no negative influences; be that setting, character and use of the lands, will be realized as a result of the proposed development. Further, the proposed alterations are in keeping with the policies and guidelines outlined in Sections 7.0 and 8.0 of the Old Port Credit HCD Plan.

## Introduction

This Heritage Impact Assessment is scoped according to direction provided by heritage planning staff at the City of Mississauga. The field review was conducted on July 12th, 2021.



## Location & Site Description

The subject property is a one-storey yellow brick building built prior to 1966. The property has two units, each with a separate address; 167 and 169 Lakeshore Road West. 169 Lakeshore Road West is occupied by a real-estate office. The proposal for the subject property, 167 Lakeshore Road West, is a bakery, Carlo's Bake Shop. Shared parking is provided at the rear of the property for both premises.

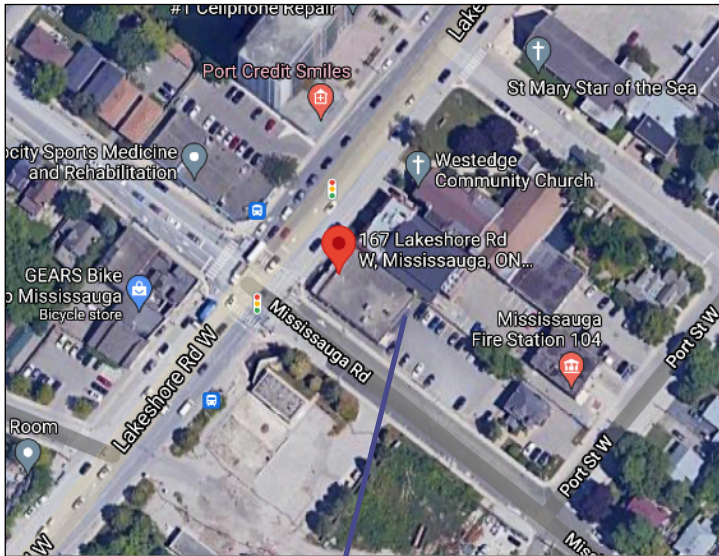
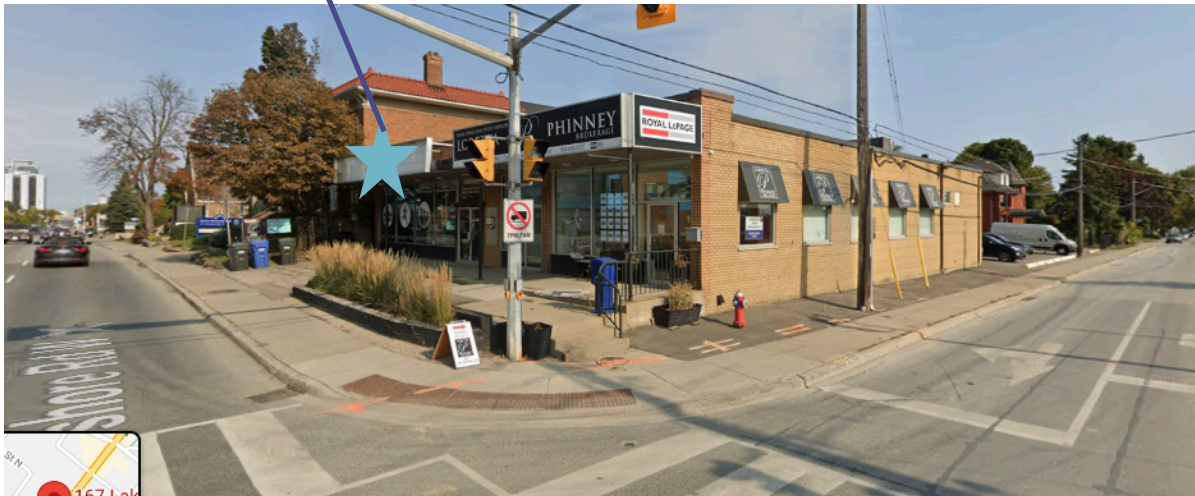


Photo1: Site Location

Subject building

Photo 2: Corner Street View

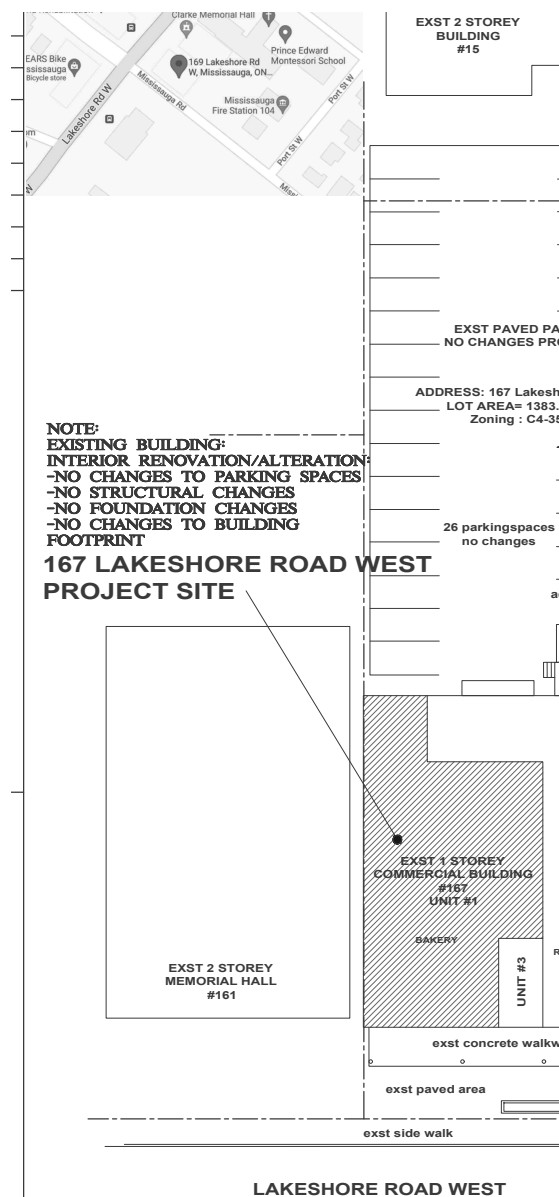


The subject property is a one-storey yellow brick building built prior to 1966. The property has two units, each with a separate address, 167 and 169 Lakeshore Road West.

169 Lakeshore Road West is occupied by a real-estate office. The proposal for the subject property, 167 Lakeshore Road West, is a bakery, Carlo's Bake Shop. Parking is provided at the rear of the property.

There are no changes to the existing site plan. Changes are proposed to the front elevation only, and interior alterations for the change of use.

Figure 1: Existing Site Plan



## Heritage Designation

The subject property has an Old Port Credit HCD plan classification as “other”, as per the attached excerpt from the plan.

### OLD PORT CREDIT VILLAGE HERITAGE CONSERVATION DISTRICT PROPERTY INVENTORY 2018

<b>1.0 ADDRESS</b>	<b>167-169 Lakeshore Road West</b>
<b>2.0 LOT</b>	
2.1 Land use designation	Convenience Commercial
2.2 Period of construction	1960s
2.3 Zoning	C4-35
2.4 Lot size (m <sup>2</sup> )	1005
2.5 Building front yard setback (m)	3.81
2.6 Building side yard setback (m)	0 / 0
<b>3.0 LANDSCAPE / SETTING / CONTEXT</b>	
3.1 Trees and shrubs	n/a
3.2 Soft landscaping	n/a
3.3 Driveways and parking	Asphalt parking lot
3.4 Landscape/property features	n/a
3.5 Fencing	n/a
<b>4.0 ARCHITECTURE</b>	
4.1 Building type	Single-story strip mall
4.2 Building size (m <sup>2</sup> )	565
4.3 Wall assembly / cladding	Brick
4.4 Roof shape / pitch / material	Flat
4.5 Storeys	1
4.6 Alterations	
4.7 Architectural style	Commercial shop front
<b>5.0 HERITAGE</b>	
5.1 Current status/designation	Designated under Part V
5.2 HCD plan classification	Other
5.3 Heritage notes	n/a
5.4 Heritage attributes	n/a

Figure 2: Old Port Credit HCD Designation<sup>1</sup>

<sup>1</sup> Old Port Credit District Heritage Plan

Historically the subject building has operated as commercial space. However, its last use was a medical office; Velocity Sports Medicine & Rehabilitation. The proposal today is for a take-out restaurant, Carlo's Bake Shop. A historical image of the site (1980) is included below.



Photo 3: 1980 Site Photo, Image reference G597, City of Mississauga

At the time this photo was taken in 1980 the building housed a branch of the Royal Bank. Several other business have been located in the building, which, as of 2010, was a commercial building housing Adobe Home Collection and Hatty's Bar & Grill.<sup>2</sup>

The subject property is adjacent to Clarke Hall, a designated property. Clarke Memorial Hall is one of the key buildings within the Old Port Credit Village Heritage Conservation District, forming part of a significant institutional block on Lakeshore Road. Its size, location along the west bank of the Credit River and architecture make this building a landmark structure.<sup>3</sup>

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<sup>2</sup> City of Mississauga, Historic Images Gallery

<sup>3</sup> [historicplaces.ca](http://historicplaces.ca)





Photo 4: 1975 Photo, Image Reference: MI0069, City of Mississauga (1975)

Erected by Mary Louise Clarke in 1922 as a memorial to her husband, Alfred Russel Clarke. He had died of pneumonia contracted as a result of the sinking in 1915 of the Lusitania, on which he was a passenger. The Hall was donated to the United Church and used as a Sunday School. During the Depression it was handed over to the Village of Port Credit and used as a Community Hall. It was the site of the Port Credit Municipal Offices from 1941 to 1974. Since then it has been used for meetings, auctions and various events, and as a theatre. The Hall is built in Spanish Colonial Revival style.<sup>4</sup>

## Proposal

The proposal is the establishment of a take-out restaurant, Carlo's Bake Shop, in the subject property. Carlo's is an American bakery chain, expanding to Canada. The store in Port Credit is the first Canadian location. In keeping with the style of the original location in Hoboken, New Jersey, the Port Credit store-front will maintain aspects the iconic look.

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<sup>4</sup> <sup>4</sup> City of Mississauga, Historical Image Gallery



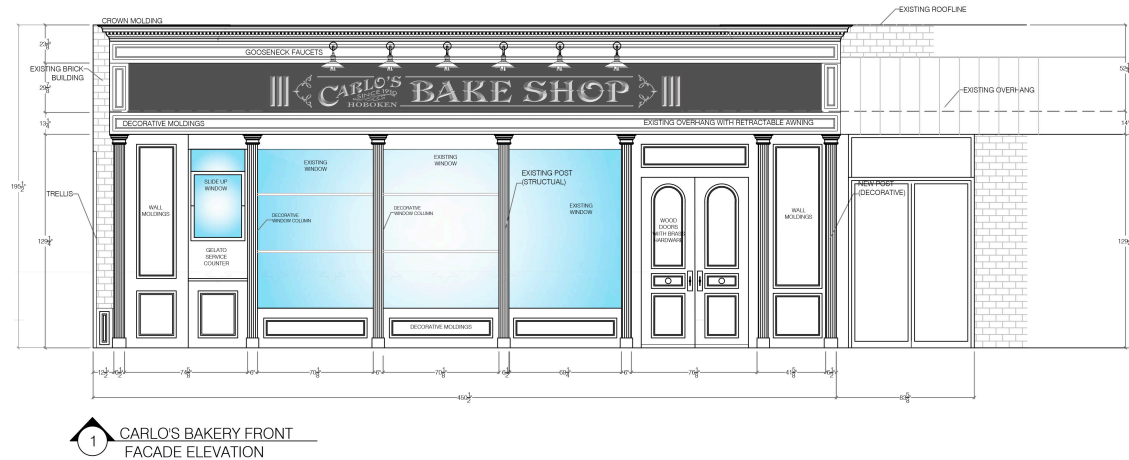
Photo 5: Carlo's Bake Shop, Hoboken, New Jersey<sup>5</sup>

The existing front elevation is nondescript with an empty sign-box on the facade. The existing structural columns will be enhanced to mimic a roman doric column. An enlarged sign canopy will be constructed to allow for signage and design features.



Photo 6: Existing street view

<sup>5</sup> Image - [en.wikipedia.org](https://en.wikipedia.org)



TYPE EXTERIOR ELEVATIONS		DATE	24/07/2021
DESIGNED FOR	CARLO'S BAKERY	SCALE	NTS

DESIGN AND CONSTRUCTION PROVIDED FOR THE FAIR USE BY THE CLIENT OR HIS AGENT IN COMPLETION OF THE PROJECT AS LISTED WITHIN THIS CONTRACT. DESIGN PLANS SHALL BE THE PROPERTY OF THIS FIRM AND CAN NOT BE REPRODUCED OR USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN PERMISSION OF CONIBEAR DESIGN. ALL DIMENSIONS AND SIZE SPECIFICATIONS GIVEN SUBJECT TO VERIFICATION ON JOB SITE AND ADJUSTMENT TO FIT JOB CONDITIONS. ANY ALTERATIONS OR DEVIATIONS FROM THE SPECIFICATIONS ON THIS DRAWING ARE SUBJECT TO COST RESPONSIBILITY BY CLIENT.

Figure 3: Proposed elevation

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## Excerpts from Heritage Conservation District Plan

The subject property is classed as 'other' within the Old Port Credit Heritage Conservation District Plan. Sections 7.0, Alterations and additions to other properties; and Section 8.0, Properties fronting Lakeshore Road East and West, of the HCD Plan provide the guidelines for alterations and additions to 'other' properties.

### *7.0 Alterations and additions to other properties*

*There are some other residential properties in the District, although new residential construction may be permitted in the future.*

*New construction and work undertaken to non-heritage residential buildings should respect the overall character of the Old Port Credit Village Heritage Conservation District and be sensitive to any neighbouring heritage buildings. The following should be consulted for general guidance:*

### *7.1 Policies for alterations and additions to other properties*

*7.1.1 Property owners must abide by the Property Standards By-Law 654-98.*

*7.1.2 When permitted additions to buildings on other properties are proposed, the design will respect the District's general historical character but integrate with the existing building. In designing additions, property owners will also have regard for any impact the addition may have on adjacent properties in terms of scale, massing, height and setback.*

*7.1.3 Animated signs are prohibited.*

### *7.2 Guidelines for alterations and additions to other properties*

#### *7.2.1 Location*

*a) Exterior additions are encouraged to be located at the rear or on an inconspicuous side of the building, limited in size and scale to complement the existing building and neighbouring properties.*

#### *7.2.2 Height*

*a) New additions should be no higher than the existing building height. Where additions are proposed that exceed the existing building height, a heritage impact assessment must be completed that demonstrates that the additional height does not negatively impact the District.*

#### *7.2.3 Width*

*a) New additions should be designed in a building mass that extends rearward in depth on the lot rather than along the horizontal width.*

#### *7.2.4 Setback*

*a) In streetscapes of similar building setbacks new construction should match existing.*

#### *7.2.5 Roofs*



*a) Roof types encouraged in new construction are front gabled and side gabled. Flat roofs are permitted for additions to existing multi-unit residential buildings, and new multi-unit residential buildings on other properties.*

*b) Asphalt, wood shingles or metal are appropriate for new construction.*

*c) Any required roof vents, skylights, satellite dishes, solar panels, metal chimneys and flues, other venting devices and roof features should be located to the rear of new additions.*

*d) Roof lines should reflect the horizontal cornice of adjacent heritage buildings where adjacent properties are of a comparable scale.*

#### *7.2.6 Exterior cladding*

*d) Most of the properties in the District are clad in traditional materials such as brick, stucco or wood siding. These materials are encouraged for new additions.*

### *8.1 Policies for properties fronting Lakeshore Road East and West*

*8.1.1 Property owners must abide by the Property Standards By-Law 654-98.*

### *8.2 Guidelines for properties fronting Lakeshore Road East and West*

#### *8.2.1 Location*

*a) Exterior additions are encouraged to be located at the rear or on an inconspicuous side of the building, limited in size and scale to complement the existing building and neighbouring properties.*

#### *8.2.2 Height*

*a) The height of additions should respect the height of existing adjacent buildings.*

#### *8.2.3 Relation to street*

*b) Additions and alterations to commercial buildings on other properties shall continue to maintain the street wall with appropriate use of materials on the ground and upper floors.*

*c) Additions to commercial buildings on corner sites shall address the side lot line with appropriate materials, glazing, entrances and other architectural features to ensure continuity with street front façade design and avoiding the construction of large blank walls.*

#### *8.2.4 Windows and entrances*

*a) Storefronts should be primarily glazed, and upper storey windows should be rectangular and vertical in proportion.*

*b) Entrances from the sidewalk may be flush or recessed.*

*c) Blank windowless walls are discouraged.*

#### *8.2.5 Exterior cladding*

*a) The principal building cladding for commercial buildings in the District has been historically either brick or rough cast stucco. Residential buildings in commercial use are frame with stucco or*

horizontal weatherboard cladding. These materials will be encouraged for major additions, with an emphasis on distinguishing old and new portions of the building.

b) The installation of Exterior Insulation and Finish Systems (EIFS) on buildings on other properties is discouraged within the District.<sup>6</sup>

## Compliance with Heritage District Conservation Plan

Table 1: Compliance with HCD

HCD Plan Section	Compliance (yes/no)	Comments
7.1.1 Property Standards	Yes	By-law will be adhered to.
7.1.2 Permitted additions	Yes	The alteration is sympathetic to the adjacent properties.
7.1.3 Animated Signs	Yes	No animated sign is proposed.
7.2.1 Location	Yes	It is not an addition
7.2.2 Height	Yes and No	The proposed addition to the front is for signage. There is no height increase to the overall building.
7.2.3 Width	N/A	
7.2.4 Setback	Yes	No changes to the existing front yard setback
7.2.5 Roofs	Yes	No alterations proposed to the existing roof (unless structural changes are required).
7.2.6 Exterior Cladding	Yes	No changes proposed to the existing cladding.
8.1.1 Property Standards	Yes	By-law will be adhered to.
8.2.1 Location	Yes	Technically, this is not an addition, as no GFA is being added to the building.
8.2.2 Height	Yes	The proposed alteration to the front will
8.2.3 Relation to Street	Yes	The proposed materials are compatible and complimentary to the adjacent buildings.
8.2.4 Windows and entrances	Yes	The existing windows are to be maintained. A new front door is proposed.
8.2.5 Exterior Cladding	No	A small portion of the exterior brick will be covered with panels to mimic the new door.

<sup>6</sup> Old Port Credit Heritage Conservation District Plan

## Streetscapes



Figure 4: Existing Streetscape



Figure 5: Proposed Streetscape

## Mitigation Measures

The subject building is located on the fringe of the boundary of the Old Port Credit Village HCD. Situated immediately east of the subject property is Clarke Memorial Hall, a designated heritage property and the Port Credit Methodist Church.

Subject property

Old Port Credit Village Heritage Conservation District Plan 2018  
Heritage Conservation District Plan

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Figure 16: Old Port Credit Village HCD property types

George Robb Architect | MHBC | WSLA | HHI

July 2019

## Figure 6: HCD Mapping<sup>7</sup>

<sup>7</sup> Old Port Credit Heritage District Conservation Plan





Photo 7: Lakeshore Road West & Mississauga Road Intersection<sup>8</sup>

Approaching the intersection of Mississauga Road and Lakeshore Road West, from the west (travelling east), there is no clear indication that you are entering the Old Port Credit Heritage Conservation District. The north side of Lakeshore Road reflects more modern structures, including a twenty storey residential tower. The south side is low rise mixed use buildings. The subject property is contained within the corner building at the south east corner of Lakeshore Road West and Mississauga Road, Clarke Memorial Hall is not easily identifiable as a designated property, nor is the Port Credit Methodist Church. Neither building has a plaque or something out front screaming “I am a heritage building” The proposed exterior alterations to the subject building will not obstruct the adjacent properties

heritage value or the nature of the HCD.

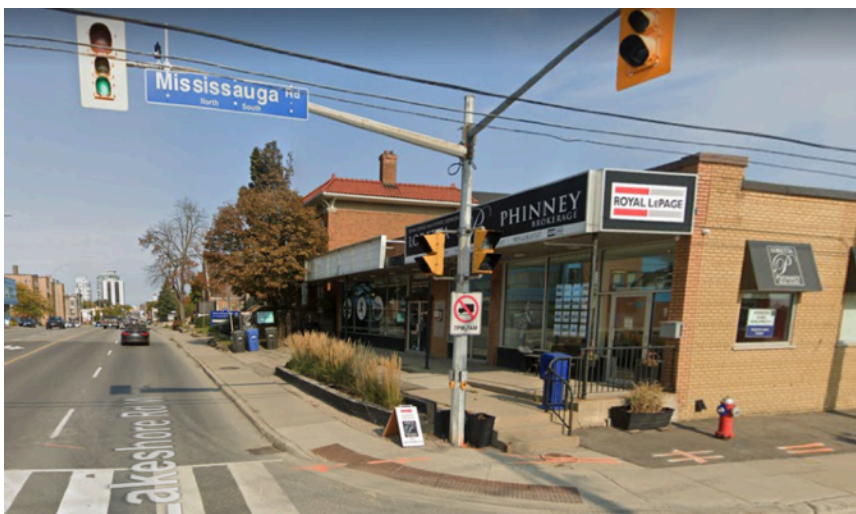


Photo 8: View of Subject property<sup>9</sup>

<sup>9</sup> Google Streetview

Photo 8 (above) demonstrates that the current view of Clarke Memorial hall is the west side of the building which does not provide any indication that it is a heritage designated building.

Mitigation measures, outlined in the table below, have been considered to ensure the heritage resources of the adjacent property and the conservation district are not negatively impacted.

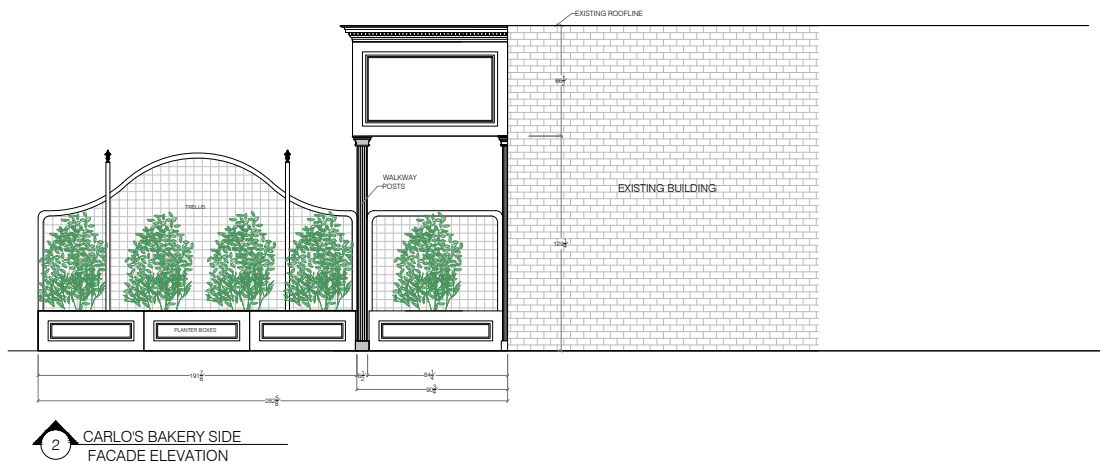
**Table 2: Mitigation Measures**

Potential Impact	Identified Impact	Mitigation
Destruction of any, or part of any, significant heritage attributes or features	None	Not required
Alteration that is not sympathetic, or is incompatible with the historic fabric and appearance	None	Not required
Isolation of a heritage attribute from its surrounding environment, context or a significant relationship	None	Not required
A change in land use where the change in use negates the property's cultural heritage value	N/A	Not required
Removal of natural heritage features, including trees	None	Not required
Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature, or plantings, such as a garden	None	Not required
Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features	None	Not required
Land disturbances such as change in grade that alter soils, and drainage patterns that adversely affect cultural heritage resources.	None	Not required

Mitigation methods recommended by The Guide to Cultural Heritage Resources in the Land Use Planning Process are identified in Table 2. As there are no site alterations proposed, only facade and interior, these mitigation methods are not applicable or not required. Further, we have reviewed the report and have compiled Table 3, outlined below, of further considerations that were undertaken to avoid or limit the impacts of the proposed development.

Table 3: Additional Mitigation Measures

Strategy	Mitigation Measure
Vegetation screening, fencing	The existing mature tree between the subject property and the property to the east will be maintained. Future plans include the installation of a (see architectural drawing attached - Figure 5)
Buffers	See above
Site Lines	The proposed alteration to the building is for signage only. This will not obstruct the view of the adjacent heritage property.
Facade treatment	Subdued / muted colours have been chosen so as not to provide a 'stark' contrast to adjacent buildings



TYPE	EXTERIOR ELEVATIONS	DATE	24/07/2021	 Alex Corbier Interior Design   Fine Art 216-218-0766 alex@corbierdesign.com www.corbierdesign.com	 Corbier Design	DESIGN PLANS ARE PROVIDED FOR THE FAIR USE BY THE CLIENT OR AGENT IN COMPLETION OF THE PROJECT AS LISTED WITHIN THIS CONTRACT. DESIGN PLANS REMAIN THE PROPERTY OF THE FIRM AND CANNOT BE USED OR REPRODUCED WITHOUT PERMISSION. ALL DIMENSIONS AND SIZE REQUIREMENTS GIVEN SUBJECT TO VERIFICATION ON JOB SITE AND ADJUSTMENT TO SET CONDITIONS. ANY ALTERATIONS OR DEVIATIONS FROM THE SPECIFICATIONS ON THIS DRAWING ARE SUBJECT TO THE CLIENT'S RESPONSIBILITY.
DESIGNED FOR	CARLO'S BAKERY	SCALE	NTS			

Figure 7: Future Screening &amp; Landscaping

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## Conclusions, Recommendations

The subject property contains a one-storey commercial building built in the 1960's. The proposed alterations will have no impact on the cultural heritage resource or the adjacent property to the east. Mitigation measures, as noted above, have been considered and no further action is required.

### ABOUT THE AUTHOR

William Oughtred of W.E. Oughtred & Associates Inc., is a development and land use consultant who has been practicing in the Mississauga and GTA area for over 30 years. Mr. Oughtred has a Bachelor of Arts from McMaster University. Mr. Oughtred is well versed in both Planning and building procedures and the City of Mississauga Zoning By-law and Official Plan.

Mr. Oughtred specializes in infill development projects. His extensive experience has afforded him the opportunity to see the City evolve and be at the forefront of growing trends and patterns in land development in Mississauga. He consults regularly on both heritage and urban design for infill projects.

Heritage Impact Statements and Assessments have been completed for many properties in Mississauga, including, but not limited to the properties listed below.

- ❖ 965 Whittier Crescent
- ❖ 1503 Petrie Way
- ❖ 1470 Pinetree Way
- ❖ 2375 Mississauga Road
- ❖ 943 Whittier Crescent
- ❖ 2417 Mississauga Road
- ❖ 1641 Blythe Road



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## REFERENCES

[www5.mississauga.ca/pdfs/Cultural\\_Landscape\\_Inventory\\_Jan05.pdf](http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf)

<https://www.mississauga.ca/portal/services/property>

[Historic properties.ca](http://Historicproperties.ca)

Wikipedia.com

[google.com](http://google.com)

Ministry of Tourism & Sport, Guide to Cultural Heritage Resources in the Land Use Planning Process

Old Port Credit Heritage District Conservation Plan