

City of Mississauga Corporate Report



Date: December 23, 2021

To: Chair and Members of Planning and Development Committee

From: Andrew Whittemore, M.U.R.P., Commissioner of Planning & Building

Originator's files:
CD.03-DOW

Meeting date:
January 24, 2022

Subject

INFORMATION REPORT (WARDS 4 and 7)

Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards

Recommendation

1. That the report titled "Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards" dated December 23, 2021 from the Commissioner of Planning and Building, be received for information.
2. That staff undertake community engagement to inform, consult and receive feedback on the draft official plan policies and built form standards contained in this report and include holding a community meeting and a statutory public meeting.

Executive Summary

- In 2019, Mississauga City Council passed a motion directing planning staff to prepare amendments to Mississauga Official Plan (MOP) for three communities located along the Hurontario corridor – Downtown Fairview, Cooksville and Hospital Character Areas. With transit investments on the way and anticipated population and employment growth, an update to MOP policies for these lands are necessary.
- Through this update, staff is exploring how official plan policies can support the creation of 15 minute communities in Downtown Fairview, Cooksville and Hospital. The 15 minute city is an approach to city planning focussing on building high-quality, liveable communities where the basic needs of everyday life can be safely reached within 15 minutes by foot.

Originator's file: CD.03-DOW

- This report presents a draft Official Plan Amendment (OPA) and complementary Built Form Standards (BFS) to manage future growth and achieve vibrant, 15 minute communities. The proposed OPA incorporates a new vision and guiding principles, and policies on community uses, road and pedestrian connections and built form requirements. The BFS proposes a range of design standards to support the implementation of official plan policies, such as: setbacks, podium conditions, tower separation distance, and additional standards to achieve a main street character.
- The Region of Peel is undertaking a Major Transit Station Area (MTSA) study to establish new policy requirements for higher density, mixed use growth in areas with existing or planned transit. Within Downtown Fairview, Cooksville and Hospital, the Regional MTSA study identifies five potential MTSA's along Hurontario Street. Several amendments to official plan policies and schedules are contained in this report for information purposes that are intended to be implemented through a future MTSA MOP Amendment once the draft MTSA Regional Official Plan Amendment (ROPA) is approved.
- As part of the next steps, staff is planning to hold a virtual community meeting to seek feedback from the public on the draft OPA and BFS in February 2022. This will subsequently follow with a statutory public meeting held at a future Planning and Development Committee meeting.

Background

In 2019, Mississauga City Council passed a motion directing staff to prepare amendments to MOP for three communities located along the Hurontario corridor – Downtown Cooksville, Fairview and Hospital Character Areas. The last comprehensive policy review of these lands was completed in the late 1990s. With transit investments on the way and anticipated population and employment growth, an update to these policies are necessary. The Hurontario Light Rail Transit (LRT) is under construction, planning is underway for Bus Rapid Transit (BRT) along Dundas Street, and the potential exists for two-way, all-day GO Transit service at the Cooksville GO Station. This report presents the draft MOP policies and BFS intended to manage that growth and help achieve vibrant, 15 minute communities. Transit-oriented development will ensure transit investments are supported with greater access to housing, jobs and services.

IMPLEMENTING THE OUTCOMES OF PREVIOUS STUDIES

The 2019 Mississauga City Council motion identified four previous studies and plans for consideration in preparing amendments to MOP: the *Hurontario/Main Street Corridor Master Plan* (2010); the *Cooksville Mobility Master Plan Study* (2011); *Vision Cooksville* (2016); and the *Dundas Connects Master Plan* (2018). These studies provide clear guidance on how Downtown Fairview, Cooksville and Hospital should develop, and include direction on land uses, heights, transportation improvements and urban design.

Planning and Development Committee	2021/12/23	3
------------------------------------	------------	---

Originator's file: CD.03-DOW

These studies have been considered in the draft MOP policies and BFS. A brief description of each study or plan is provided below.

1. The Hurontario/Main Street Corridor Master Plan was approved by City Council in June 2010, and provides a planning and urban design framework that envisions a mixed use, pedestrian oriented corridor. The Plan includes detailed recommendations for a transit-supportive height framework; mixed residential, retail, office and commercial uses along Hurontario Street; improved connections for pedestrians, cyclists and transit users; and a walkable and human-scale built form.
2. The Cooksville Mobility Hub Master Plan Study was completed in 2011 in partnership with Metrolinx to develop a long-term vision and integrated development framework for the Cooksville GO Station and surrounding lands.
3. Vision Cooksville was approved in June 2016 by the Planning and Development Committee. Developed in partnership with local residents, businesses and property owners, the study establishes a vision for Downtown Cooksville rooted in its multicultural history. The study recommends: new and improved parks and open space; pedestrian-oriented street and building design; a Cooksville community hub; increased housing opportunities and choice; support for small, independent business; and a distinct Cooksville identity through signage, public art and an enhanced public realm.
4. The Dundas Connects Master Plan was endorsed by City Council in June 2018. The Plan makes recommendations for rapid transit, land use and streetscape changes along the Dundas corridor, including a link to the Cooksville GO Station and Hurontario LRT.

In addition to these four studies, staff also considered city-wide studies that may inform the planning direction for these areas such as the *Downtown Growth Area Park Provision Strategy* (2015), *2019 Parks & Forestry Master Plan*, the *2019 Culture Master Plan*, and the *Pedestrian Master Plan* (2021). Furthermore, recent changes to provincial planning policies and legislation, and evolving housing and community needs, has resulted in the need to update official plan policies to reflect the current context.

ALIGNING WITH REGIONAL MAJOR TRANSIT STATION AREA STUDY

The Region of Peel is undertaking a Municipal Comprehensive Review (MCR) to conform to provincial planning policies and legislation that will require approval by the Province. As part of the MCR, the Region is conducting a MTSA study to establish new policy requirements for higher density, mixed use growth in areas with existing or planned transit. In the fall of 2021, Regional staff presented a draft MTSA Regional Official Plan Amendment (ROPA) to Regional Council. Within Downtown Fairview, Cooksville and Hospital, the MTSA ROPA identified five potential MTSA's along Hurontario Street – at Central Parkway, Cooksville GO Station, Dundas Street, Queensway and North Service Road.

Originator's file: CD.03-DOW

The MTSA ROPA will delineate MTSA boundaries and introduce minimum densities for each MTSA. Local municipalities will be directed to address: land uses that support complete communities; minimum densities; minimum and maximum heights; and improved multi-modal access and connectivity, among other matters. The draft MTSA ROPA included a minimum density target of 300 persons plus jobs per hectare for potential MTSA's in Downtown Fairview, Cooksville and Hospital. Once the MTSA ROPA is approved, MOP policies will need to be updated to conform to the Region of Peel Official Plan and achieve Regional requirements related to land uses, heights and densities for protected MTSA's.

CREATING A '15 MINUTE CITY'

Through this policy review, the City is exploring how official plan policies can support the creation of 15 minute communities in Downtown Fairview, Cooksville and Hospital. The 15 minute city is an approach to city planning focussing on building high-quality, liveable communities where the basic needs of everyday life can be safely reached within 15 minutes by foot. This approach emphasizes the features of a community that are needed to serve residents – including parks, groceries, jobs, medical clinics, pharmacies, schools, shops, restaurants, and more – and that these everyday needs can be safely and conveniently accessed without the use of a car.

On April 22, 2021, a 15 minute city forum was held with the community to explore how the concept of the 15 minute city can be applied to policy with a focus on Downtown Cooksville. With its mix of shops, restaurants and services, Downtown Cooksville in particular contains many of the elements of a 15 minute city. At the forum, three policy directions were proposed to ensure new development contribute to the success of the 15 minute city concept: create a mixed use community; plan for more housing and people; and achieve a walkable and connected community. Examples as to how they would apply were also provided. These included:

- the extension of Cook Street to improve pedestrian and cycling connectivity;
- promoting a main street character along Hurontario and Dundas Streets with a mix of services and amenities that are readily available and accessible by walking;
- increased housing choice and availability; and
- improved pedestrian and cycling amenities to enhance safety and comfort.

Comments

A land use review of the Downtown Fairview, Cooksville and Hospital Character Area policies and schedules of MOP has been undertaken by planning staff to set a new planning framework for these areas of Mississauga. The proposed changes will implement a new direction to manage the growth and development of these areas.

The draft policies outlined in Appendix 1 of this report incorporates a new vision and guiding principles for each Character Area. It sets out ways to support more housing along rapid transit corridors by permitting taller buildings while considering the need for future school facilities,

Originator's file: CD.03-DOW

public parkland and open spaces and a mix of other uses for access to basic needs within a short walking distance from residents' homes. It considers how new buildings can be utilized to provide employment and create vibrancy to the area through a variety of uses. The type of non-residential uses is wide-ranging, from restaurants to personal service establishments, banks, office (including medical office), community or cultural uses and more. Policies on improved transportation connections will help ensure this mix of uses can be accessed safely and conveniently by pedestrians and cyclists.

The draft OPA also contains urban design policies that will help shape the design of the built environment to achieve vibrant, 15 minute communities. Urban design staff have developed a set of draft complementary BFS that further guides development at the planning application stage.

OVERVIEW OF THE DRAFT OFFICIAL PLAN AMENDMENT (OPA)

1. MOP Amendment to the Downtown Fairview, Cooksville and Hospital Character Area Policies

Staff is proposing an OPA to amend three sections of MOP: 12.3 Downtown Fairview Character Area, 12.4 Downtown Cooksville Character Area and 12.5 Downtown Hospital Character Area. The Character Area policies will consist of several new sections: an introduction that provides the context of the area, a vision statement and guiding principles that set out the priorities and direction for growth and the development, general policies regarding community uses, transportation policies on the design and requirements for more pedestrian and road connections, and urban form policies that provide direction on built form and street frontage provisions.

(a) Setting a Vision and Guiding Principles

Downtown Fairview, Cooksville and Hospital are expected to grow with more residents and jobs. A vision and complementary guiding principles provide a framework that sets forth the development priorities in each Character Area.

(b) Providing Community Uses (Community Infrastructure, Public Parkland and Open Spaces)

The draft OPA contains policies that encourage partnerships and collaborations to develop more community infrastructure, such as the integration of school or daycare facilities within new buildings. It also includes policies on the provision of public parkland and open spaces to support a range of social and recreational activities.

(c) Ensuring High Quality Urban Design and Built Form

A new set of urban design policies was developed to provide direction on the built form of Downtown Fairview, Cooksville and Hospital. The urban design policies identify future street types based on their land use function and character. Three street types are identified – 'A', 'B' or 'C' Streets. These policies are complemented by the draft Downtown Fairview, Cooksville and Hospital BFS which set out to communicate design

expectations and ensure that development occurs in a coordinated and comprehensive manner.

The draft urban form policies include general requirements such as: a transition in height between high and low density development that is generally consistent with a 45 degree angular plane; minimum building separation distances between tall buildings; and podium conditions. Additional policies address urban design requirements that are unique to each street type such as: minimum building streetwall conditions; activated building frontages; the design of retail and service commercial units; the seamless interface and transition between the sidewalk and principal entrances of residential developments; and vehicular access to parking and other services.

(d) Establishing Transportation Connectivity

Additional policies for improved transportation connectivity in Downtown Fairview, Cooksville and Hospital are proposed to ensure that new roads will be public and that pedestrian connections link to the LRT, transit route/stop or to other park/open space amenities.

2. Connecting Policies to the MTSA Study

City staff continue to collaborate closely with Regional staff as work on the draft MTSA ROPA proceeds. This report identifies several draft official plan policy and schedule changes that are intended to be implemented as one part of a future MTSA MOP Amendment. These are marked with a blue box in Appendices 1 and 2 for information purposes to begin consultation in advance of MTSA ROPA approval. Once the draft MTSA ROPA is approved, city staff will be bringing forward a draft MTSA MOP Amendment that will contain the policy and schedule changes marked with a blue box in this report; subject to further comment and revisions. Other policies that are not marked with a blue box will be implemented through the existing MOP structure.

The following key items are intended to be incorporated in the future MTSA MOP Amendment. Appendices 1 and 2 provides a complete set of the draft policies.

(a) Setting Out the Building Heights

The draft OPA proposes a new transit-supportive density framework that varies maximum heights based on the vision for each Character Area, and proximity to a transit stop or station. Limiting maximum building heights will also help manage growth in these areas. The transit-supportive height framework recognizes the existing built form conditions today while allowing for greater heights in key areas, such as up to 16 or 18 storeys along the Dundas corridor, and up to 25 storeys around the Cooksville GO

Station and other potential MTSAs. The minimum and maximum building heights are identified on a Map in the draft OPA.

(b) Directing the Uses of Land

Planned mixed use areas are an important component of vibrant, 15 minute communities. The draft OPA proposes several policy changes to achieve a mix of uses, including re-designating key sites to Mixed Use (Appendix 2 - Schedule 10) and the addition of public open spaces (Appendix 2 - Schedules 1, 1a and 4). At-grade retail and service commercial uses will be required along key corridors such as Hurontario and Dundas Streets. In addition, a minimum of three storeys of non-residential uses in new buildings will be required at three key locations – at the lands around the Cooksville GO Station, the intersection of Hurontario/Dundas and the Trillium Mississauga Hospital. These locations are intended to serve as one-stop destinations where the greatest mix of services and amenities are readily available and accessible by walking or cycling.

As redevelopment continues in the Downtown Fairview, Cooksville and Hospital areas, staff are challenged with development applications that offer a large number of dwelling units but minimal office or retail service uses. To ensure that these areas remain attractive, livable and complete communities, staff is introducing policies that require developments to retain or incorporate the same amount of office, retail and service commercial floor space.

Special Site and Exempt Site Policies have also been updated. Several special sites that are no longer needed are deleted while other special site policies are added or revised to provide supplementary policy direction in unique areas such as the lands around Cooksville GO Station. New exempt site policies recognize the existing use with the intent that the lands will be redeveloped over time with the underlying land use designation.

OVERVIEW OF THE BUILT FORM STANDARDS (BFS)

The Downtown Fairview, Cooksville and Hospital BFS (Appendix 3) is a draft document, prepared by Urban Design staff, to further guide and direct developments at the development application review stage. It expresses the City's expectations for good design, and high quality built form and public realm in creating vibrant places.

The BFS provides more details on various built form standards as well as architectural design guidelines. The following summarizes several key design standards:

- Setbacks – for retail and service commercial uses, the building may be located closer to the street edge, between 2 to 4 metres. For residential units, the building may be located between 4.5 and 6 metres to provide a buffer to enhance the public realm of residential streets;

Originator's file: CD.03-DOW

- Podium setbacks – the podiums of buildings should be defined. The standards sets the minimum and maximum podium heights of 3 and 6 storeys, respectively. The minimum setback between the podium face and a tower is 3 to 6 metres. At the intersection of Hurontario Street and Dundas Street in Downtown Cooksville (MOP, Downtown Cooksville – Special Site 1), podiums will be between 3 and 4 storeys and towers will be setback a minimum of 6 metres from the podium face;
- Floor plate sizes – massing for buildings up to 12 storeys will be no more than 1,000 square metres above the podium; buildings greater than 12 storeys will permit a maximum floor plate of 750 square metres above the podium;
- Tower separation – the placement of the building should allow for open spaces within the site and ensure adequate access to sunlight. Tower separation distances will be required of at least 30 metres from the edges of the tower face;
- Building articulation – the podium will be designed to appear to be broken down into multiple parts by inseting small parts of the floor plan, articulating balconies differently, and varying the massing and use of materials;
- Height variation – development with multiple towers should vary in height and massing. The height of the towers will vary by 3 to 5 storeys;
- Main street character – development will provide a continuous streetwall and coordinate with adjacent properties to create a consistent edge along street frontages. Storefronts will be designed with: a traditional mainstreet character that has regular sized bays and a combination of glazed storefronts between solid piers; awnings and canopies above the storefronts approximately 3 to 5 metres above grade; and wind protection over storefronts and entrances; and
- Parking structures – development will locate structured parking and vehicular access in a manner that minimizes impacts on the property and surrounding properties.

The BFS provides further direction on design standards based on street types that are categorized as 'A', 'B' or 'C' Streets. These include, but are not limited to, requirements for ground floor height, building entrances, patio depths, and consistent signage and lighting. The street types are generally described as follows:

- 'A' Streets are considered to be primary arteries and must incorporate buildings with at-grade commercial and/or retail uses. The built form and streetscape treatment along 'A'

Originator's file: CD.03-DOW

Streets are critical to achieving a lively urban environment that fosters active uses and a pleasant pedestrian environment;

- 'B' Streets are secondary streets and are more residential in character. Entrances to residential developments should be raised from the sidewalk, generally between 0.6 metres and 1.2 metres above grade. Setbacks and landscaping should be designed in a manner that creates a buffer between the public and the private realms and maintain the residential character of these streets; and
- 'C' Streets are tertiary streets that connect 'A' and 'B' Streets. 'C' Streets should incorporate development blocks with access for deliveries, garbage pick-up, service and loading, including vehicular access to structured and off-street parking within development sites.

Financial Impact

There are no financial impacts resulting from the recommendations in this report.

Conclusion

The draft OPA and BFS set a new framework for Downtown Fairview, Cooksville and Hospital and provides direction to help guide the development of land and growth in these areas. Staff is recommending Planning and Development Committee receive the draft documents for information and to begin the public engagement process. The engagement will enable staff to inform and consult with the public and get feedback on the draft OPA and BFS. As part of the next steps, staff is planning to hold a virtual community meeting as early as February 2022. This will subsequently follow with a statutory public meeting held at a future Planning and Development Committee meeting.

Attachments

- Appendix 1: Downtown Fairview, Cooksville and Hospital Draft Official Plan Amendment – Character Area Policies
- Appendix 2: Downtown Fairview, Cooksville and Hospital Draft Official Plan Amendment – Official Plan Schedules
- Appendix 3: Downtown Fairview, Cooksville and Hospital Draft Built Form Standards



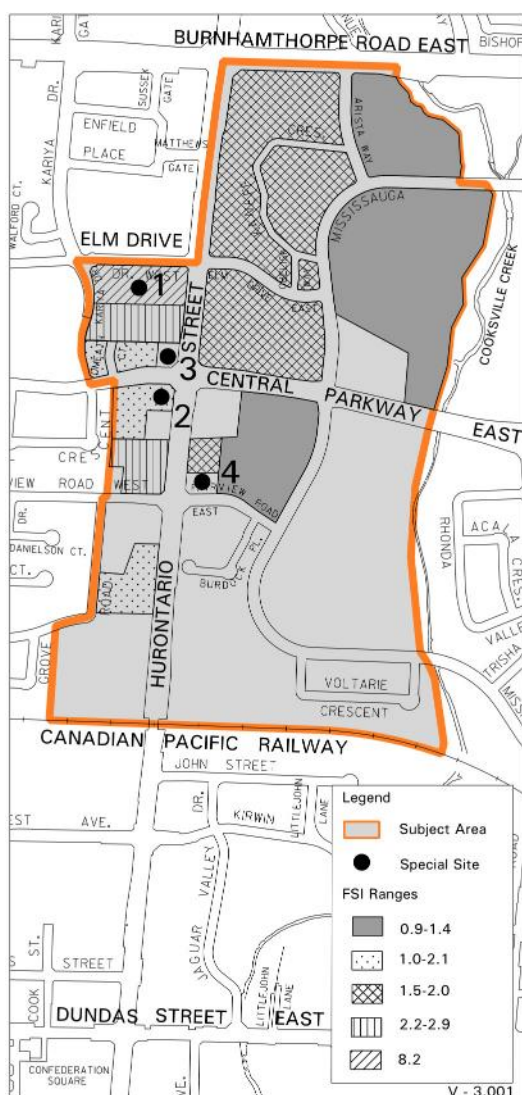
Andrew Whittemore, M.U.R.P., Commissioner of Planning & Building

Prepared by: Karin Phuong, Planner, Christian Binette, Planner and Ayman Jalloul, Urban Designer

Notes:

Existing policies are shown in the black text. Proposed new changes are shown in the **red text**. Proposed deletions to existing policies are shown as ~~strike-outs~~. Text and maps shown in a **blue box** have been included in this document for information purposes only and are intended to be included in a broader amendment related to Major Transit Station Areas to conform to the Region's Official Plan Amendment.

12.3 Downtown Fairview



Map 12-3: Downtown Fairview Character Area

Changes to Map 12-3:

- Delete FSI Ranges
- Delete Special Sites 1 to 4
- Add new Exempt Site – Exempt Site 1

12.3.1 Introduction

Downtown Fairview is situated on the periphery of Mississauga's Downtown Core and provides an area of transition to Downtown Cooksville. Downtown Fairview contains a number of sub-areas, each with its own unique character and built form. Along Mississauga Valley Boulevard, there is a collection of 'tower in the park' developments with generally 20 to 24 storey apartment buildings that are surrounded by large open space areas with connections to trails and the Cooksville Creek. The area at Hurontario Street and Elm Drive West contains the greatest densities in Downtown Fairview, with point towers as tall as 50 storeys. The townhouses located in the southern end of Downtown Fairview are the predominant built form and provides housing for families.

There is limited retail and commercial uses along Hurontario Street, however with planned higher order transit, new developments can transform the street into vibrant places to shop and visit. Anchored by a grocery store, the existing retail site located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard provides convenient shopping for nearby residents.

12.3.2 Vision

Downtown Fairview will be a distinct walkable community that provides a transition to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where residents can access their daily needs within a short distance from their homes. Mixed use development located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will continue to serve the retail and commercial needs of the local community. The area along Mississauga Valley Boulevard will continue to be defined by existing towers in the park and beautiful green spaces, with improved connections to trails and to Cooksville Creek.

An expanded pedestrian network will connect new development with nearby amenities and residential

areas, and improve access to transit, parks and open spaces.

The vision is based on the following guiding principles:

1. Heights and densities that are appropriate to the planned context. The greatest heights will be located at Hurontario Street and Elm Drive West, with a transition to lower building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard;
2. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;
3. Accessible public spaces for all, with enhanced connections for walking and cycling; and
4. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.

12.3.3 General

12.3.3.1 In accordance with the City's housing strategy, the City will require that development be supported by a range of housing options for residents, such as a mix of tenure and the provision of units that are affordable to middle income households.

12.3.3.2 Development will explore partnerships with the Region of Peel and non-profit housing organizations to provide housing with deeper affordability to lower income households and to contribute towards Peel-wide needs-based housing targets on density, rental tenure, and affordability.

12.3.3.3 Mississauga will encourage partnerships and collaborations to identify needs and develop community infrastructure such as integrating a school or a daycare facility within the podium of a building.

12.3.3.4 The City will work in collaboration with the school board(s) to determine the need for educational facilities. The location and size of a school facility will

be confirmed through the review of development applications.

12.3.3.5 Downtown Fairview will develop with a high quality and well-designed network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections to existing and future parks and open spaces, commercial developments, community facilities and to surrounding neighbourhoods.

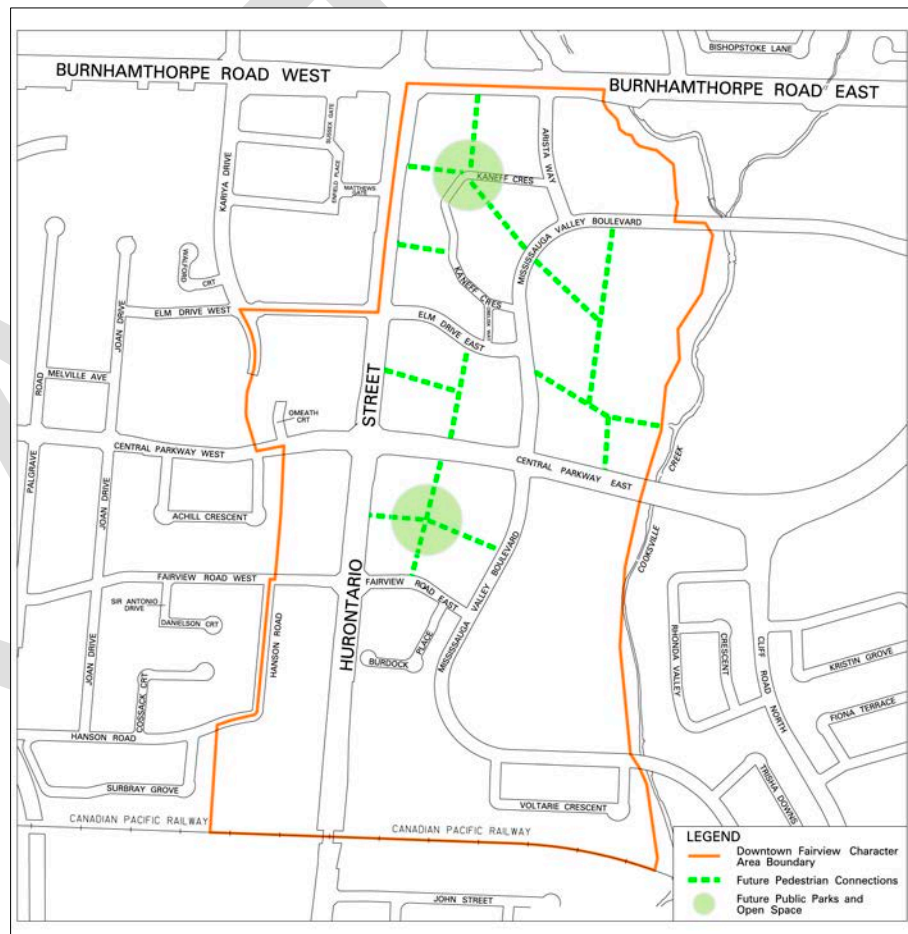


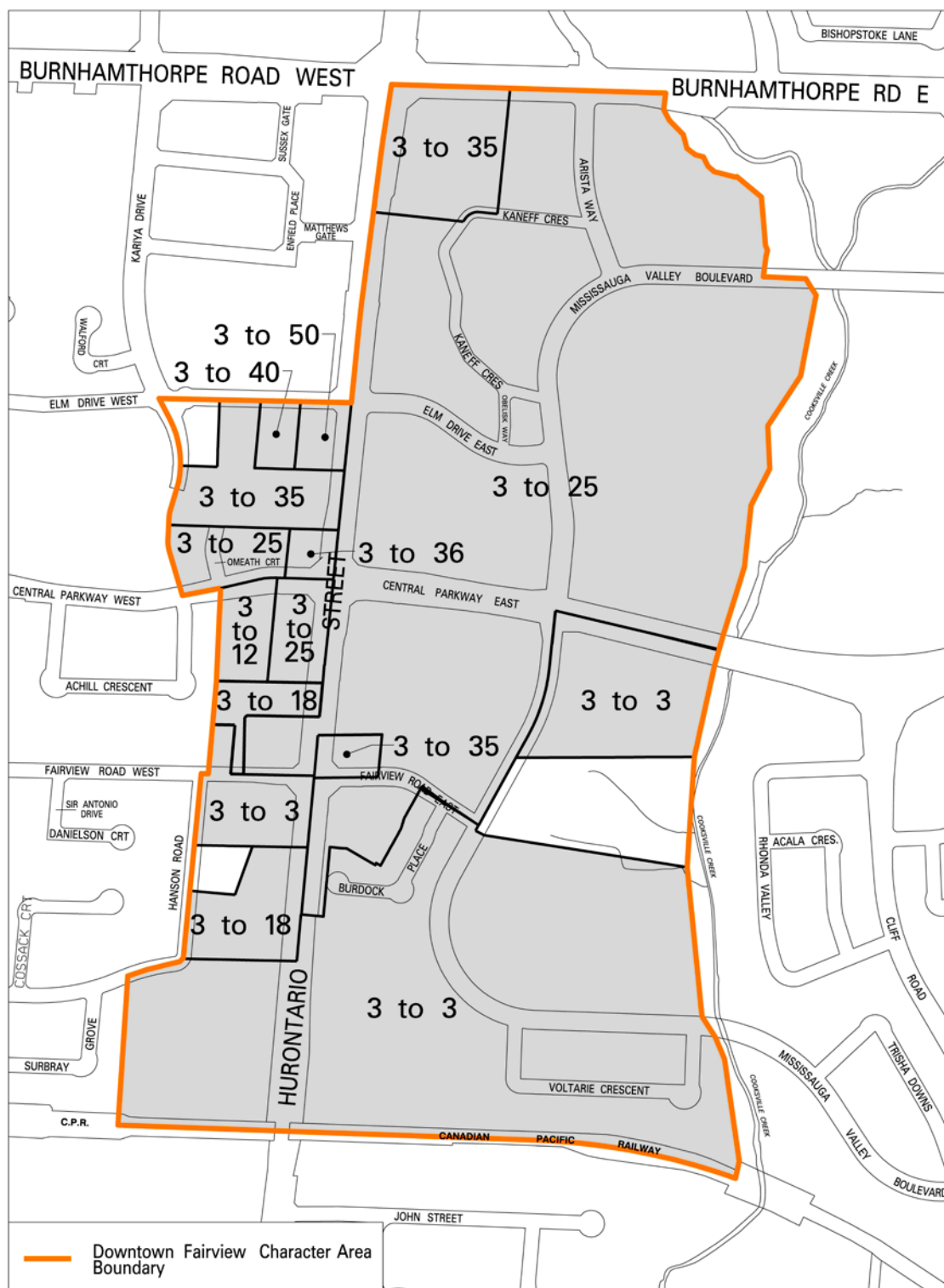
Figure 1: Future pedestrian connection and public parkland and open space network. These symbols represent the general location of new pedestrian connections, as well as public parkland and open spaces in the vicinity.

12.3.3.6 As part of the development application review process, the City will determine the location, configuration and size of the parkland block(s), and the requirement for land dedication. Opportunities to obtain parkland will also be explored through purchase by the City.

12.3.3.7 The City will explore opportunities to further increase the provision of public open space by establishing public easements and entering into future agreements, where appropriate. These open spaces will be complementary to the public parkland system.

12.3.4 Building Height

12.3.4.1 The greatest heights will be located in the existing and planned buildings at Elm Drive West and Hurontario Street. Building heights will not exceed the maximum limits as shown on Map 12-3.2: Downtown Fairview Character Area Minimum to Maximum Building Heights.



Map 12-3.2 Downtown Fairview Character Area Minimum to Maximum Building Heights

12.3.5 Urban Form

12.3.5.1 General

12.3.5.1.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:

- create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to low and medium density residential areas;
- maintain a minimum separation distance, generally of 30 metres between portions of buildings that are greater than six storeys;
- add visual interest by varying the use of massing and materials; and
- promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.

12.3.5.1.2 For larger developments where more than one tower is proposed, a variation in heights of three to five storeys will be achieved.

12.3.5.1.3 **Tall buildings** will incorporate podiums that are a minimum of three storeys and a maximum of six storeys.

12.3.5.1.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For **tall buildings**, the tower above the podium will have a limited floor plate size.

12.3.5.1.5 Along Hurontario Street, development will incorporate appropriate setbacks between the edge of the podium and tower portion of the building.

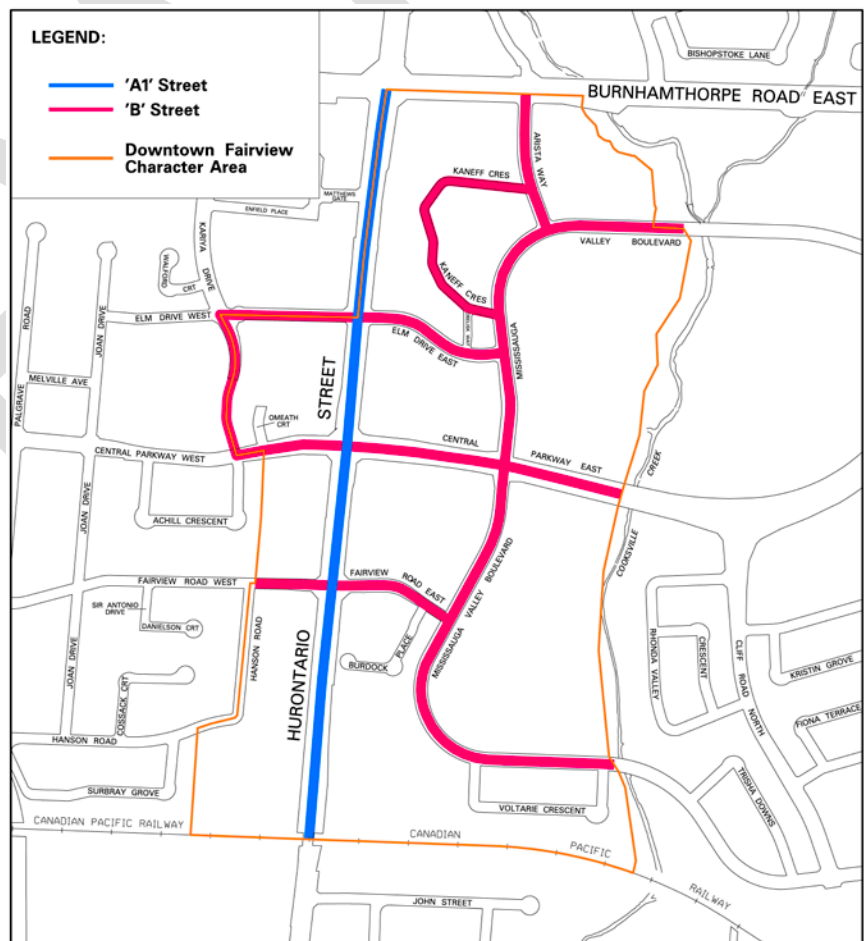
12.3.5.1.6 Infill development on existing 'tower in the park' sites will orient new buildings to face Hurontario Street, where possible.

12.3.5.2 Street Frontage Provisions

The majority of existing and new roads in Downtown Fairview are categorized as 'A' or 'B' Streets and are shown on Map 12-3.3: Downtown Fairview Street Types.

'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses. The built form and streetscape treatment of 'A' streets will be designed to foster active uses and a pedestrian friendly environment.

The built form along 'B' Streets will be primarily residential in character. The design of the built form and streetscape along 'B' streets will help establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms.



Map 12-3.3 Downtown Fairview Street Types

12.3.5.2.1 Provisions for 'A' Streets

12.3.5.2.1.1 'A' Streets will have a strong sense of place and main street character with active, mixed use building frontages and a highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.

12.3.5.2.1.2 Development abutting the street will incorporate grade related retail with residential and/or offices above. Development will address the following:

- a. promote a mainstreet character with smaller retail units and frequent entrances accessible from the public sidewalk;
- b. frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;
- c. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- d. design retail and service commercial units at street corner locations with animated storefronts that wrap the corner;
- e. generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios;
- f. appropriate setbacks between the edge of the podium and tower portion of the building; and
- g. development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character and to reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied *streetscape*.

12.3.5.2.2 Provisions for 'B' Streets

12.3.5.2.2.1 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature, development may include some commercial uses on the ground floor. Development will address the following:

- a. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, and pedestrian scaled lighting;
- c. a high quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy; and
- d. appropriate setbacks between the edge of the podium and tower portion of the building; and

12.3.6 Transportation

12.3.6.1 Development will be required to provide pedestrian connections to connect to the Hurontario Light Rail Transit (LRT), transit routes/stops, trails, and parks and open spaces.

12.3.6.2 Public easements will be required where pedestrian connections are proposed on private lands.

12.3.6.3 Vehicular access from Hurontario Street will be restricted and consolidated to minimize driveways and disruption to pedestrians, cyclists and transit.

12.3.6.4 Roads will be designed as complete streets. New roads will be designed to incorporate active transportation and transit infrastructure.

Existing arterial and collector roads dissecting and surrounding the Character Area will be redesigned to incorporate active transportation and transit infrastructure, as appropriate.

~~12.3.1~~ 12.3.7 Land Use

~~12.3.1.1 For lands designated Residential Medium density, building height should not exceed three storeys.~~

12.3.7.1 Notwithstanding the policies of this Plan, the following additional policies will apply to new development along Hurontario Street:

- a. at grade retail and service commercial uses will be required; and
- b. residential dwellings will not be permitted at grade.

12.3.7.2 Mixed Use

12.3.7.2.1 Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted, unless it can be demonstrated that the planned function of the non-residential component will be maintained or replaced as part of the redevelopment.

12.3.7.2.2 For the purposes of the policies in this section, maintaining the non-residential planned function of the Mixed Use designation means providing:

- a. a concentration of convenient, easily accessible office, retail and service commercial uses such as a grocery store that meet the needs of local residents and employees; and
- b. employment opportunities, such as office and institutional jobs.

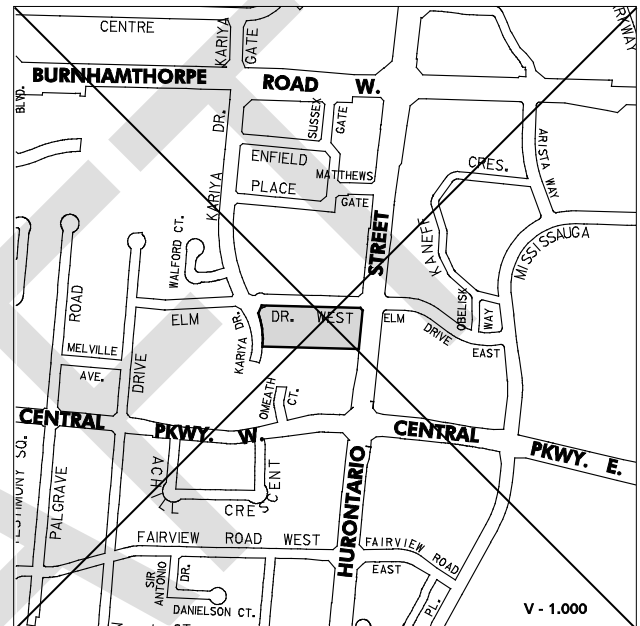
12.3.7.3 Office

12.3.7.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through development.

~~12.3.2~~ 12.3.8 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

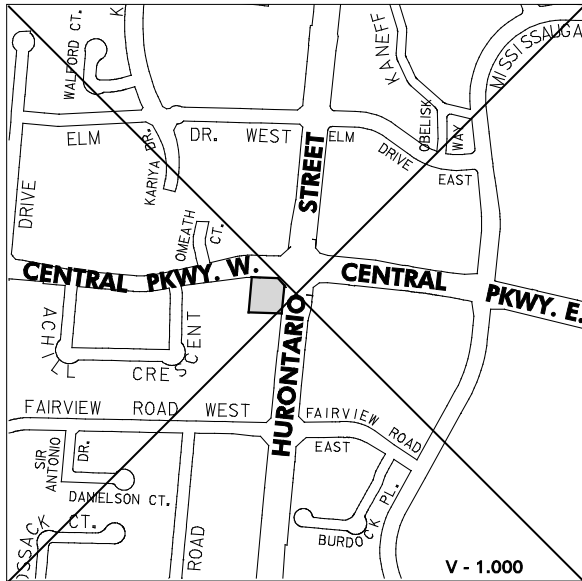
~~12.3.2.1~~ Site 1



12.3.2.1.1 The lands identified as Special Site 1 are located on the west side of Hurontario Street, south of Elm Drive.

12.3.2.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

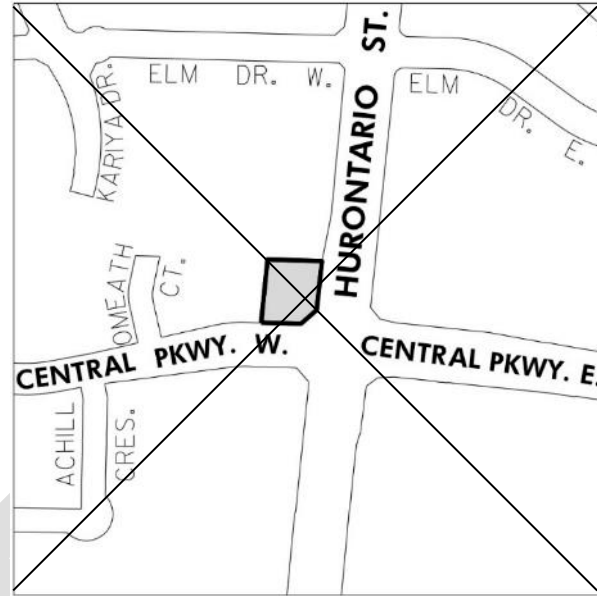
- a. three apartment buildings with maximum heights of 35, 40 and 50 storeys with a total maximum floor space index (FSI) of 8.2 will be permitted;
- b. commercial uses will be permitted; and
- c. access to the site will only be permitted from Kariya Drive and Elm Drive West.

~~12.3.2.2 Site 2~~

~~12.3.2.2.1 The lands identified as Special Site 2 are located on the west side of Hurontario Street, south of Central Parkway West.~~

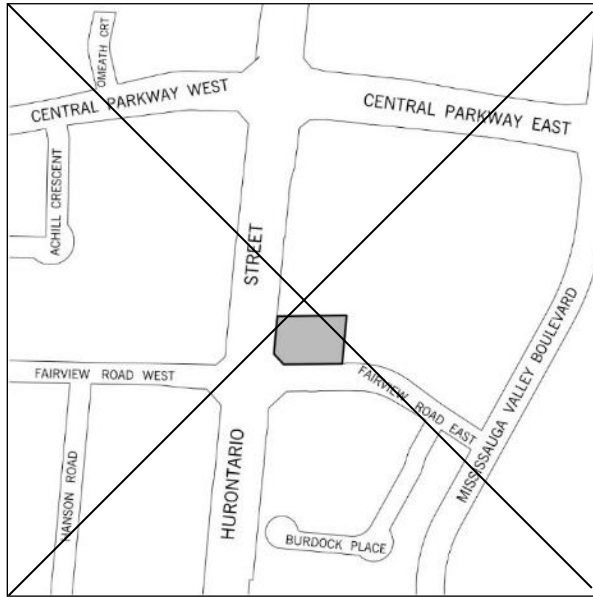
~~12.3.2.2.2 Notwithstanding the provisions of the Residential High Density designation and applicable policies, the following additional policies will apply:~~

- ~~a. the total number of units constructed on the site will not exceed 135; and~~
- ~~b. a maximum *floor space index (FSI)* of 3.75 will be permitted.~~

~~12.3.2.3 Site 3~~

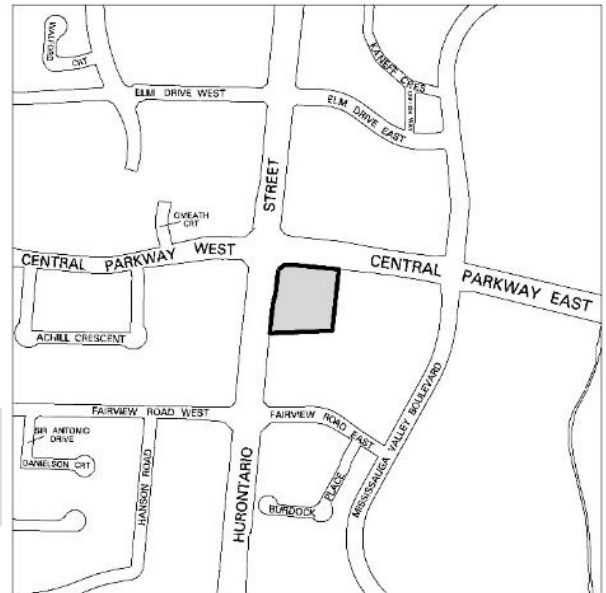
~~12.3.2.3.1 The lands identified as Special Site 3 are located at the northwest corner of Central Parkway West and Hurontario Street.~~

~~12.3.2.3.2 Notwithstanding the policies of this Plan, an apartment dwelling with a maximum height of 36 storeys will be permitted.~~

~~12.3.2.4 Site 4~~

~~12.3.2.4.1 The lands identified as Special Site 4 are located at the northeast corner of Fairview Road East and Hurontario Street.~~

~~12.3.2.4.2 Notwithstanding the policies of this Plan, an apartment building with a maximum height of 32 storeys will be permitted.~~

12.3.9 Exempt Site Policies**12.3.9.1 Site 1**

12.3.9.1.1 The lands identified as Exempt Site 1 are located on the southeast corner of Hurontario Street and Central Parkway East.

12.3.9.1.2 Notwithstanding the policies of this Plan, motor vehicle commercial uses will also be permitted.

Notes:

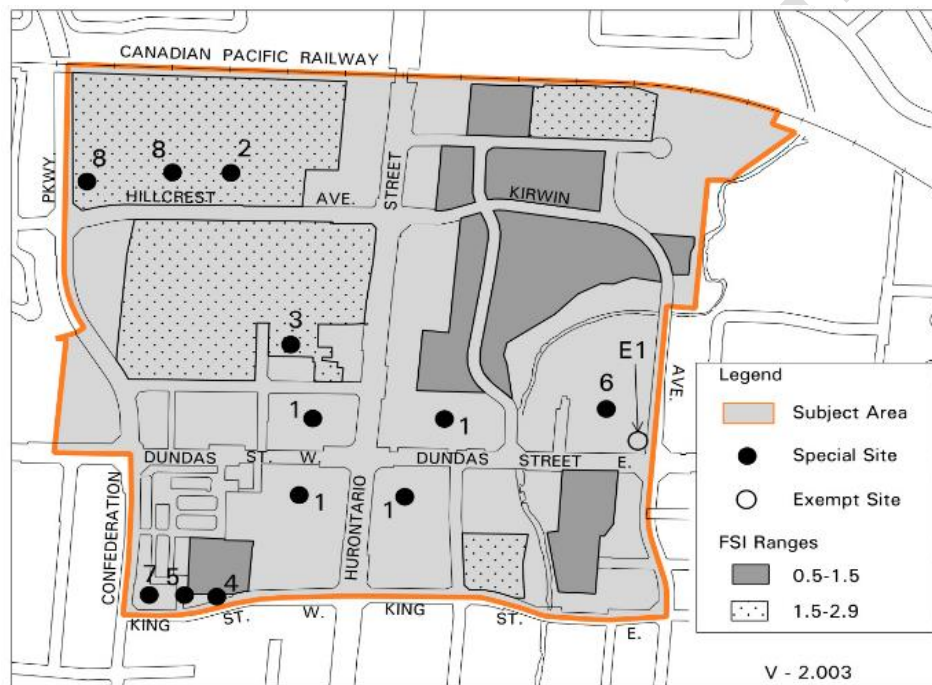
Existing policies are shown in the black text. Proposed new changes are shown in the red text. Proposed deletions to existing policies are shown as ~~strike-outs~~. Text and maps shown in a blue box have been included in this document for information purposes only and are intended to be included in a broader amendment related to Major Transit Station Areas to conform to the Region's Official Plan Amendment.

thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provides an important function in helping families transition to a new country.

Hurontario Street and Dundas Street is generally characterized by retail plazas that are one to two storeys with residential and/or office uses above and surrounded with surface parking. There are a few office and residential buildings along the corridor that are generally of a low or mid-rise form.

With the potential for two-way, all-day GO Transit service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and the Hurontario Light Rail Transit (LRT), significant transit investments will strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment and other destinations, while also attracting more visitors. Transit-oriented development will ensure transit investments are supported with greater access to housing, jobs and services.

12.4 Downtown Cooksville



Map 12-4: Downtown Cooksville Character Area

Changes to Map 12-4:

- Delete FSI ranges
- Delete Special Sites 2, 3, 6 and 8
- Add new Special Site – Special Site 9

12.4.2 Downtown Cooksville as a 15-Minute City

12.4.1 Introduction

Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and

Downtown Cooksville will continue to reflect its welcoming and multicultural heritage while integrating key elements of a 15-minute city concept. Downtown Cooksville is walkable and liveable, where the basic needs of everyday life are accessible and can easily be reached within a 15-minute walk. It has amenities that serve residents – parks, groceries, jobs, medical clinics, pharmacies, schools, shops, restaurants, and more –

that can be conveniently accessed without the use of a car.

12.4.3 Vision

Downtown Cooksville will be a walkable mixed use community with places for culture and art. Hurontario and Dundas streets will be animated with storefronts and other active uses that support local businesses. The area immediately surrounding the intersection of Hurontario and Dundas (Cooksville Corners) will be developed with a vibrant mainstreet focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit oriented development with a concentration and mix of residential, community, office, retail and service commercial uses.

The vision is based on the following guiding principles:

1. Create a mixed use, vibrant community – mix of local and unique businesses along the Hurontario and Dundas streets, spaces for educational, cultural and recreational uses, a vibrant public realm, and a network of connected parks and open spaces;
2. Plan for more housing and people – provide new housing and jobs with the greatest heights and densities located at the Cookville GO Station;
3. Achieve a walkable, connected community - pedestrian-oriented mainstreet character along Hurontario and Dundas streets, improved pedestrian and cycling infrastructure, public art and public realm; and
4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and higher-order transit service.

12.4.4 General

12.4.4.1 Arts and cultural uses are encouraged to concentrate in Downtown Cooksville to add vibrancy to the area.

12.4.4.2 In accordance with the City's housing strategy, the City will require that development be supported by a range of housing options for residents, such as a mix of tenure and the provision of units that are affordable to middle income households.

12.4.4.3 Development will explore partnerships with the Region of Peel and non-profit housing organizations to provide housing with deeper affordability to lower income households and to contribute towards Peel-wide needs-based housing targets on density, rental tenure, and affordability.

12.4.4.4 Mississauga will encourage partnerships and collaborations to identify needs and develop community infrastructure such as integrating a school or a daycare facility within the podium of a building.

12.4.4.5 The City will work in collaboration with the school board(s) to determine the need for educational facilities. The location and size of a school facility will be confirmed through the review of development applications.

12.4.4.6 Downtown Cooksville will develop with a high quality and well-designed network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections, to existing and future parks and open spaces, commercial developments, community facilities and to surrounding neighbourhoods.

12.4.4.7 As part of the development application review process, the City will determine the location, configuration and size of the parkland block(s), and the requirement for land dedication. Opportunities to obtain parkland will also be explored through purchase by the City.

12.4.4.8 The City will explore opportunities to further increase the provision of public open space by establishing public easements and entering into future agreements, where appropriate. These open spaces will be complementary to the public parkland system.

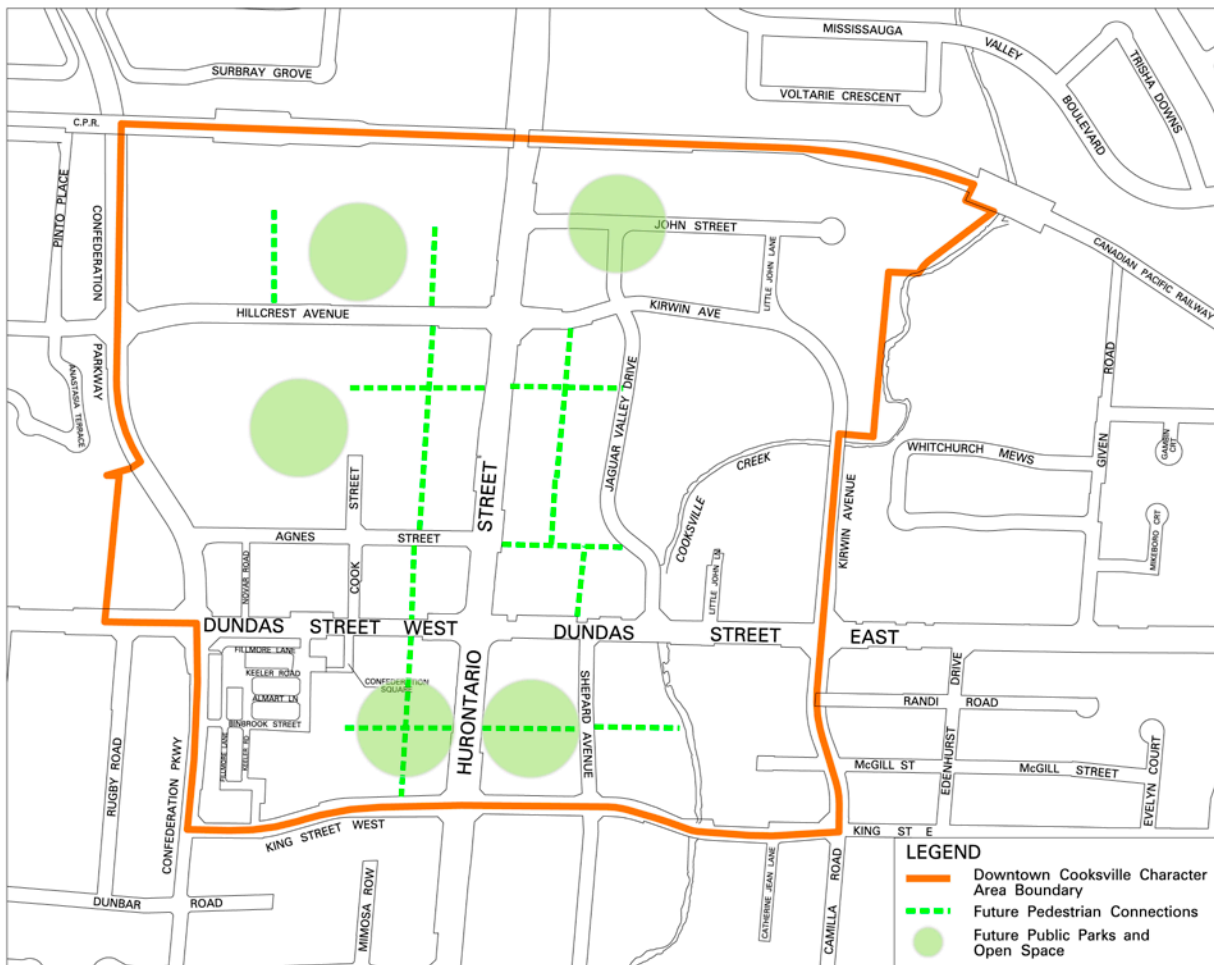
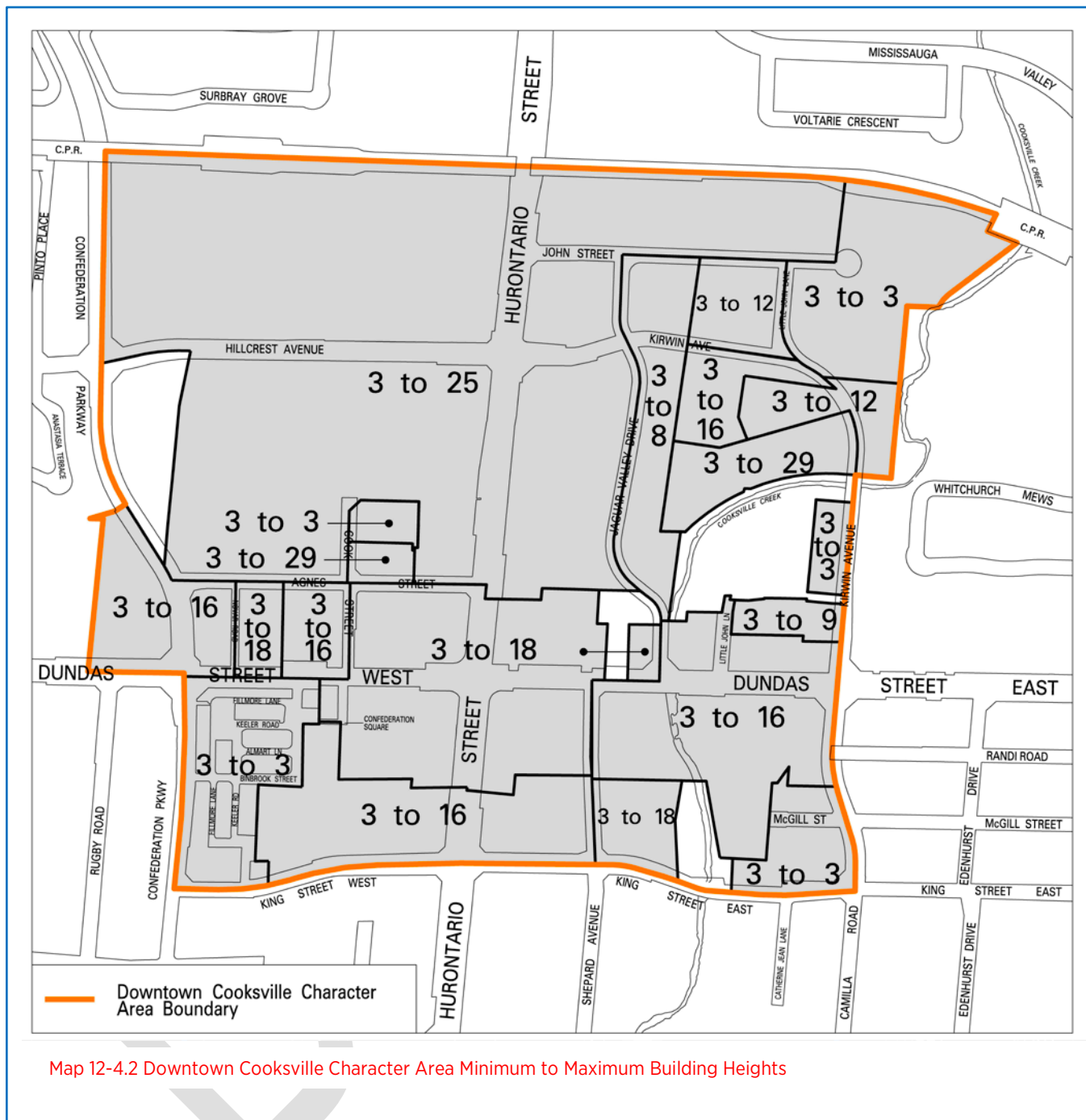


Figure 1: Future pedestrian connection and public parkland and open space network. These symbols represent the general location of new pedestrian connections, as well as public parkland and open spaces in the vicinity.

12.4.5 Building Height

12.4.5.1 The greatest heights will be located at the Cooksville GO station and along Hurontario Street north of Agnes Street. Building heights in Downtown Cooksville will not exceed the maximum limits as shown on Map 12-4.2: Downtown Cooksville Character Area Minimum to Maximum Building Heights.



12.4.1 ~~Urban Design Policies~~

Overall Character

~~12.4.1.1 A high level of urban design, pedestrian amenity, and intensity of development is encouraged along principal street frontages. A sense of entry to the Character Area should be articulated at these locations by prominent built form, landscaping and signage components.~~

~~12.4.1.2 The sections of Hurontario Street and Dundas Street within the Character Area should function as a focus for the Cooksville Neighbourhood, having a strong sense of place and main street character with active mixed use building frontages and highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.~~

Character of Dundas Street and Hurontario Street

~~12.4.1.3 Street Edge Uses Development abutting the street should encourage a high level of activity along the street by incorporating grade related retail with residential and/or offices above. Retail units should be clearly oriented to, and accessed from, the public sidewalk.~~

~~12.4.1.4 Street Scale and Enclosure Development should be closely related to, and integrated with, the public sidewalk to focus activity on the street and provide a sense of spatial enclosure for the street. Development should address the following:~~

- ~~a. limited building setback range of three to five metres from the street line, with the larger setback in areas of high transit or pedestrian use;~~
- ~~b. minimum building height of two to four storeys and maximum of six storeys directly abutting the street line;~~
- ~~c. maximum continuity of street walls with built form occupying a minimum of 80% of the street frontage; and~~
- ~~d. a minimum setback of ten metres from the street line is required for buildings exceeding six storeys in height.~~

~~12.4.1.5 Bulk and Massing Development should be generally consistent in its bulk, massing and scale within the Character Area with use of taller more prominent buildings to highlight the Dundas and Hurontario Street intersection. A general height progression should be encouraged from this intersection to the Character Area edges and abutting the Downtown Hospital Character Area. A ratio of 1:2 (building height to eventual street width) should be provided by built form abutting Dundas and Hurontario Streets.~~

~~12.4.1.6 Development Fabric Development fronting on the Hurontario Street and Dundas Street frontages should reinforce patterns and characteristics supportive of a main street role with highly animated pedestrian spaces. Development should address the following:~~

- ~~a. commercial frontages should be broken up into smaller retail units and accessed directly from the public sidewalk with frequent access doorways;~~
- ~~b. no parking should be provided between the buildings and street line;~~
- ~~c. blank walls should be avoided along the street in favour of fenestration;~~
- ~~d. service, loading and garbage storage areas should be accessed from rear lanes or abutting side streets;~~
- ~~e. split level commercial frontages should be avoided;~~
- ~~f. periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;~~
- ~~g. storefront signage should respect the pedestrian scale and architectural character of development;~~
- ~~h. pedestrian weather protection should be provided in the form of canopies and/or inset arcades; and~~
- ~~i. the development and integration of rear yard parking lots to reduce the number of driveways along Hurontario Street and Dundas Street.~~

~~12.4.1.7 Streetscape (Open Space and Landscaping) Development of private land abutting the sidewalk~~

should closely coordinate with the public boulevard to create an integrated design character at the pedestrian level and to reinforce the sense of a community main street. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied *streetscape*.

~~12.4.1.8 Private Development Zone – Development should address the following:~~

- ~~a. front yard setback areas should be provided with site furnishings, landscaping and paving materials, coordinate with the public boulevard design, and extend the usability of the public sidewalk; and~~
- ~~b. opportunities for pedestrian spaces and circulation areas should be promoted at intersection corners, major building entrances and terminations of vistas.~~

~~12.4.1.9 Public Realm: The Design of the Public Boulevard – The following features should be encouraged to reduce the perceived visual width of the street and improve the level of pedestrian comfort, safety and convenience within the public boulevard:~~

- ~~a. common paving materials and patterns, street furniture and signage;~~
- ~~b. road crossings defined by special paving;~~
- ~~c. intersection design to moderate speed of turning traffic in favour of pedestrian movements, i.e. avoid the construction of designated right turn lanes with islands;~~
- ~~d. setting back of bus bays from intersection corners to allow sidewalks to project;~~
- ~~e. curb edge parking; and~~
- ~~f. provision of street trees, feature lighting and related pedestrian amenities.~~

12.4.6 Urban Form

12.4.6.1 General

12.4.6.1.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:

- a. create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to low and medium density residential areas;
- b. maintain a minimum separation distance, generally 30 metres between portions of buildings that are greater than six storeys;
- c. add visual interest by varying the use of massing and materials; and
- d. promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.

12.4.6.1.2 For larger developments where more than one tower is proposed, variation in heights of three to five storeys will be achieved.

12.4.6.1.3 *Tall buildings* will incorporate podiums that are generally a minimum of three storeys and a maximum of six storeys.

12.4.6.1.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For *tall buildings*, the tower above the podium will have a limited floor plate size.

12.4.6.2 Street Frontage Provisions

The majority of existing and new roads in Downtown Cooksville are categorized as 'A', 'B' or 'C' Streets and are shown on Map 12-4.3: Downtown Cooksville Street Types.

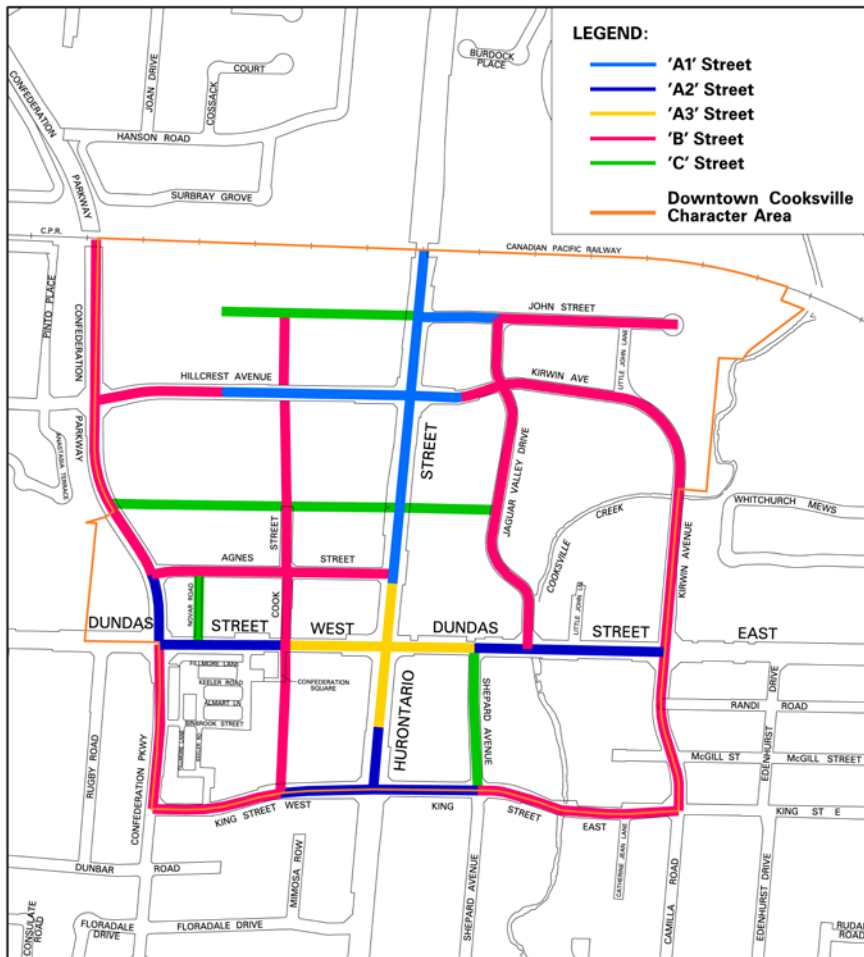
'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses. The built form and streetscape treatment of 'A' streets will be designed to foster active uses and a pedestrian friendly environment.

The built form along 'B' Streets will be primarily residential in character. The design of the built form and streetscape along 'B' streets will help establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms.

'C' Streets are tertiary streets that connect to 'A' or 'B' Streets. 'C' Streets are intended to support a pedestrian environment by integrating a high standard

of urban design. These streets will also provide for vehicular access to on-site service, loading, parking and garbage storage areas.

b. frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;



Map 12-4.3 Downtown Cooksville Street Types

c. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;

d. design retail and service commercial units at street corner locations with animated storefronts that wrap the corner;

e. generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios;

f. appropriate stepbacks between the edge of the podium and tower portion of the building; and

g. development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character and to reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied *streetscape*.

12.4.6.2.1 Provisions for 'A' Streets

12.4.6.2.1.1 'A' Streets will have a strong sense of place and main street character with active, mixed use building frontages and a highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.

12.4.6.2.1.2 Development abutting the street will incorporate grade related retail with residential and/or offices above. Development will address the following:

a. promote a mainstreet character with smaller retail units and frequent entrances accessible from the public sidewalk;

12.4.6.2.2 Provisions for 'B' Streets

12.4.6.2.2.1 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature, development may include some commercial uses on the ground floor. Development will address the following:

a. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;

b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, and pedestrian scaled lighting;

- c. a high quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy; and
- d. appropriate setbacks between the edge of the podium and tower portion of the building.

12.4.6.2.3 Provisions for 'C' Streets

12.4.6.2.3.1 Development along 'C' Streets will support a pedestrian environment and vehicular access to parking and other service areas such as deliveries, garbage pick-up, service and loading. 'C' Streets may also support residential, retail and service commercial uses. Development should address the following:

- a. opportunities to consolidate service, parking and loading to serve a number of buildings;
- b. ensure the visual impact of the parking, loading and service areas from the street is minimized and screened from view; and
- c. provide a secondary entrance for pedestrian access, where appropriate.

12.4.27 Transportation

~~12.4.2.1 All development applications are required to address the relationship with adjacent properties and the need to minimize access onto Hurontario Street and Dundas Street.~~

~~12.4.2.2.1~~ A future signalized intersection with Hurontario Street is proposed in the vicinity of the former Melissa Street right-of-way along the southerly boundary of T.L. Kennedy Secondary School. This will connect with Cook Street initially and, with the potential redevelopment of the school site, may extend to Confederation Parkway in conjunction with the possible future extension of Cook Street northerly to the east GO Access Road. Redevelopment proposals in this area will consider these options.

~~12.4.2.3 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and~~

~~implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.~~

12.4.7.2 Development will be required to provide pedestrian connections to connect to the Hurontario Light Rail Transit (LRT), transit routes/stops, trails, and parks and open spaces.

12.4.7.3 Public easements will be required where pedestrian connections are proposed on private lands.

12.4.7.4 Vehicular access from Hurontario Street and Dundas Street will be restricted and consolidated to minimize driveways and disruption to pedestrians, cyclists and transit.

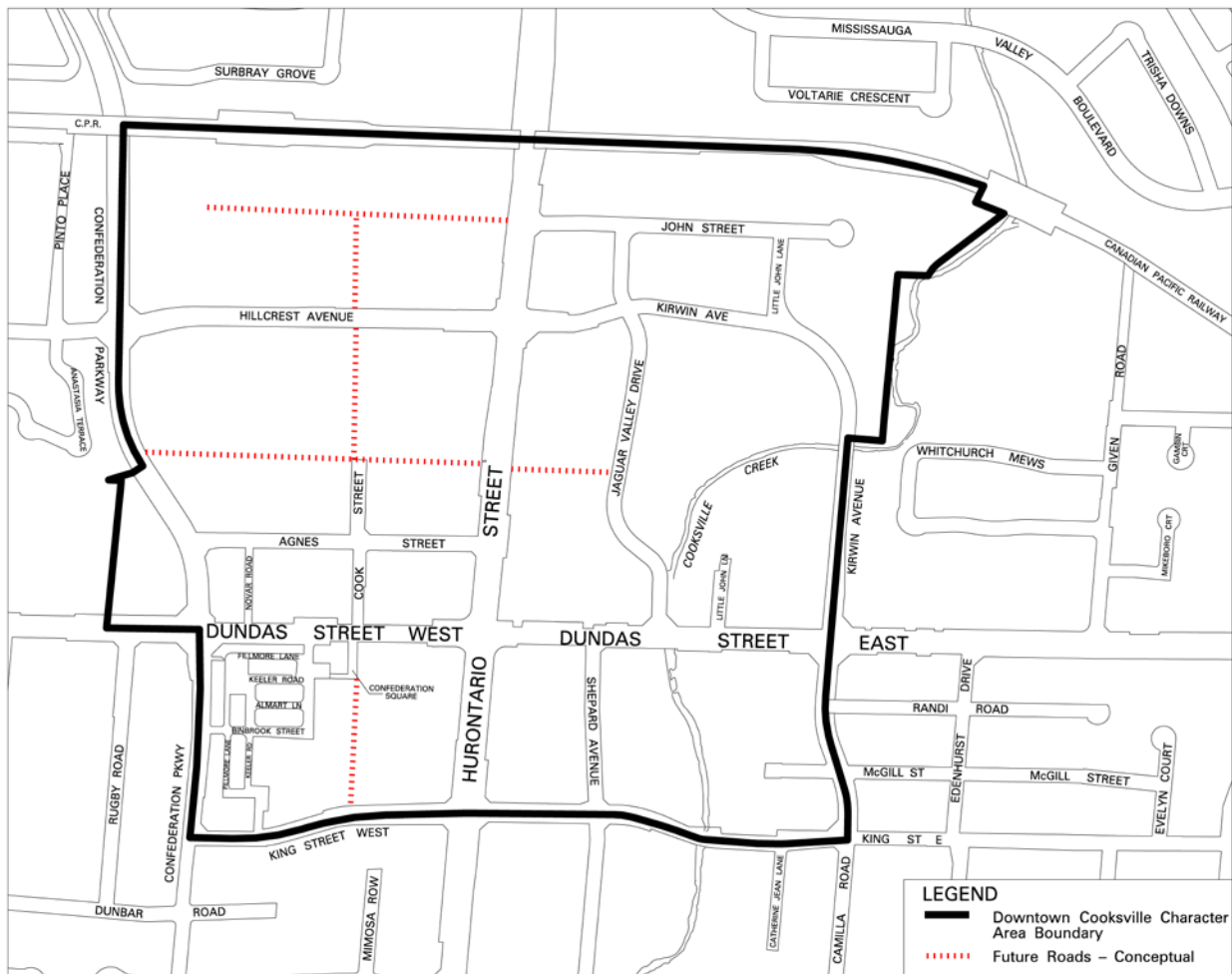
12.4.7.5 Roads will be designed as complete streets. New roads will be designed to incorporate active transportation and transit infrastructure.

Existing arterial and collector roads dissecting and surrounding the Character Area will be redesigned to incorporate active transportation and transit infrastructure, as appropriate.

12.4.7.6 The road network identified on Map 12-4.3 will support improved connectivity within Downtown Cooksville for pedestrian and cycling movement and to transit.

12.4.7.7 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. all roads will be public;
- b. the design and access requirements for roads will be determined through the development application review process; and
- c. minor adjustments to the road network may be made without amendment to Map 12-4.3 at the City's discretion to accommodate block development, while maintaining the goal of breaking up large blocks with roads.



Map 12-4.3 Downtown Cooksville Character Area Future Roads

12.4.8 Land Use

12.4.8.1 Notwithstanding the policies of this Plan, the following additional policies will apply to new development along Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street) and King Street (new Cook Street to Shepard Avenue):

- a. at grade retail and service commercial uses will be required; and
- b. residential dwellings will not be permitted at grade.

12.4.8.2 Mixed Use

12.4.8.2.1 Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted unless it can be demonstrated that the planned function of the non-residential component will be maintained or replaced as part of the redevelopment.

12.4.8.2.2 Notwithstanding the policies of this Plan, the following additional uses will be permitted:

- a. banquet hall;
- b. conference centre; and
- c. entertainment, recreation and sports facilities.

12.4.8.2.3 For the purposes of the policies in this section, maintaining the non-residential planned function of the Mixed Use designation means providing:

- a. a concentration of convenient, easily accessible office, retail and service commercial uses such as a grocery store that meet the needs of local residents and employees; and
- b. employment opportunities, such as office and institutional jobs; and

12.4.8.3 Office

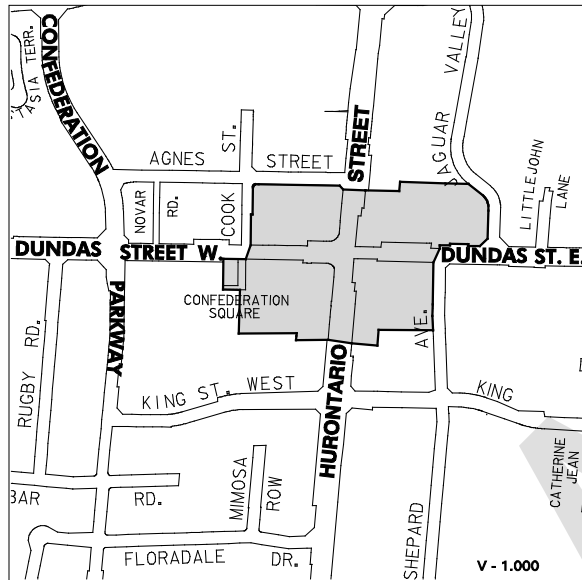
12.4.8.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be

permitted, unless the same amount of office space is retained or replaced through development.

12.4.3.9 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

12.4.3.9.1 Site 1



12.4.3.9.1.1 The lands identified as Special Site 1, known as Cooksville Corners, are the lands surrounding the intersection of Hurontario Street and Dundas Street.

12.4.3.9.1.2 Notwithstanding the provisions of the Mixed Use and Utility designations, the Urban Design Policies, and other provisions of policies of this Plan, the following additional policies will apply:

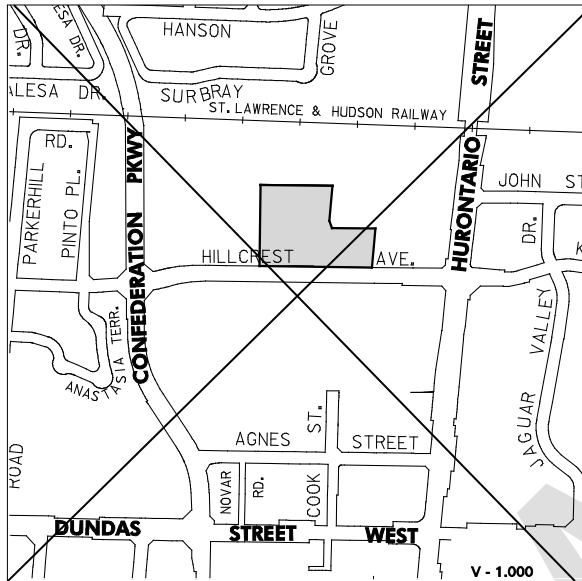
- a. development up to a **floor space index (FSI)** of 1.0 will be permitted for street related retail, office, and community uses. Development between an **floor space index (FSI)** of 1.0 and 2.9 will be permitted at a ratio of 80% residential, office, community uses and overnight accommodation uses and 20% retail uses;
- b. principal building entries and related forecourts are to be provided at the Dundas and Hurontario streets intersection. Dundas and Hurontario street frontages are to be developed with retail uses having direct access to the public street and

~~continuous weather protection by an arcade and/or projecting awnings;~~

- e. ~~internal pedestrian courtyards with retail/restaurant uses will be permitted subject to: the provision of integral walkways to the public street; visibility from the sidewalk; and, such courtyards being clearly secondary to street related retail uses;~~
- d. minimum building heights of three storeys and a maximum of **six 18** storeys directly abutting Dundas and Hurontario street frontages ~~are encouraged~~ to create an appropriate **main** street scale **of development** and enclosure relationship. Development exceeding six storeys to a maximum of 18 storeys will be considered subject to a minimum setback of ten metres from the building edge abutting the public sidewalk. Terracing of taller built form will be encouraged; and
- e. the lands located at the southeast corner of Agnes Street and Cook Street can be utilized on an interim basis for parking for the Mixed Use development to the west. When these lands are developed, they are to be in compliance with the Residential High Density designation. The majority of parking for the mixed retail and residential development shall be accommodated in a below-grade parking structure.
- f. **tall buildings** will incorporate podiums that are reflective of a main street character;
- g. buildings along Hurontario Street and Dundas Street will incorporate a generous setback between the edge of the podium and tower portion of the building;
- h. a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are adjacent to Hurontario Street and Dundas Street; and
- i. to achieve vibrant walkable spaces, buildings along pedestrian connections will promote animation at-grade with outdoor patios and

storefront entrances to individual retail and service commercial units.

12.4.3.2 Site 2

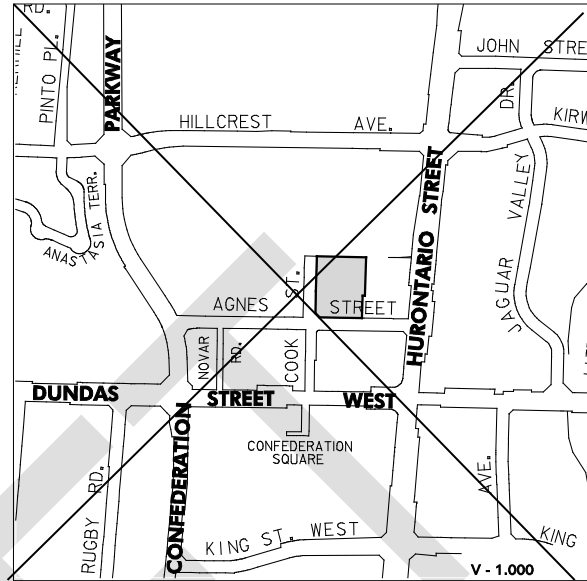


12.4.3.2.1 The lands identified as Special Site 2 are located on the north side of Hillcrest Avenue, west of Hurontario Street.

12.4.3.2.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

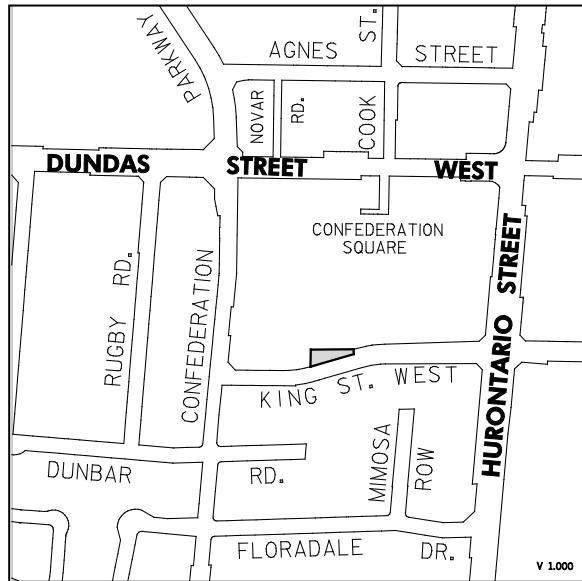
- a. office development will be permitted at a maximum Gross Floor Area (GFA) of 61 439 m², of which a maximum of 9 290 m² will be used for accessory uses; and
- b. apartments will be permitted at a maximum *floor space index (FSI)* of 1.5 - 2.9.

12.4.3.3 Site 3



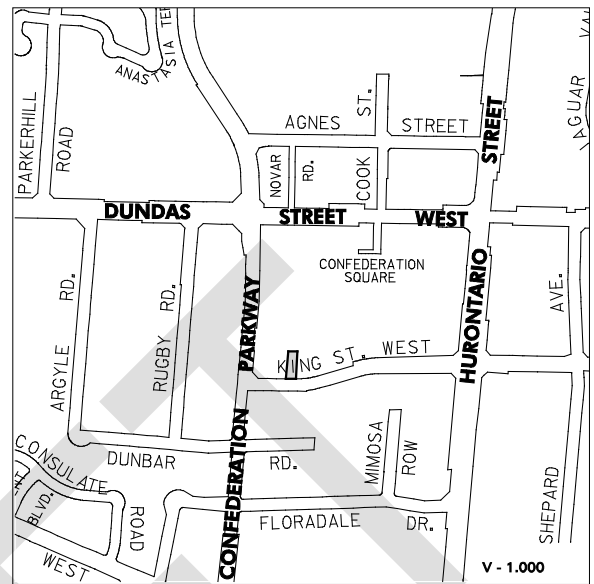
12.4.3.3.1 The lands identified as Special Site 3 are located at the northeast corner of Agnes Street and Cook Street.

12.4.3.3.2 Notwithstanding the policies of this Plan, a maximum of 27 townhouse dwellings and a 28 storey apartment building will be permitted.

12.4.3.9.4 Site 4

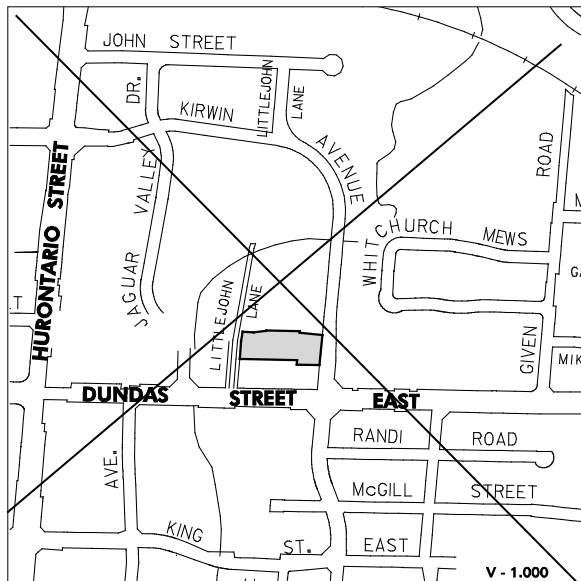
12.4.3.9.4.1 The lands identified as Special Site 4 are located on the north side of King Street West, east of Confederation Parkway.

12.4.3.9.4.2 Notwithstanding the provisions of this Plan, these lands must only be developed as part of a comprehensive development with adjacent lands designated Residential High Density.

12.4.3.9.5 Site 5

12.4.3.9.5.1 The lands identified as Special Site 5 are located on the north side of King Street West, east of Confederation Parkway.

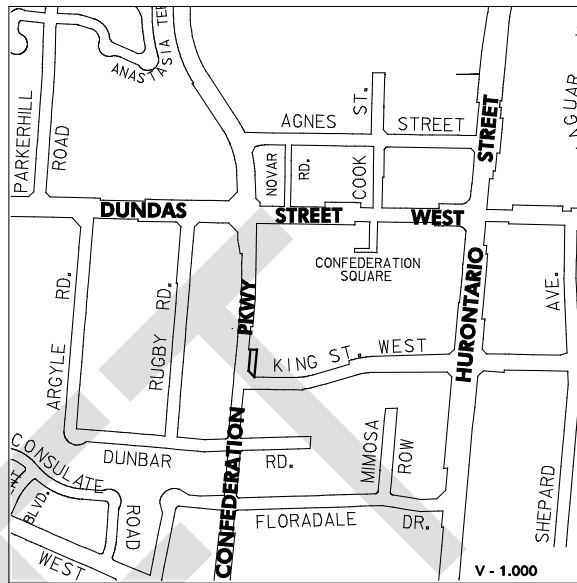
12.4.3.9.5.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

12.4.3.6 Site 6

12.4.3.6.1 The lands identified as Special Site 6 are located on the west side of Kirwin Avenue, north of Dundas Street East.

12.4.3.6.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

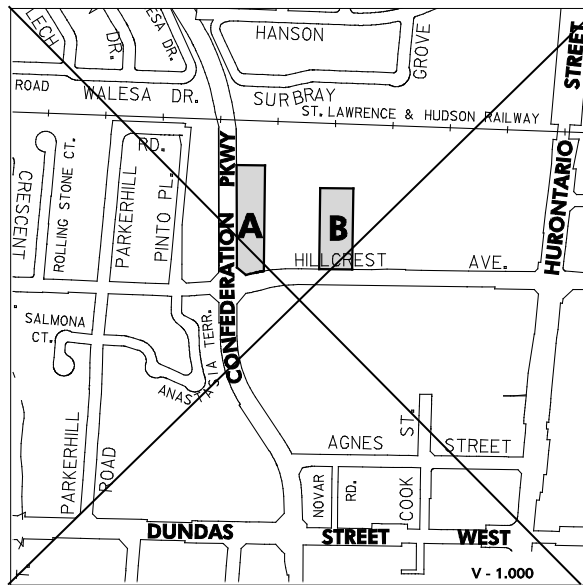
- a. a hotel not exceeding 42 storeys will be permitted.

12.4.3.9.7 Site 7

12.4.3.9.7.1 The lands identified as Special Site 7 are located on the northeast corner of King Street West and Confederation Parkway.

12.4.3.9.7.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

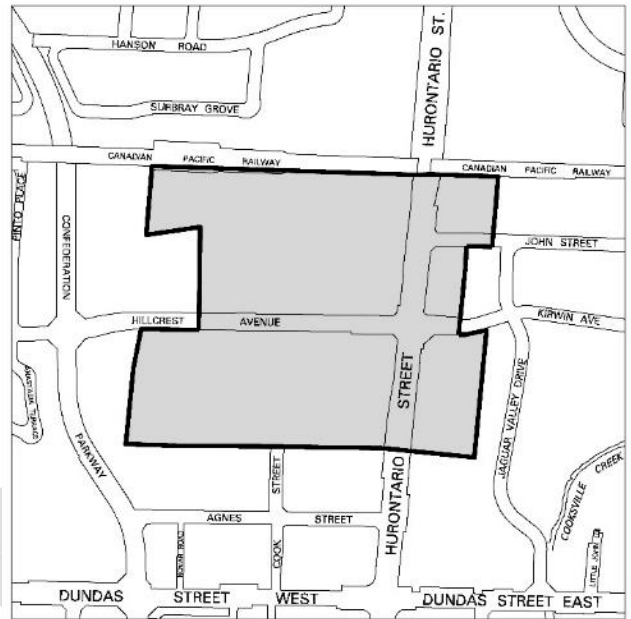
12.4.3.8 Site 8



12.4.3.8.1 The lands identified as Special Site 8 are located on the north side of Hillcrest Avenue, east of Confederation Parkway.

12.4.3.8.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 8A will be permitted to develop to a maximum *floor space index (FSI)* of 3.5 and the lands identified as Area 8B will be permitted to develop to a maximum *floor space index (FSI)* of 4.0.

12.4.9.9 Site 9



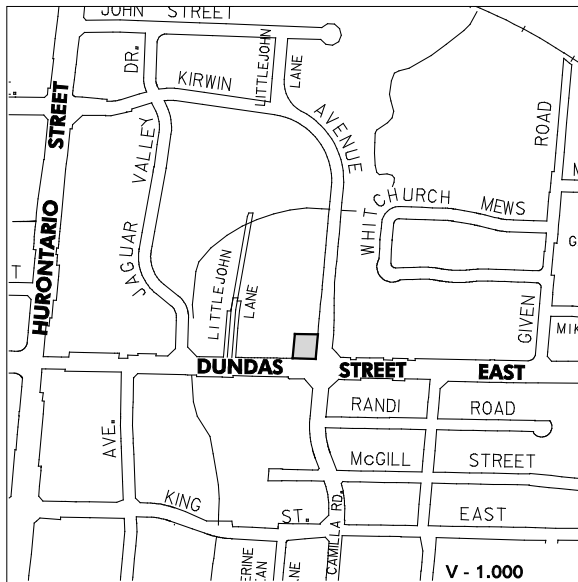
12.4.3.9.1 The lands identified as Special Site 9 are generally located on the east and west sides of Hurontario Street between the Canadian Pacific Railway and Agnes Street.

12.4.3.9.2 Notwithstanding the policies of this Plan the following additional policy will apply:

- a. a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are adjacent to Hillcrest Avenue, Hurontario Street, John Street, and Kirwin Avenue.

12.4.4-10 Exempt Site Policies

12.4.4-10.1 Site 1



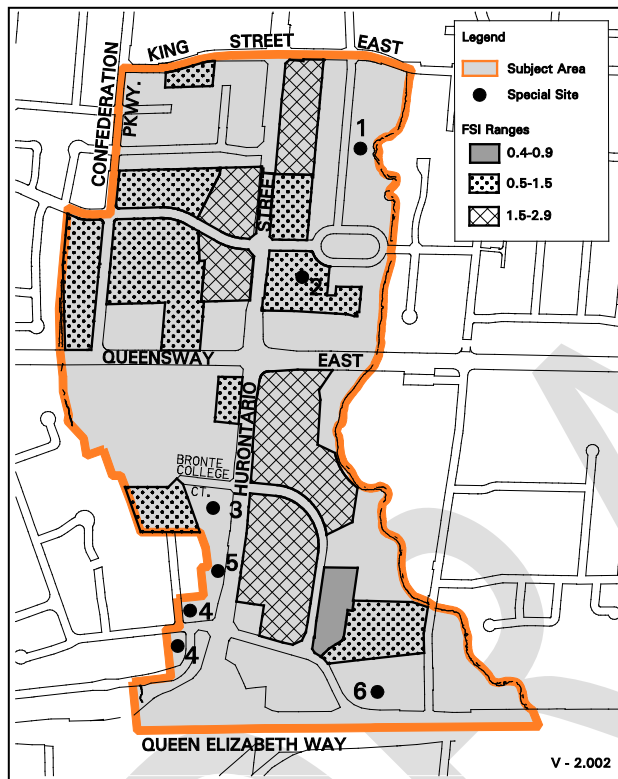
12.4.4-10.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Dundas Street East and Kirwin Avenue.

12.4.4-10.1.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle commercial uses will also be permitted.

Notes:

Existing policies are shown in the black text. Proposed new changes are shown in the **red text**. Proposed deletions to existing policies are shown as **strike-outs**. Text and maps shown in a **blue box** have been included in this document for information purposes only and are intended to be included in a broader amendment related to Major Transit Station Areas to conform to the Region's Official Plan Amendment.

12.5 Downtown Hospital



Map 12-5: Downtown Hospital Character Area

Changes to Map 12-5:

- Delete FSI Ranges
- Delete Special Sites 2 to 5
- Add a new Special Site - Special Site 7

~~12.5.1.1 For lands designated Residential Medium Density, building height should not exceed a maximum of four storeys unless otherwise specified in the Urban Design policies of this Plan.~~

12.5.1 Introduction

The focal point of Downtown Hospital continues to be the Trillium Mississauga Hospital, serving as one of Canada's leading health centres with specialized

regional programs. Downtown Hospital consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial is clustered at three main intersections along Hurontario Street at King Street, Queensway and North Service Road.

Cooksville Creek is a significant natural feature and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management.

The open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, additional parkland and improvements to existing parks are planned.

12.5.2 Vision

Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Trillium Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

Transit-oriented development along Hurontario and Queensway will ensure transit investments are supported with greater access to housing, jobs and services.

The vision is based on the following guiding principles:

1. Support health care services – promote a range of health care services, supportive uses and research and development; and
2. Create a more walkable and transit-oriented community – provide additional pedestrian connections to Hurontario Street and Queensway and incorporate street related retail and service commercial uses along these corridors.

12.5.3 General

12.5.3.1 In accordance with the City's housing strategy, the City will require that development be supported by a range of housing options for residents, such as a mix of tenure and the provision of units that are affordable to middle income households.

12.5.3.2 Development will explore partnerships with the Region of Peel and non-profit housing organizations to provide housing with deeper affordability to lower income households and to contribute towards Peel-wide needs-based housing targets on density, rental tenure, and affordability.

12.5.3.3 Mississauga will encourage partnerships and collaborations to identify needs and develop community infrastructure such as integrating a school or a daycare facility within the podium of a building.

12.5.3.4 The City will work in collaboration with the school board(s) to determine the need for educational facilities. The location and size of a school facility will be confirmed through the review of development applications.

12.5.3.5 Downtown Hospital will develop with a high quality and well-designed network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections to existing and future parks and open spaces, commercial developments, community facilities and to surrounding neighbourhoods.

12.5.3.6 As part of the development application review process, the City will determine the location, configuration and size of the parkland block(s), and the requirement for land dedication. Opportunities to obtain parkland will also be explored through purchase by the City.

12.5.3.7 The City will explore opportunities to further increase the provision of public open space by establishing public easements and entering into future agreements, where appropriate. These open spaces will be complementary to the public parkland system.

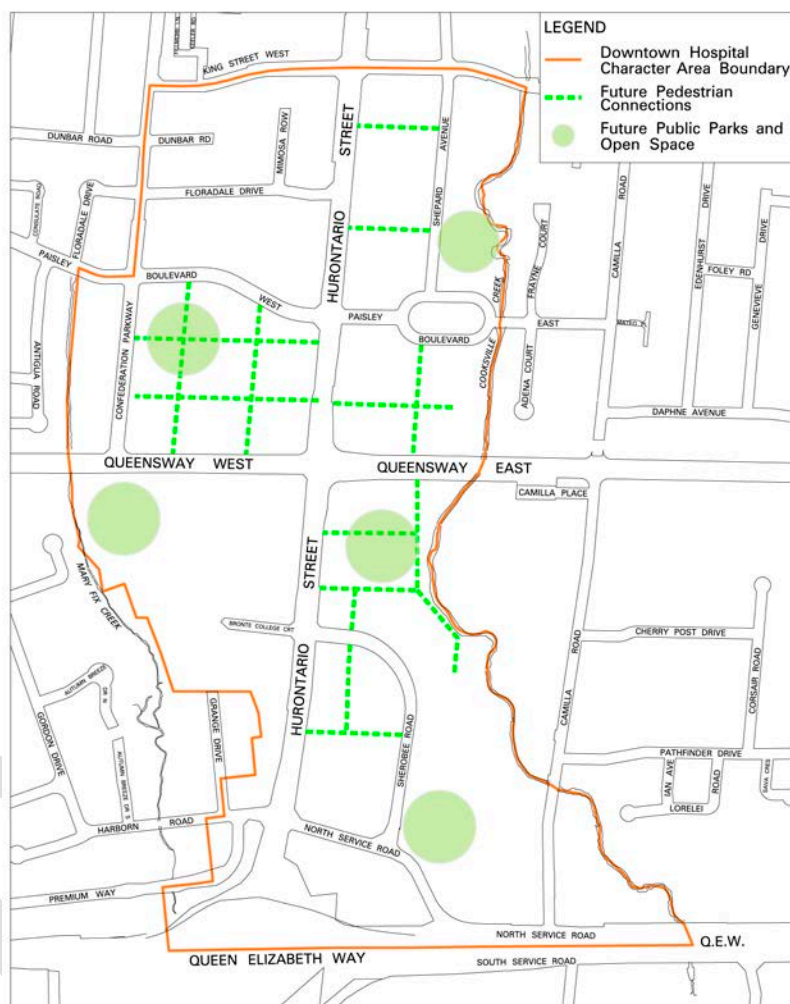
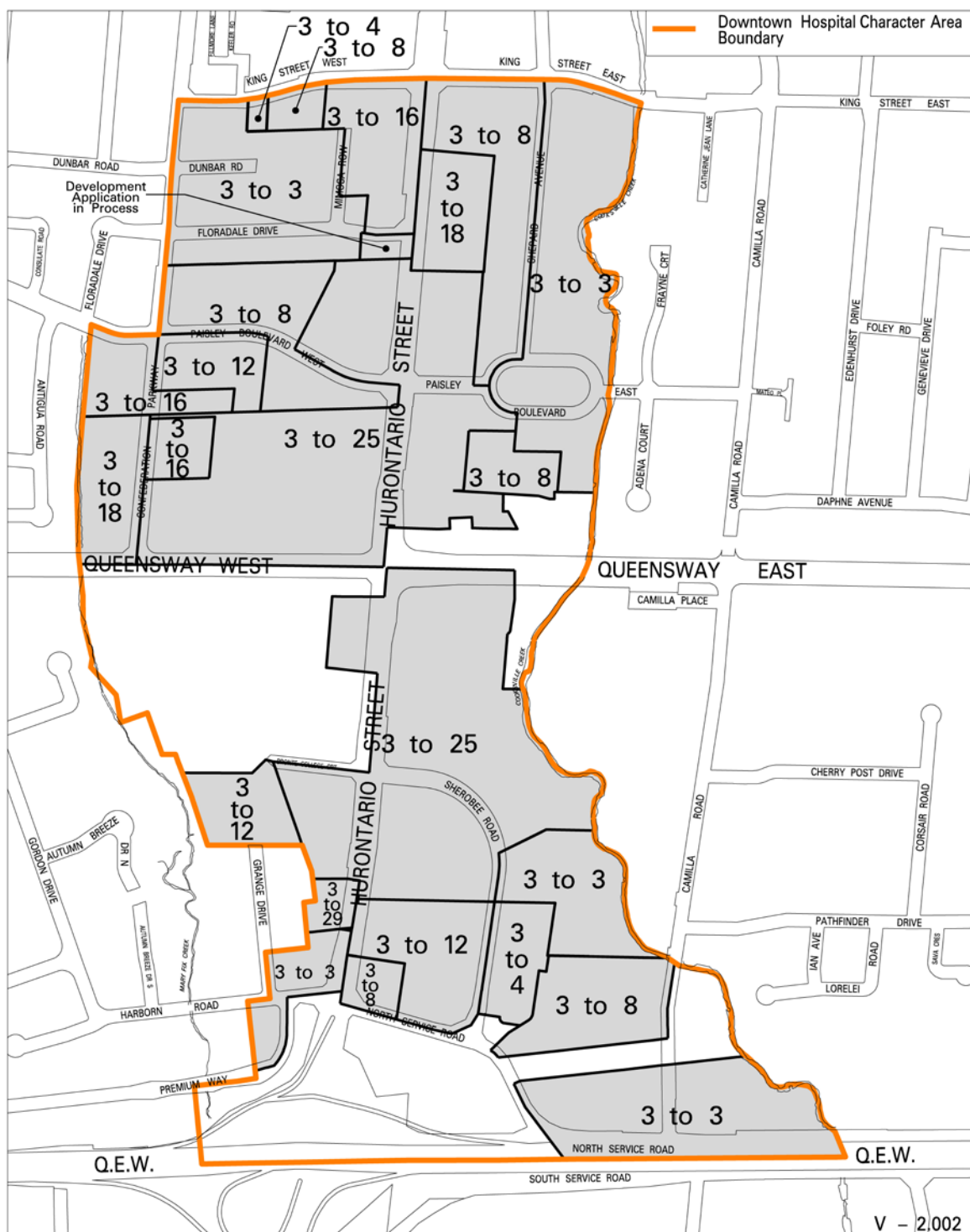


Figure 1: Future pedestrian connection and public parkland and open space network. These symbols represent the general location of new pedestrian connections, as well as public parkland and open spaces in the vicinity.

12.5.4 Building Height

12.5.4.1 The greatest heights will be located at the rapid transit stop at Queensway and Hurontario Street. Building heights in Downtown Hospital will not exceed the maximum limits as shown on Map 12-5.2: Downtown Hospital Character Area Minimum to Maximum Building Heights.



Map 12-5.2 Downtown Hospital Character Area Minimum to Maximum Building Heights

12.5.5 Urban Form

12.5.5.1 General

12.5.5.1.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:

- create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to low and medium density residential areas;
- maintain a minimum separation distance, generally 30 metres between portions of buildings that are greater than six storeys;
- add visual interest by varying the use of massing and materials; and
- promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.

12.5.5.1.2 For larger developments where more than one tower is proposed, a variation in heights of three to five storeys will be achieved.

12.5.5.1.3 **Tall buildings** will incorporate podiums that are a minimum of three storeys and a maximum of six storeys.

12.5.5.1.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For **tall buildings**, the tower above the podium will have a limited floor plate size.

12.5.5.1.5 Along Hurontario Street, development will incorporate appropriate setbacks between the edge of the podium and tower portion of the building.

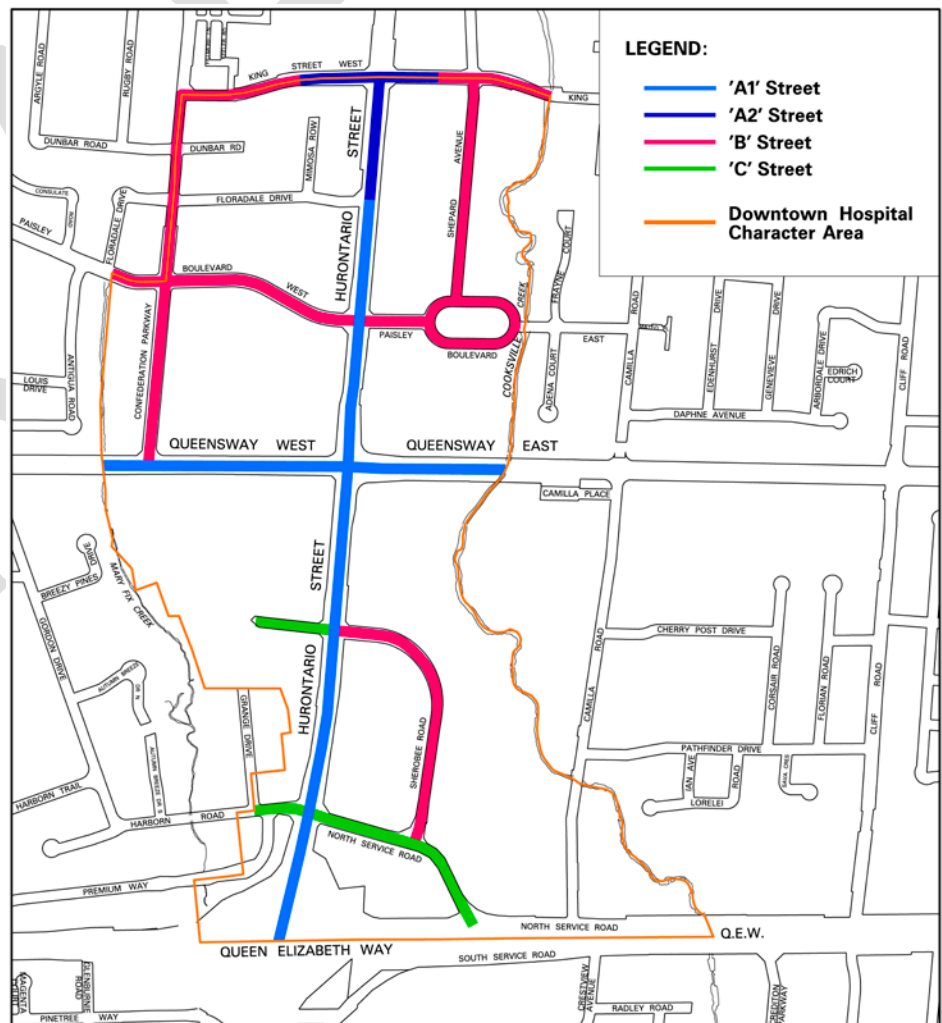
12.5.5.2 Street Frontage Provisions

The majority of existing and new roads in Downtown Hospital are categorized as 'A', 'B' or 'C' Streets and are shown on Map 12-5-3 Downtown Hospital Street Types.

'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses. The built form and streetscape treatment of 'A' streets will be designed to foster active uses and a pedestrian friendly environment.

The built form along 'B' Streets will be primarily residential in character. The design of the built form and streetscape along 'B' streets will help establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms.

'C' Streets are tertiary streets that connect to 'A' or 'B' Streets. 'C' Streets are intended to support a pedestrian environment by integrating a high standard of urban design. These streets will also provide for vehicular access to on-site service, loading, parking and garbage storage areas.



Map 12-5.3 Downtown Hospital Street Types

12.5.5.2.1 Provisions for 'A' Streets

12.5.5.2.1.1 'A' Streets will have a strong sense of place and main street character with active, mixed use building frontages and a highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.

12.5.5.2.1.2 Development abutting the street will incorporate grade related retail with residential and/or offices above. Development will address the following:

- a. promote a mainstreet character with smaller retail units and frequent entrances accessible from the public sidewalk;
- b. frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;
- c. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- d. design retail and service commercial units at corner locations with animated storefronts that wrap the corner;
- e. generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios;
- f. appropriate stepbacks between the edge of the podium and tower portion of the building; and
- g. development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character and to reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied *streetscape*.

12.5.5.2.2 Provision for 'B' Streets

12.5.5.2.2.1 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature,

development may include some commercial uses on the ground floor. Development will address the following:

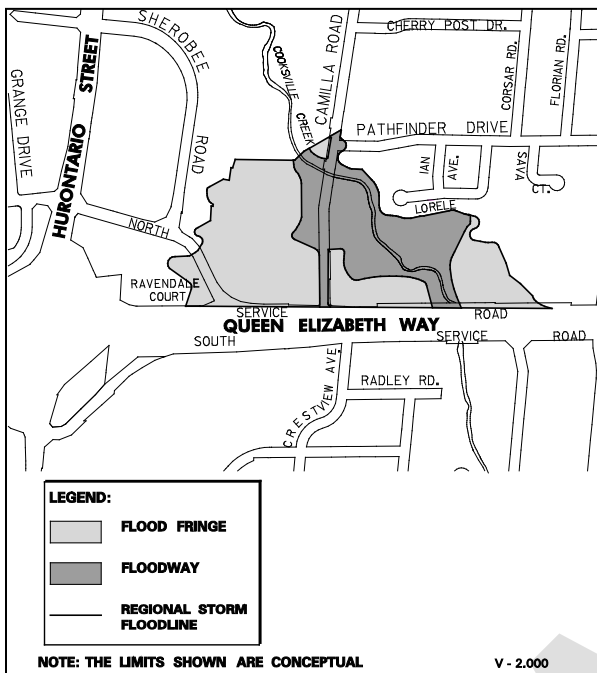
- a. a consistent streetwall will be required. Periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, and pedestrian scaled lighting;
- c. a high quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy; and
- d. appropriate stepbacks between the edge of the podium and tower portion of the building.

12.5.5.2.3 Provision for 'C' Streets

12.5.5.2.3.1 Development along 'C' Streets will support a pedestrian environment and vehicular access to parking and other service areas such as deliveries, garbage pick-up, service and loading. 'C' Streets may also support residential, retail and service commercial uses. Development should address the following:

- a. opportunities to consolidate service, parking and loading to serve a number of buildings;
- b. ensure the visual impact of the parking, loading and service areas from the street is minimized and screened from view; and
- c. provide a secondary entrance for pedestrian access, where appropriate.

12.5.2.6 Environmental Planning Area



Map 12-5.1-12-5.4: Cooksville Creek Floodplain Management Concept

12.5.2.6.1 The lands identified above are located generally between the North Service Road and where the Cooksville Creek crosses Camilla Road. The area subject to these policies within the Downtown Hospital Character Area, is generally located west of Cooksville Creek. The lands shown are subject to the *two-zone floodplain management concept*, which divides the regulatory floodplain into two portions known as the floodway and the flood fringe. The limits of the flood fringe and the floodway are conceptual, the exact limits of which will be determined through further study.

12.5.2.6.2 Notwithstanding the Natural Hazards policies of this Plan, the following policies will apply to those lands within the regulatory floodplain:

- a. ~~the lands within the floodway are designated Residential High Density, Office, Greenlands and Utility. Notwithstanding the Residential High Density, Office, Greenlands and Utility policies of this Plan, the following uses will be permitted within the floodway subject to the satisfaction of the City and Credit Valley Conservation:~~

- ~~flood and/or erosion works;~~
 - ~~facilities which by their nature must locate near water or traverse *watercourse* (i.e. bridges, storm sewer outlets and stormwater management facilities); and~~
 - ~~passive recreation activities; and~~
- b. floodway lands will be zoned in an appropriate hazard category in the implementing Zoning By-law;
 - c. the lands within the flood fringe are subject to their respective land use designations and the following additional policies:
 - development may be permitted provided the use, building or structure is flood proofed to the regulatory flood level as required by Credit Valley Conservation;
 - ingress/egress for all development located in the flood fringe will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access shall be made by Credit Valley Conservation based on the depth and velocity factors;
 - enclosed underground parking will be subject to the installation of stringent flood proofing measures to the elevation of the regulatory flood level; and
 - the zoning of lands may utilize a holding **zone provision** to **ensure** ~~provide direction as to future permitted uses while ensuring flood proofing and safe access are addressed prior to development to the satisfaction of the City and Credit Valley Conservation~~ The Zoning By-law will be amended to remove the holding symbol when the requirements for flood proofing, the provision of safe access to the proposed development and a detailed spill assessment and a financing agreement for the reconstruction of the culvert at the QEW has been completed to the satisfaction of the City, Credit Valley Conservation and the Ministry of Transportation;

d. the following uses will not be allowed within the floodplain:

- institutional services such as hospitals, nursing homes, and schools where there would be a significant threat to the safety of inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;
- new uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and
- emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures.

12.5.7.4 Roads will be designed as complete streets. New roads will be designed to incorporate active transportation and transit.

Existing arterial and collector roads dissecting and surrounding the Character Area will be redesigned to incorporate active transportation and transit infrastructure, as appropriate.

12.5.3 12.5.7 Transportation

~~12.5.3.1 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.~~

12.5.7.1 Development will be required to provide pedestrian connections to connect to the Hurontario Light Rail Transit (LRT), transit routes/stops, trails, and parks and open spaces.

12.5.7.2 Public easements will be required where pedestrian connections are proposed on private lands.

12.5.7.3 Vehicular access from Hurontario Street will be restricted and consolidated to minimize driveways and disruption to pedestrians, cyclists and transit.

12.5.8 Land Use

12.5.8.1 Notwithstanding the policies of this Plan, the following additional policies will apply to new development along Hurontario Street, King Street (new Cook Street to Hurontario) and Queensway:

- a. at grade retail and service commercial uses will be required; and
- b. residential dwellings will not be permitted at grade.

12.5.8.2 Mixed Use

12.5.8.2.1 Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted, unless it can be demonstrated that the planned function of the non-residential component will be maintained or replaced as part of the redevelopment.

12.5.8.2.2 For the purposes of the policies in this section, maintaining the non-residential planned function of the Mixed Use designation means providing:

- a. a concentration of convenient, easily accessible office, retail and service commercial uses such as a grocery store that meet the needs of local residents and employees; and
- b. employment opportunities, such as health sector, office and institutional jobs.

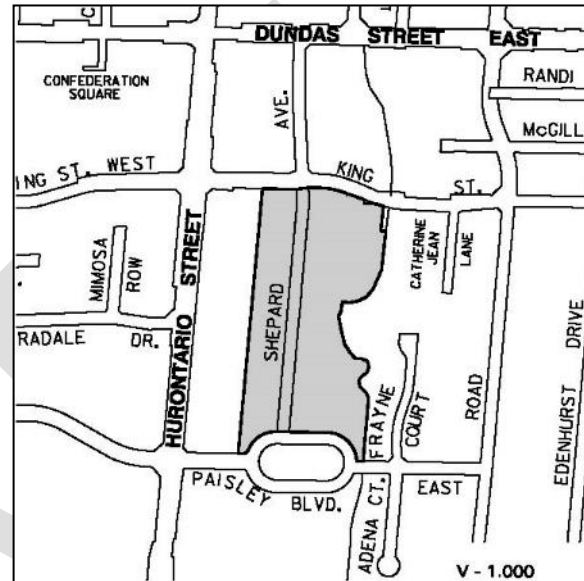
12.5.8.3 Office

12.5.8.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through development.

12.5.4 12.5.9 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

12.5.4.9.1 Site 1



12.5.4.9.1.1 The lands identified as Special Site 1 are located on Shepard Avenue between King Street East and Paisley Boulevard East.

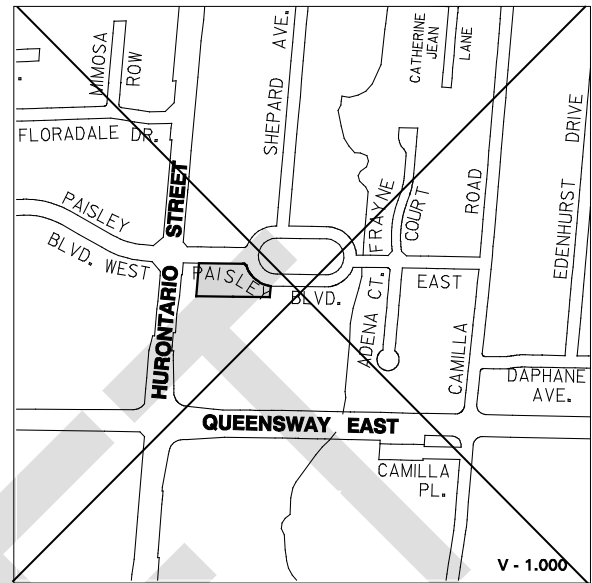
12.5.4.9.1.2 Notwithstanding the ~~provisions of the Residential Low Density I, Residential Medium Density and Greenlands designations~~, the policies of this Plan, the following additional policies will apply:

- a. any proposal to alter the natural alignment of the Cooksville Creek and associated Greenlands lands through filling or channelization will require the appropriate approvals from Credit Valley Conservation and the City of Mississauga. The final design of such alteration works will address the following:
 - criteria for erosion measures; and
 - incorporation of the recommendations of the Cooksville Creek Rehabilitation Study for the reach of the Cooksville Creek between King Street East and Paisley Boulevard East; and

- b. in the event that structural management options are not feasible and, subject to a detailed planning design and supporting engineering studies for the Special Site 1 lands, a *two-zone floodplain management concept*, or combination of a *two-zone floodplain management concept* and structural management options, may be implemented;
- c. the limits of the development will be determined through detailed studies to address limits of flooding. Lands not suitable for development will be redesignated to Greenlands; and
- d. the preparation of a concept plan promoting the integrated redevelopment of the existing residential lots will be required in conjunction with any redevelopment applications. The concept plan will be required to address the following:
 - naturalization of the Cooksville Creek and retention of existing vegetation;
 - an alternate street access and design to limit access to the lands from only King Street East; and
 - housing forms that provide for the stepping of densities and building heights from higher at the north and west ends of the site to lower at the south and east ends of the site.

The policies in this section and the land use designation on Schedule 10 – Land Use Designations with respect to Site 1 are under appeal under OMB Case No. PL980724 (OPA 3).

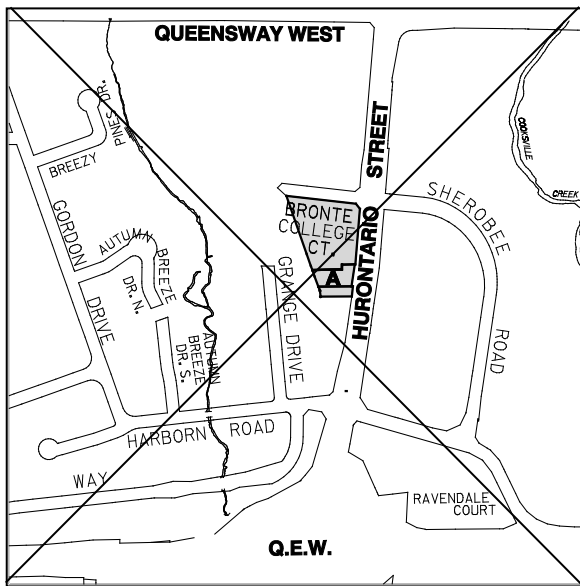
12.5.4.2 Site 2



~~12.5.4.2.1 The lands identified as Special Site 2 are located on the south side of Paisley Boulevard, east of Hurontario Street.~~

~~12.5.4.2.2 Notwithstanding the provisions of the Residential High Density designation, apartments will be permitted at a maximum *floor space index (FSI)* of 0.5 – 1.5.~~

12.5.4.3 Site 3



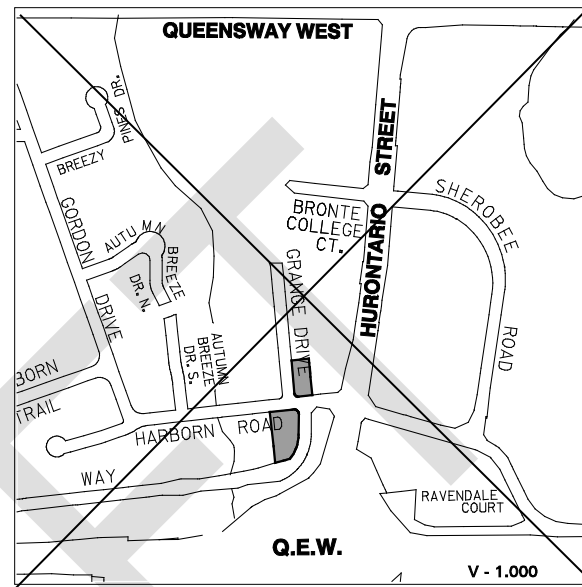
12.5.4.3.1 The lands identified as Special Site 3 are located along the west side of Hurontario Street, north of Harborn Road.

12.5.4.3.2 Notwithstanding the provisions of the Office designation, the following additional policies will apply:

- a. funeral homes and nursing homes will also be permitted;
- b. vehicular access and internal circulation and parking must respect both the visual and functional quality of the street as well as the privacy of the residential areas to the west;
- c. efforts must be made to retain the existing vegetation cover maintaining the continuity of the area character;
- d. building height and form should provide a positive scale transition between Hurontario Street and existing residential development to the west;
- e. the building setback from the rear lot line should provide for the retention of natural vegetation to function as a buffer from residential uses to the west; and

- f. the lands identified as Area A, located at 2150 Hurontario Street, will be permitted to develop at a maximum *floor space index (FSI)* of 1.0.

12.5.4.4 Site 4



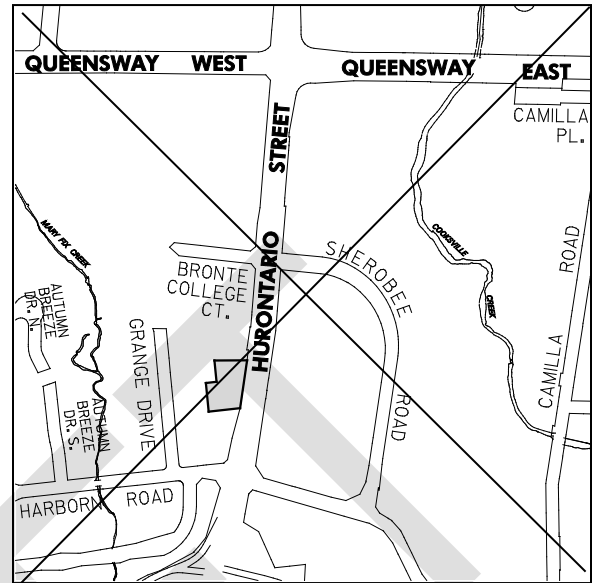
12.5.4.4.1 The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.

12.5.4.4.2 Notwithstanding the provisions of the Residential Low Density II designation on these lands, the following additional policies will apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding areas, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;
- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;

- f. ~~encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;~~
- g. ~~reduce the hard surface areas in the front yard;~~
- h. ~~preserve existing mature high quality trees to maintain the existing mature nature of these areas;~~
- i. ~~house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged;~~
- j. ~~the building mass, side yards and rear yards should respect and relate to those of adjacent lots; and~~
- k. ~~a maximum height of three storeys will be permitted for street townhouses.~~

12.5.4.5 Site 5



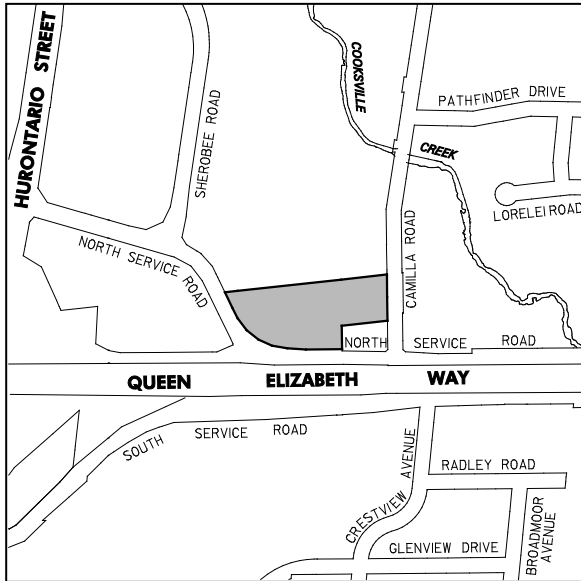
12.5.4.5.1 The lands identified as Special Site 5 are located on the west side of Hurontario Street, north of Harbourn Road.

12.5.4.5.2 Notwithstanding the policies of this Plan the following additional policies will apply:

- a. ~~ground level commercial uses will be permitted;~~
- b. ~~office uses will be permitted; and,~~
- e. ~~maximum of two towers, one with a maximum height of 26 storeys and the second with a maximum height of 29 storeys, will be permitted.~~

12.5.4.9.6

Site 6

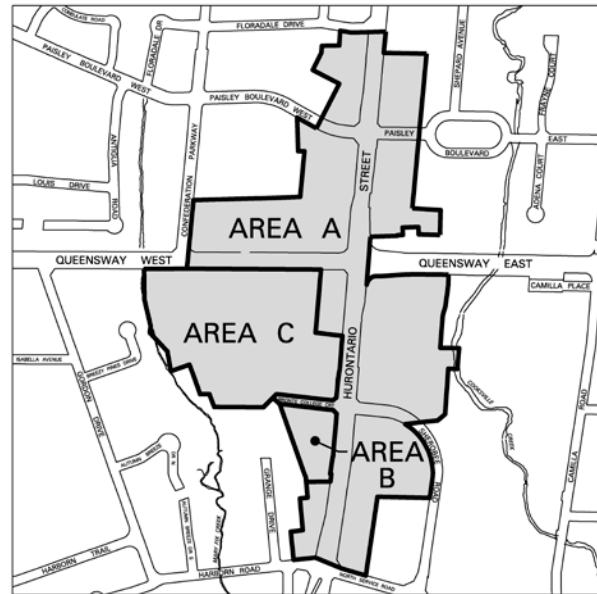


12.5.4.9.6.1 The lands identified as Special Site 6 are located on the north side of North Service Road and west side of Camilla Road.

12.5.4.9.6.2 Notwithstanding the policies of this Plan, townhouses will be permitted.

12.5.9.7

Site 7



12.5.9.7.1 The lands identified as Special Site 7 are generally located on the east and west sides of Hurontario Street between Floradale Drive and Harborn Road/North Service Road and identified as a health district.

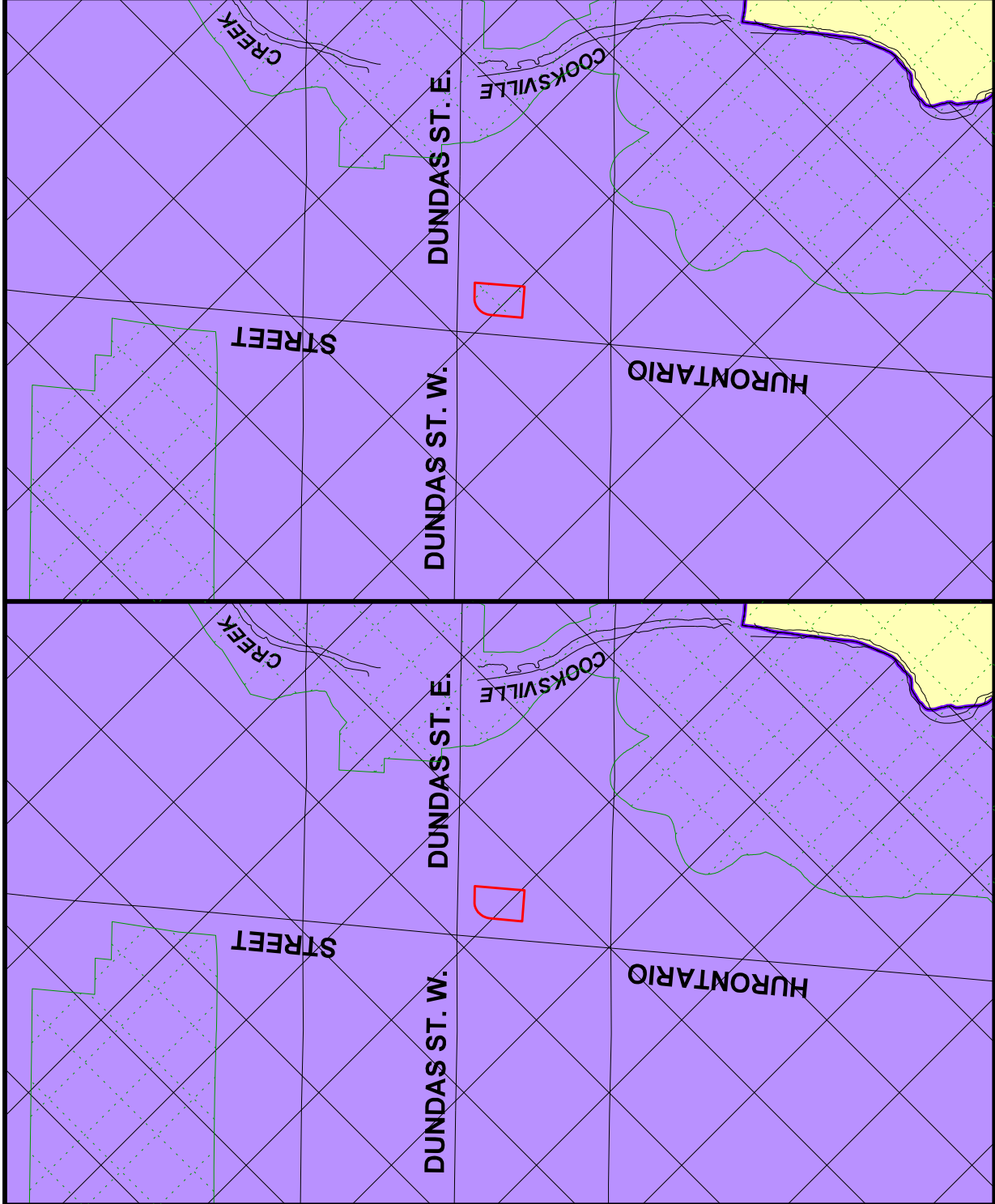
12.5.9.7.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- on lands identified as Area A and Area B, a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are adjacent to Hurontario Street and Queensway West; and
- lands identified as Area B will also permit funeral homes and nursing homes;
- lands identified as Area C will also permit the following uses:
 - conference centre;
 - financial institution;
 - long-term care facility to a maximum building height of 25 storeys;
 - major and *secondary office*;
 - overnight accommodation;

- personal service establishment;
- research and development;
- restaurant;
- retail store; and
- special needs housing to a maximum building height of 25 storeys.

DRAFT

Note: A blue box has been included in this document for information purposes only and are intended to be included in a broader amendment related to Major Transit Station Areas to conform to the Region's Official Plan Amendment.



LEGEND

- GREEN SYSTEM
 - Green System
- CITY STRUCTURE
 - Downtown
 - Major Node
 - Community Node
 - Neighbourhood
 - Corporate Centre
 - Employment Area
 - Special Purpose Area
- CORRIDORS
 - Corridor
 - Intensification Corridor
- AREA OF AMENDMENT:
 - Addition to the urban system

APPEALS

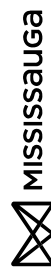
The information on the schedule reflects Council adopted amendments.
The following amendments are under appeal and affect this schedule:
No appeals at time of consultation.
For project mapping information refer to the Consolidation Tables and MOA document.

Note:
Base map information (e.g. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.

DRAFT

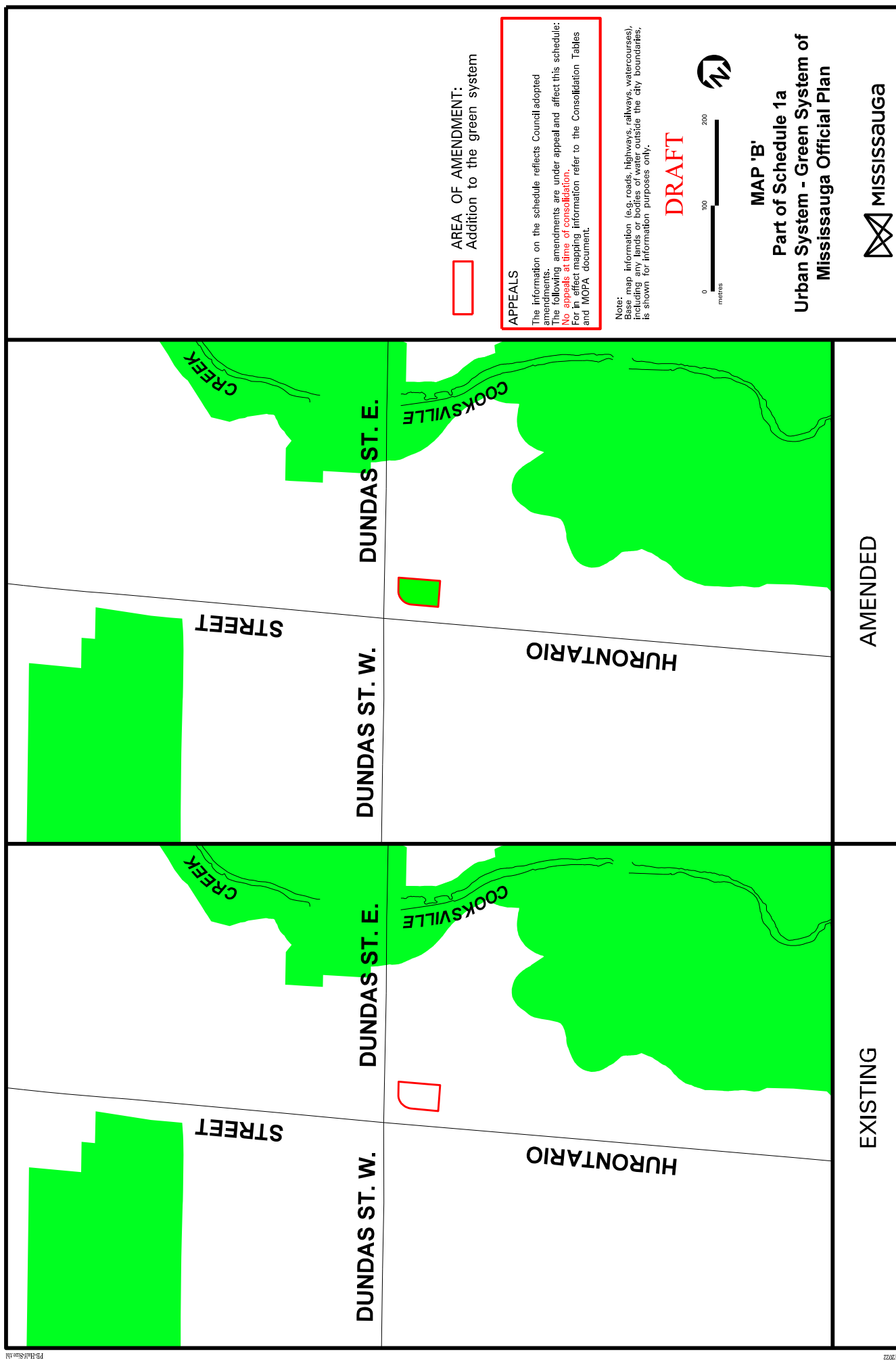


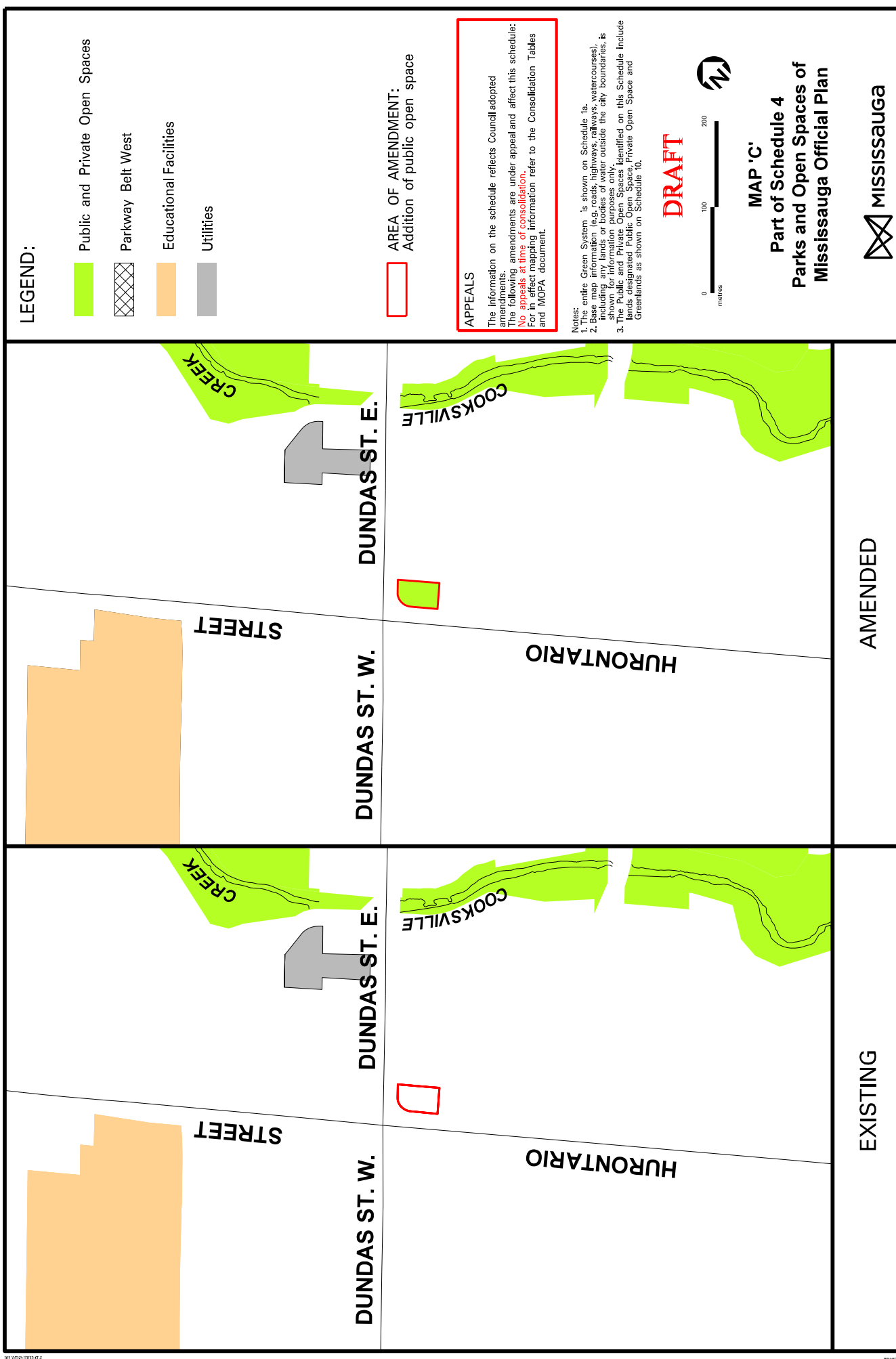
MAP 'A'
Part of Schedule 1
Urban System of
Mississauga Official Plan

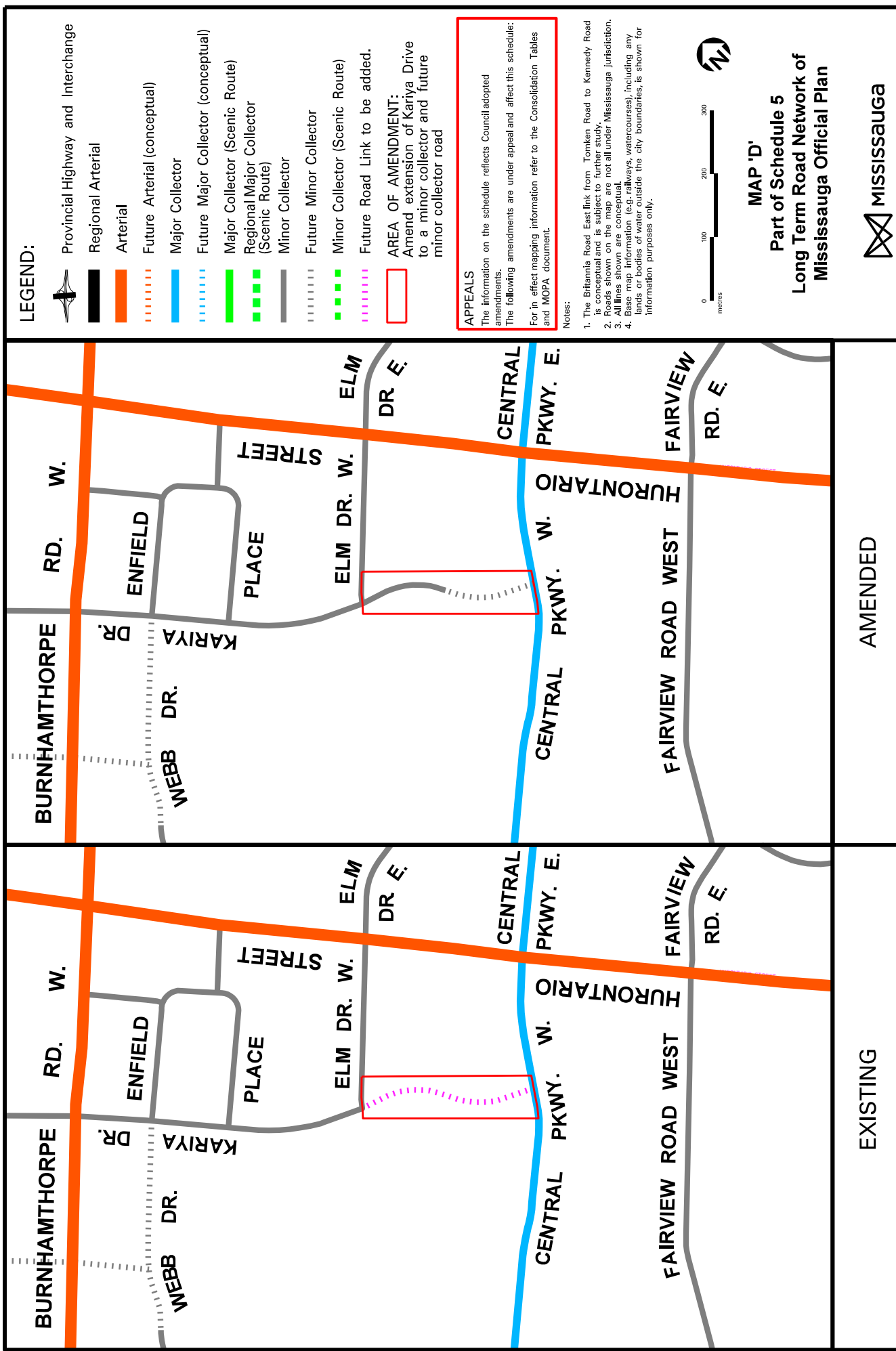


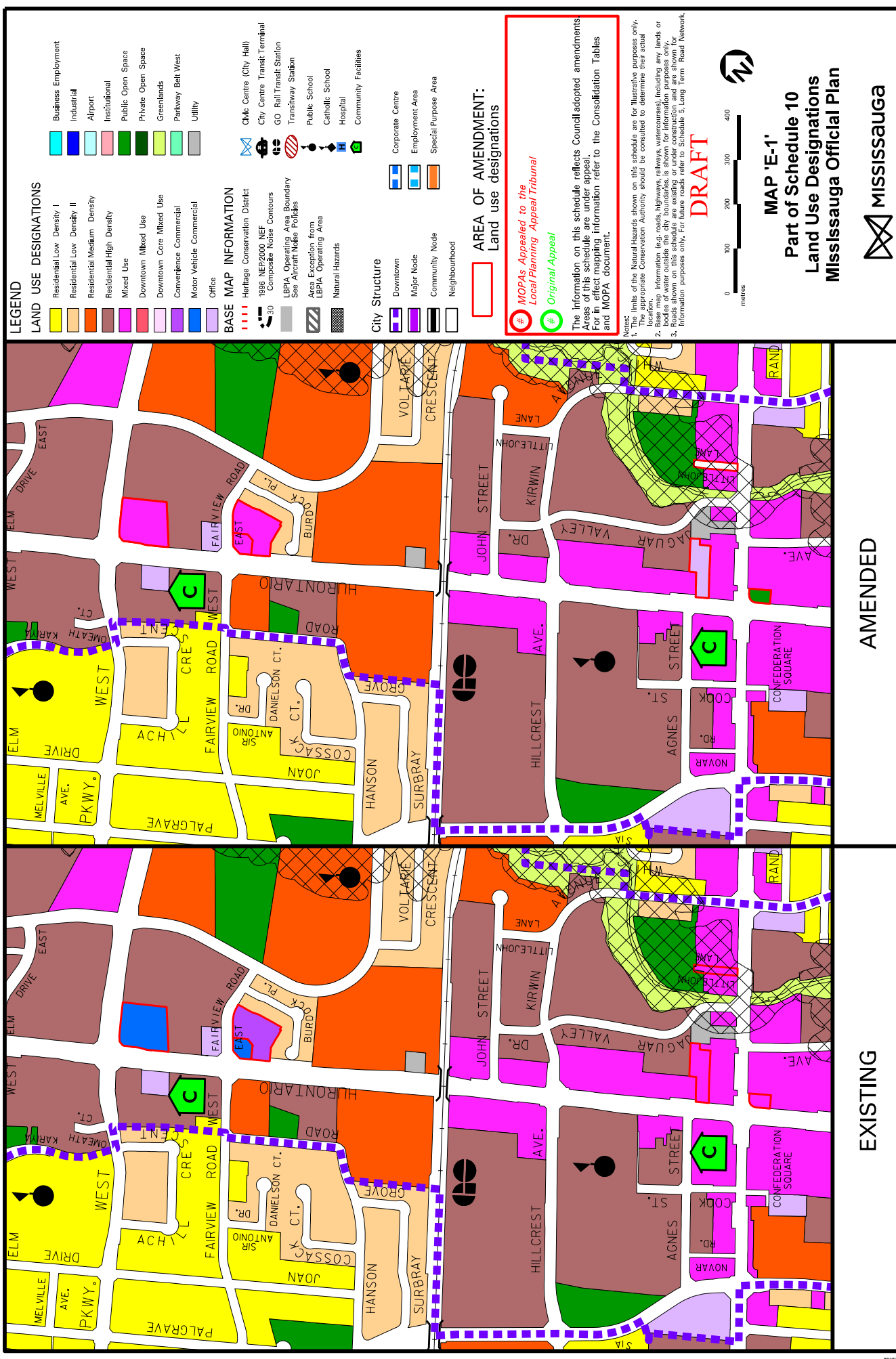
AMENDED

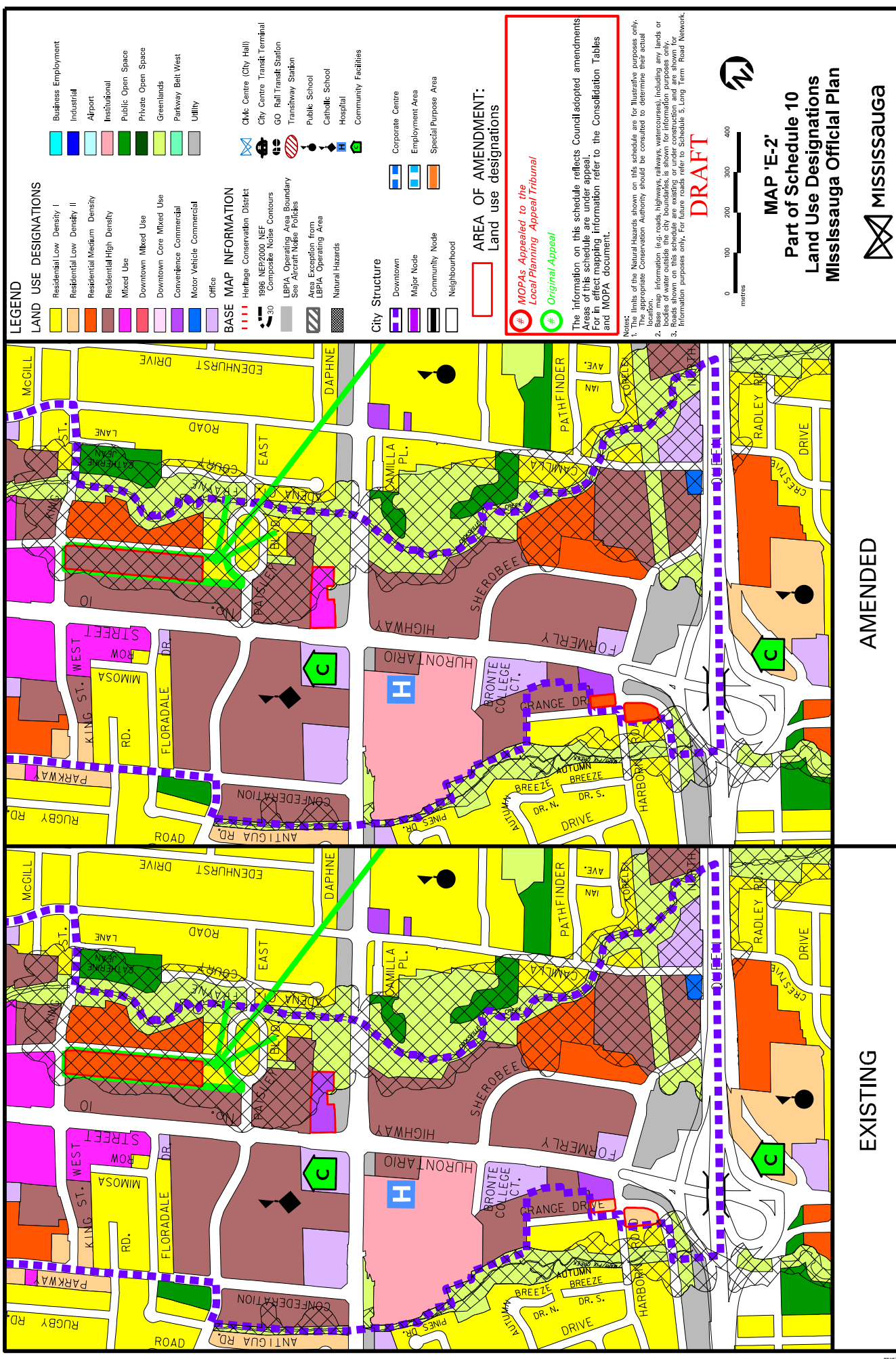
EXISTING











Downtown Fairview, Cooksville and Hospital Built Form Standards

DRAFT

City of Mississauga

Planning and Building Department
Development and Design Division
Urban Design Section

January, 2022

**All photos and illustrations provided by the Urban Design Division
of the City of Mississauga, except where noted.**

Contents

1.0 INTRODUCTION	04
1.1 Purpose of the Standards	05
1.2 Objectives of the Standards	05
1.3 Expectations of the Standards	06
1.4 How to Read the Standards	07
2.0 GENERAL STANDARDS	8
2.1 General Built Form Standards	9
2.1.1 Ground Floor Setbacks	
2.1.2 Podium Stepbacks	
2.1.3 Floor Plate Sizes	
2.1.4 Tower Separation	
2.1.5 Building Articulation	
2.1.6 Building Top	
2.1.7 Height Variation	11
2.2 Architectural Design Guidelines	
2.2.1 Main Street Character	
2.2.2 Choice of Materials	
2.2.3 Building Corners and Sides	
2.2.4 Parking Structures	
2.2.5 Service, Parking and Loading	
3.0 CHARACTER AREAS & STREET TYPES	15
3.1 Character Areas	16
3.1.1 Downtown Fairview	
3.1.2 Downtown Cooksville	
3.1.3 Downtown Hospital	
3.2 A-Streets	22
3.2.1 General Standards for A-Streets	
3.3 B-Streets	29
3.3.1 General Standards for B-Streets	
3.4 C-Streets and Pedestrian Connections	32
3.4.1 General Standards for C-Streets	
3.4.2 General Standards for Pedestrian Connections	
3.5 Transition From Established Neighbourhoods	35
4.0 GLOSSARY	37

Introduction

- 1.1** Purpose of the Standards
- 1.2** Objectives of the Standards
- 1.3** Expectations of the Standards
- 1.4** How to Read the Standards

1.1 Purpose of the Standards

The purpose of the Downtown Fairview, Cooksville and Hospital Built Form Standards (Standards) is to provide direction and guidance for proposed development at the planning application stage in order to assess, promote and fulfill the intent of the City's official plan policies, and zoning by-law - all of which shape and influence the urban structure, built form qualities of the character areas.

Selected content from this document, has been incorporated into the Downtown Fairview, Cooksville and Hospital Character Areas in the Mississauga Official Plan (MOP).

Applicants should also refer to Mississauga Zoning By-law, and Ontario Building Code to ensure that the applicable policies and requirements in these documents have been met. Furthermore, other City initiatives and special projects should be consulted to determine applicability, such as the city's strategic plan, green development strategy, shadow and wind study requirements.

soul of the City;

- Create a framework that promotes downtown development in a coordinated, comprehensive fashion through urban design standards;
- Facilitate the fair and consistent application of design objectives;
- Integrate a mix of uses, through appropriate built form, including commercial uses, offices, residential, cultural, entertainment and institutional uses — the whole of which is intended to put people in close proximity to a broad range of urban amenities and experiences;
- Achieve a high quality built form and strengthen the continuity of buildings that contribute to the emerging urban context of the three character areas;
- Ensure that development is resilient, environmentally friendly, safe and universally accessible; and
- Foster compact, pedestrian and transit-oriented development that achieves vibrant street level activity and a public realm of the highest standard.

1.2 Objectives of the Standards

Chapter 9 of MOP, 'Build a Desirable Urban Form' along with the policies contained in Chapter 12 'Downtown' set the urban design and built form policies to support the vision for a vibrant Downtown.

These built form standards act to articulate the policies in addition to the following objectives:

- Support the City of Mississauga's Strategic Plan and its Strategic Pillars for Change. A paramount goal is to create a vibrant downtown that functions as a strong economic centre, while acting as the civic heart and

1.3 Expectations of the Standards

The Standards provide further direction on the urban design policies set out in MOP and in addition to other city initiatives that support land use decisions and strategies for Downtown Fairview, Cooksville and Hospital.

The Standards set out detailed requirements to achieve a high quality built form that interfaces with the public realm in a seamless fashion. The Standards have been developed to communicate the design expectations, in advance of an application being filed, related to the quality and outcome of development.

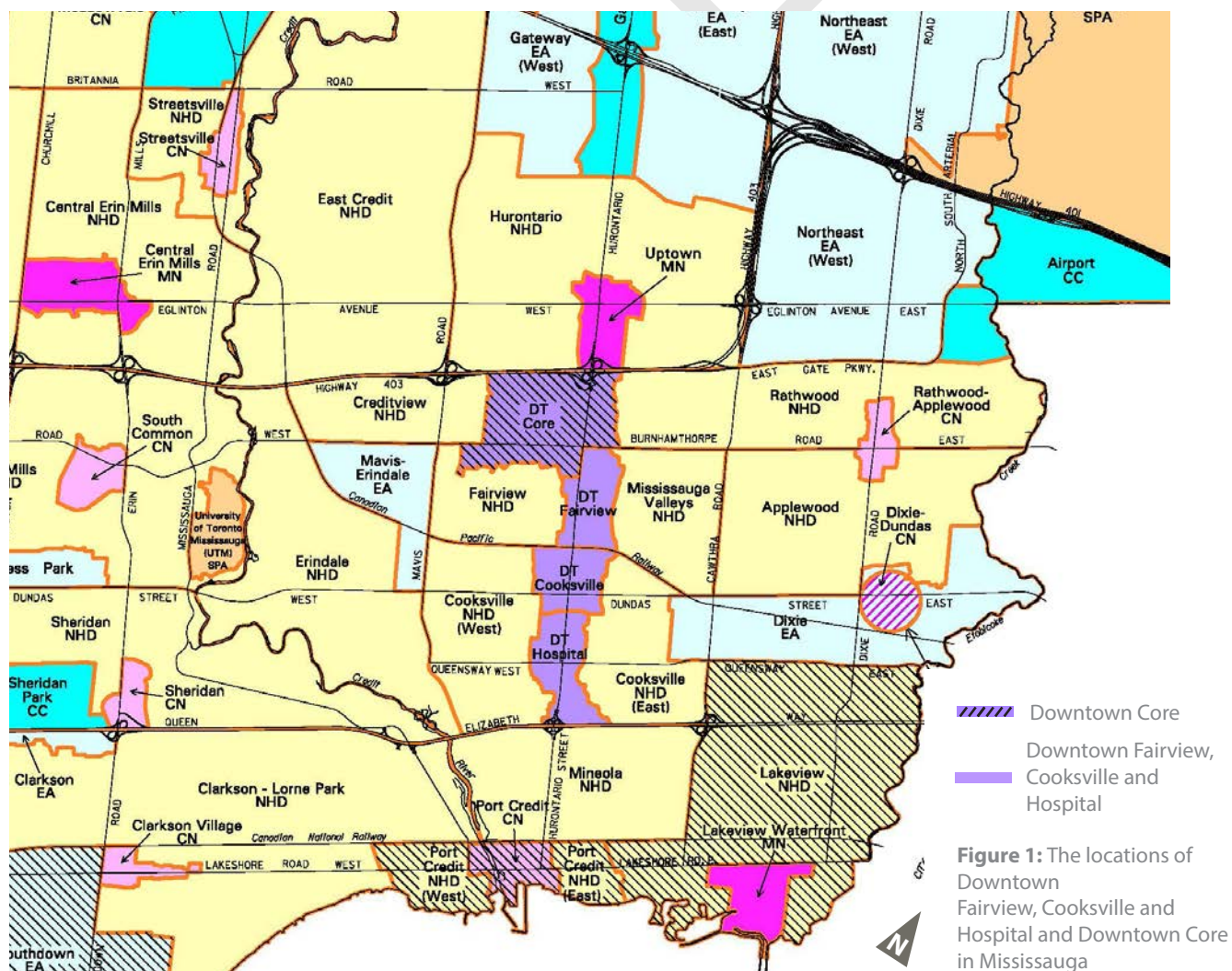
It should be noted that the Standards may be amended, modified or updated on an as-needed basis to provide clarity on the intents of the City of Mississauga's Official Plan, provisions of the zoning by-law including the outcome of other studies or initiatives that affect Downtown Fairview, Cooksville and Hospital.

1.4 How to Read The Standards

The rationale of the Standards is best understood by reviewing all sections, text and diagrams, including the policies cited within the Mississauga Official Plan. The Standards reflect an integrated approach to Downtown Fairview, Cooksville and Hospital, through building frontage standards that guide and direct the general disposition of buildings through form-based design.

Moreover, the Standards are also contingent on

an understanding that downtowns are complex urban places that require an overarching organizing structure with a view of creating a coherent, legible, high quality public realm and memorable sense of place.



General Standards

2.1 General Built Form Standards

- 2.1.1** Ground Floor Setbacks
- 2.1.2** Podium Stepbacks
- 2.1.3** Floor Plate Sizes
- 2.1.4** Tower Separation
- 2.1.5** Building Articulation
- 2.1.6** Building Top
- 2.1.7** Height Variation

2.2 Architectural Design Guidelines

- 2.2.1** Main Street Character
- 2.2.2** Choice of Materials
- 2.2.3** Building Corners and Sides
- 2.2.4** Parking Structures
- 2.2.5** Service, Parking and Loading

2.1 General Built Form Standards

2.1.1 Ground Floor Setbacks

Ground floor setbacks vary depending on the required uses on the ground floor, the character and configuration of the adjacent development and the required use on the ground floor. The setback also should provide enough space for appropriate landscape and streetscape treatment.

On streets where retail and service commercial is required, the ground floor setback from the property line should be between 2.0 metre and 4.0 metre from the property line to maintain a frontage that is close to the street.

On residential streets, the setback to a building shall be a minimum of 4.5 metre and a maximum of 6.0 metre to provide an adequate buffer between the private and public realms.

2.1.2 Podium Stepbacks

Podium heights will be a minimum of 3 storeys and a maximum of 6 storeys (20 meters) with a minimum 3 - 6 meter stepback between the podium face and tower, depending on the site context.

Podium heights for buildings located in Downtown Cooksville - Special Site 1 (MOP) will be a minimum of 3 storeys and a maximum of 4 storeys (14 meters) with a minimum 6 meter stepback between the podium face and tower.

Further details on Podium and Ground Floor Setbacks are provided in Sections 3 of these Standards.

2.1.3 Floor Plate Sizes

For buildings up to 12 storeys tall the maximum allowable tower floorplate above the podium is 1000 square meters.

For buildings greater than 12 storeys, the maximum tower floorplate is 750 square metres

2.1.4 Tower Separation

Towers must be separated at least 30 metres, measured from the tower face (excluding projections such as balconies).

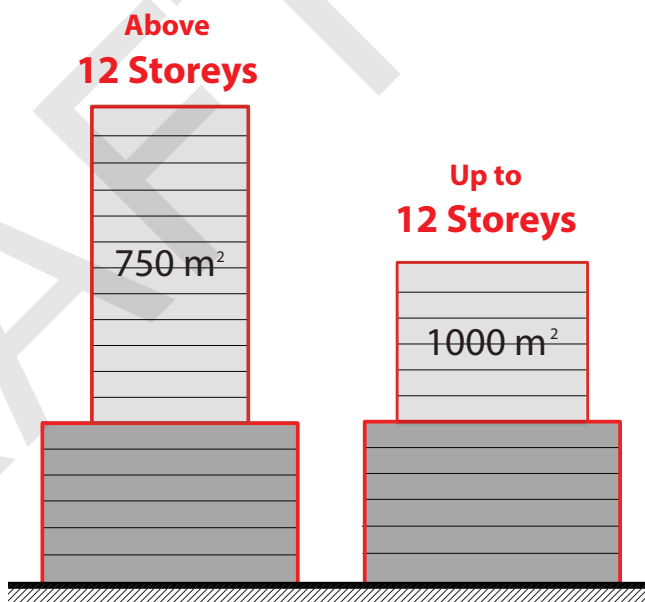


Figure 2: Maximum allowable floorplates above the podium are either 750 square meters or 1000 square meters, depending on building's overall height.

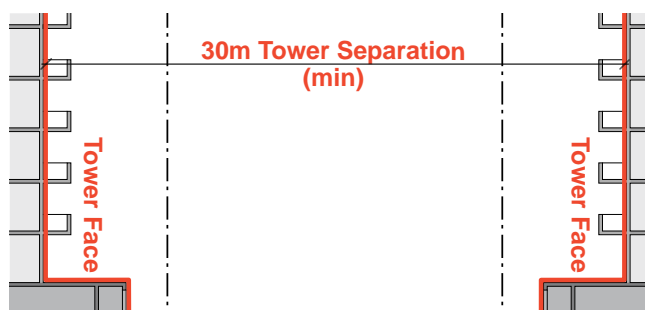


Figure 3: Balconies will not project from the outline of the building envelope. Tower separations and setbacks are measured from the outermost edge of the tower or podium face.

To maintain the 30 m separation distance, towers must have a minimum setback of 15 m from the rear and side yards.

2.1.5 Building Articulation

The podium levels must be designed to appear to be broken down into multiple parts by inseting small parts of the floor plan, articulating balconies differently, and varying the massing and materials.

2.1.6 Building Top

The top 2 to 3 levels of the building should be stepped back 3 meters to create visual relief at the top of the building.

The rooftop mechanical penthouse will be designed to stepped back and compliment the architecture of the building.

2.1.7 Height Variation

In a development with multiple towers, the height of no two towers can be the same. The height of the towers must be varied by 3 to 5 storeys.

2.2 Architectural Design Guidelines

2.2.1 Main Street Character

2.2.1.1 Street Wall Continuity

A street-wall is a single entity that is composed of the different exterior walls of buildings that face the same public right-of-way. A defined streetwall occurs when different building facades are coordinated to maintain visual continuity and a consistency in massing.

To ensure street wall continuity:

- Locate setbacks on development blocks to inform the orientation and placement of buildings and streetwalls;
- Coordinate setbacks with adjacent properties in order to create consistent edges and street walls along frontages;
- Locate buildings parallel to the street, with streetwalls placed at the setback, to contain the street and provide enclosure;
- Ensure a variation in setbacks along the building frontages to articulate façades and allow for visual interest, accommodate outdoor patios, recessed entries and landscaped areas; and
- Avoid blank walls on all street frontages unless designed as an articulated, finished architectural element.

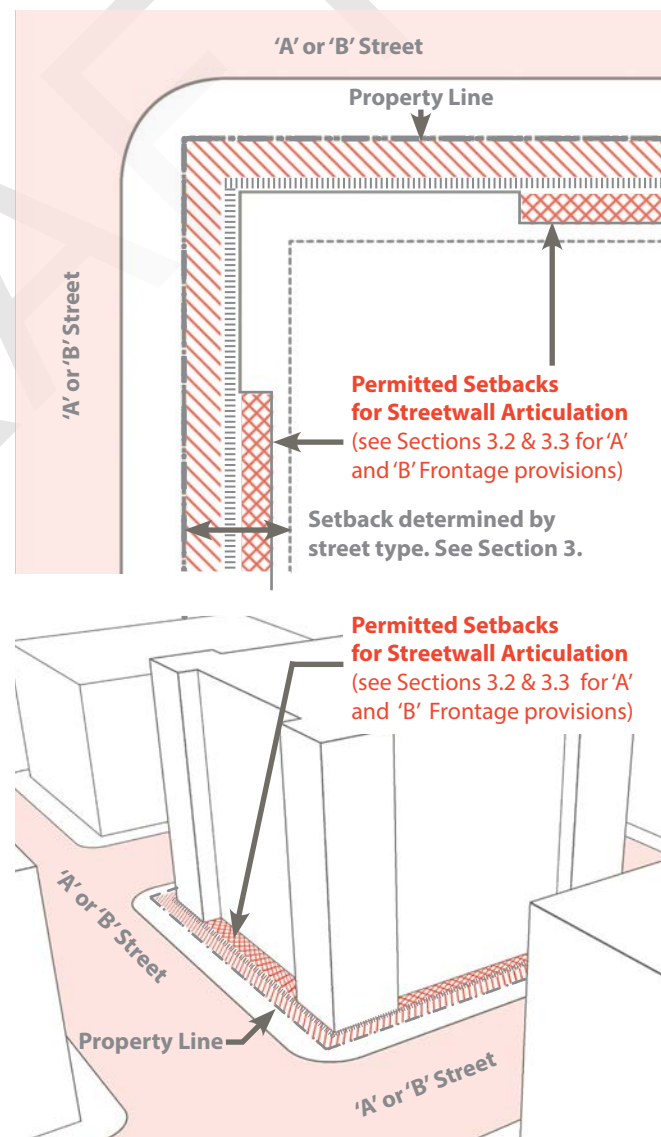
2.2.1.2 Façade Composition

When composing the building's podium ensure that the façade addresses the human scale. Building facades should be parallel with the street and provide periodic indentations for visual relief. Include features such as forecourts, plazas with streetscape amenities (such as trees, seating and public art) to improve the overall pedestrian experience.

Consider distinguishing three portions by having a base, a middle portion and a top portion or cap by:

- Reinforcing ground floor uses and access by integrating retail, entrances and lobbies

Figure 4: Development is located parallel to the street and placed at the required setbacks. The streetwall can step-back to accommodate façade articulation, patios and entrances.



together and establishing a clear base;

- Defining a middle portion of the street-wall which has material treatment that is separate from the base and the top portion of the street-wall; and
- Terminating the street-wall with a roof-line, a parapet, a juncture or a storey that further expresses the street-wall and the character of the building.

2.2.1.3 Storefronts and Bays

Storefronts must be designed with a traditional main street character that have regular sized bays with a combination of glazed storefronts between solid piers. Large expanses of glazed areas should be avoided.

- Incorporate a balance in the proportion of glazing to solid materials as well as openings to enclosure in the design of facades in order to achieve pleasing symmetries and legible asymmetries;
- A signage band should be included above the glass storefronts as a controlled location for signage;

- Locate main entrances flush with the public sidewalk;
- Ensure site designs relate to and interface with existing, proposed and future transit stops and facilities; and
- Locate main building entrances so that they are clearly identifiable and prominent with direct access to the public sidewalk, pedestrian connections and transit facilities.

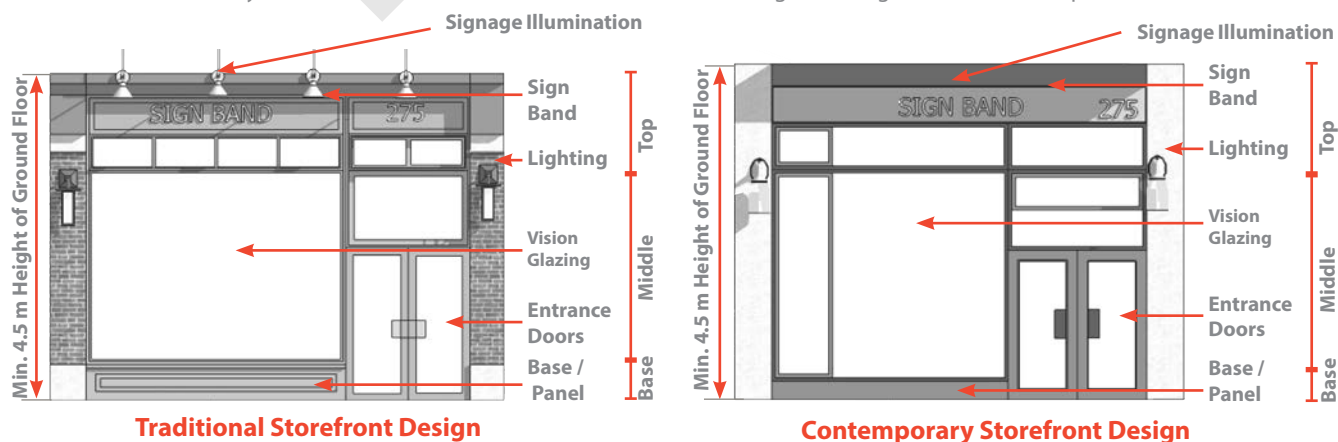
2.2.1.4 Awnings and Canopies

Continuous awning and canopies should be located above the storefronts approximately 3 - 5 meters above grade and provide wind protection over the storefronts and entrances.

2.2.1.4 Balcony Projections

Balconies should not project over the ground floor storefronts. Balconies must be setback from the face of the storefront. See sections 3.1 and 3.2 for more details.

Figure 5: Storefronts can incorporate a variety of styles and character to animate the street edge. When well executed, they contribute to the identity of the street and make retail and commercial frontages inviting and accessible to pedestrians



2.2.2 Choice of Materials

Construct building exteriors from durable, natural materials such as clay brick, stone, metal, glass and wood. Energy efficient, salvaged, recycled or reused building materials are also highly encouraged.

Building materials such as stucco, metal siding, embossed face brick panels and vinyl are generally discouraged as they do not age well, are not durable and are prone to weathering.

Deploy a hierarchy of materials with solid, or 'heavier' materials located closer to the ground as a means to visually anchor the building to grade.

2.2.3 Building Corners and Sides

Design building located at prominent corners with upgraded elevations and design features to create a focal point.

Articulate corner units with an ample setback to create space for patios and outdoor merchandising that will animate the corner.

Parti walls should be upgraded and not be exposed more than 5 storeys.

2.2.4 Parking Structures

Parking facilities will have an important role to play in supporting key uses, attractions and urban amenities. In general, development will locate structured parking and vehicular access to minimize impacts on the property and on surrounding properties and to improve the safety and attractiveness of adjacent streets, parks and open spaces. The following standards will apply:

2.2.4.1 Above-grade parking structures will not directly front onto 'A' Streets*, but will be entirely screened by 'liner' buildings incorporating a mix of buildings between the parking structure and street space;

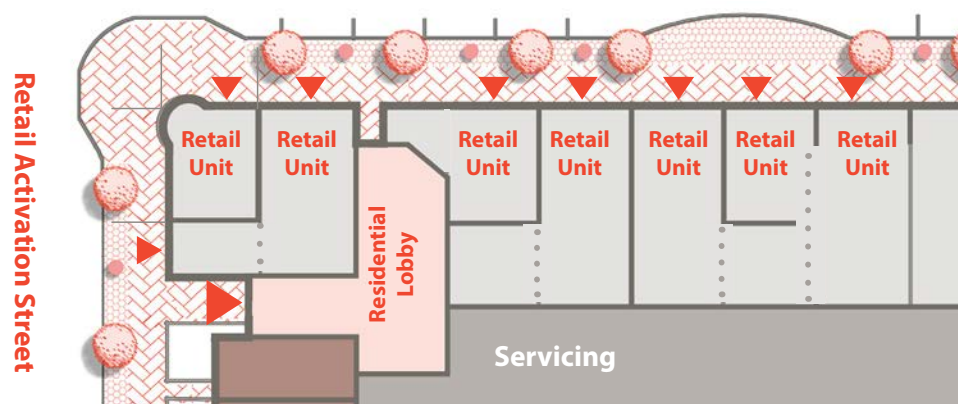
2.2.4.2 Liner uses should have a minimum depth of 10.0 m;

2.2.4.3 On the ground floor, parking structures will have active uses such as commercial or retail services with an appropriate scale and architectural expression to support activity on the streets, including those fronting onto parks and/or open spaces;

2.2.4.4 Parking structures should front C-Streets*. if not possible, parking structures will

**Please refer to Character Area Maps in Section 3 to find the location of 'A', 'B' and 'C' streets.*

Figure 6: Interior demising walls for retail units can be configured to promote an expression of narrower units and smaller retail spaces along building frontages, opening up beyond to combine units into larger retail spaces.



only directly front onto one 'B' Street* where more than one 'B' street frontage exists;

2.2.4.5 When fronting onto 'B' Streets, parking structure elevations will be designed to the highest level of architectural treatment and animation to screen views of the interior;

2.2.4.6 Entrances, lobbies and passageways that provide a convenient means of access to parking facilities will be fully enclosed, appropriately signed and integrated into the façade design without appearing as dominant elements on the street frontages;

2.2.4.7 At grade exhaust vents serving structured parking facilities should be:

- Screened from public view; and
- Integrated into the design of buildings, expressed as part of the architectural character.

2.2.5 Service, Parking and Loading

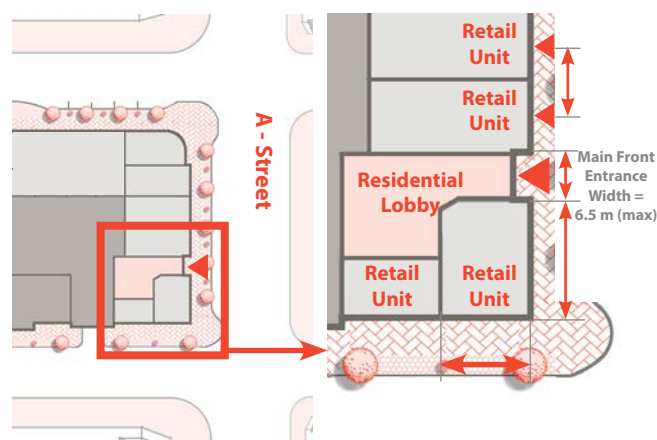
Service, parking and loading should be coordinated on sites by providing consolidated locations that can serve a number of buildings simultaneously from one area;

2.2.5.1 Loading, garbage and service spaces will:

- Be located internal to the building to avoid noise and visual impacts;
- Prioritize pedestrian safety and the appeal of the public realm by incorporating special architectural treatment and using soft and hard landscape treatments to screen loading and servicing areas; and
- Provide safe levels of illumination and lighting.

2.2.5.2 The height required for overhead loading for bulk refuse within a collection area should conform to the Region on Peel's standards.

Figure 7: Main front entrances serving residential uses above the ground floor will be located to minimize interruption of retail units along A-Streets.



Character Areas and Street Types

- 3.1** Character Areas
 - 3.1.1** Downtown Fairview
 - 3.1.2** Downtown Cooksville
 - 3.1.3** Downtown Hospital
- 3.2** A-Streets
 - 3.2.1** General Standards for A-Streets
 - 3.2.2** A-Street Subcategories
- 3.3** B-Streets
 - 3.3.1** General Standards for A-Streets
- 3.4** C-Streets and Pedestrian Connections
 - 3.4.1** General Standards for C-Streets
 - 3.4.2** General Standards for Pedestrian Connections
- 3.5** Transition From Established Neighbourhoods

3.1 Character Areas

3.1.1 Downtown Fairview

Introduction

Downtown Fairview contains a number of sub-areas, each with its own unique character and built form. Along Mississauga Valley Boulevard, the built form is characterized by 'tower in the park' style apartment buildings generally in the range of 20 to 24 storeys in height and surrounded by generous open spaces. West of Hurontario Street and north of Central Parkway, developments incorporate a podium and 'point tower' design with heights and densities that exceed other parts of Downtown Fairview. In the southeast and southwest areas of Downtown Fairview, townhouses are the predominant built form.

It is anticipated that new development, including residential, commercial and office uses, will be concentrated along Hurontario Street and Central Parkway East on existing retail and commercial sites and parking lots.

As the area redevelops, there will be a greater need for additional parkland, retail, commercial and institutional uses and pedestrian connections to create a more walkable community. Access to local and higher order transit will support compact and transit-oriented development. Anchored by a grocery store, the retail centre located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will continue to serve the retail and commercial needs of the local community.

Vision

Downtown Fairview will be a walkable, complete community with a range of housing options, retail and service commercial uses, community facilities and parks and open spaces. Along Hurontario Street, taller buildings will integrate

at-grade retail, commercial and office uses that serve the community. An expanded pedestrian network will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.

Guiding Principles

1. A transition in building height and density from the northwest area of Central Parkway and Hurontario Street to new development south along Hurontario Street and east towards Mississauga Valley Boulevard;
2. Heights and densities that are appropriate to the planned context, with the greatest heights located near rapid transit facilities;
3. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;
4. Accessible public spaces for all, with enhanced connections for walking and cycling;
5. Mixed-use, compact development oriented to transit and pedestrians; and
6. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.

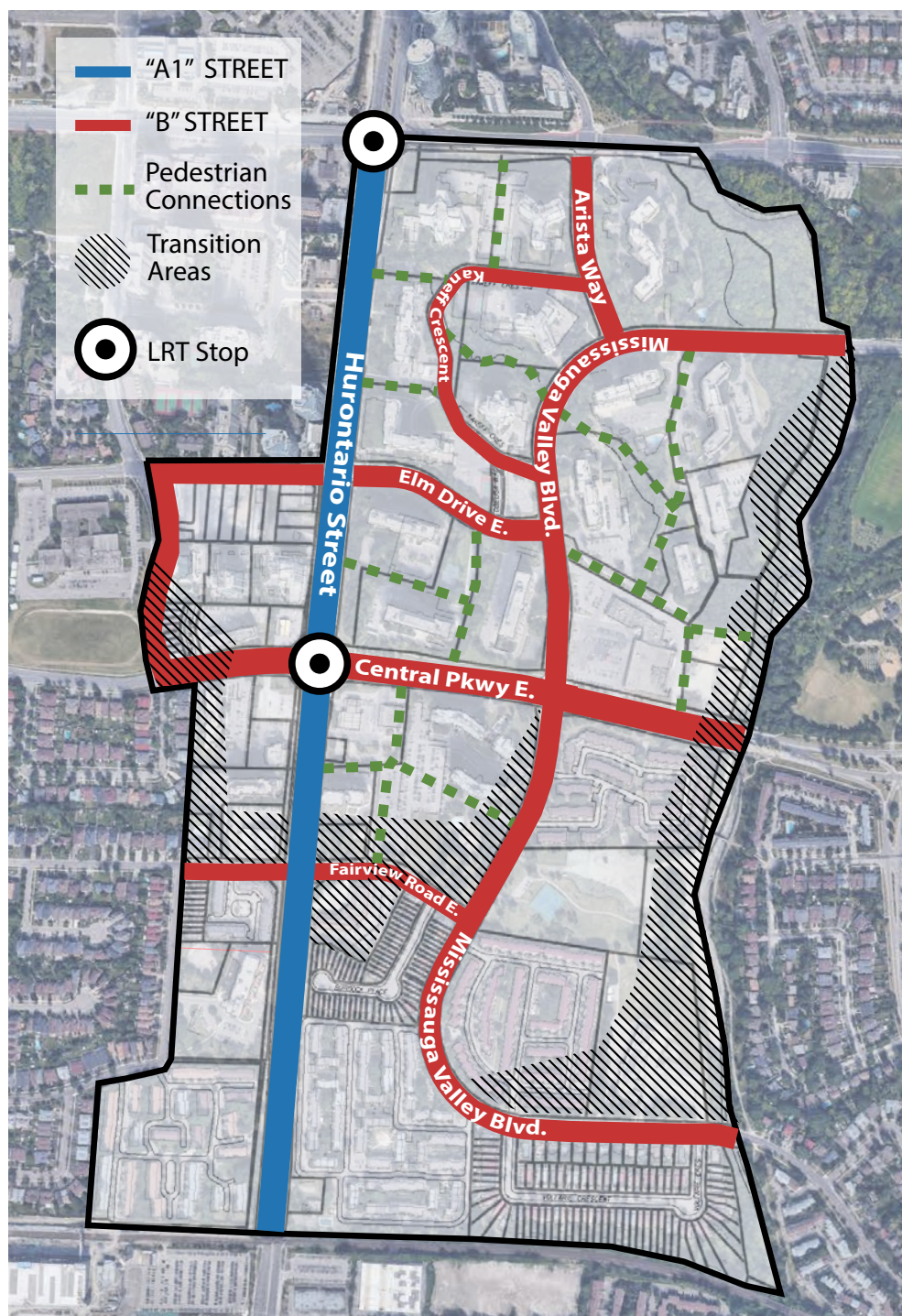


Figure 8: Street Types in the Downtown Fairview Character Area

3.1.2 Downtown Cooksville

Introduction

Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provides an important function in helping families transition to a new country.

Hurontario Street and Dundas Street is generally characterized by retail plazas that are one to two storeys with residential and/or office units above and surrounded with surface parking. There are a few office and residential buildings along the corridor that are generally of a low or mid-rise form.

With the potential for two-way, all-day GO service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and the Hurontario Light Rail Transit (LRT), significant transit investments will greatly improve the area and strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment and other destinations, while also attracting more visitors. More people, jobs and housing are also expected in Downtown Cooksville. A transit-oriented approach will support compact and street-related development in Cooksville.

Vision

Downtown Cooksville will be a walkable mixed use community with interesting places for culture and art. Hurontario and Dundas streets will be animated with storefronts and other

active uses that support local businesses. The area immediately surrounding the intersection of Hurontario and Dundas (Cooksville Corners) will be developed with a vibrant mainstreet focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit oriented development with a concentration and mix of residential, community, office, retail and service commercial uses.

Guiding Principles

1. Create a mixed use, vibrant community – mix of local and unique businesses along the Hurontario and Dundas streets, spaces for educational, cultural and recreational uses, a vibrant public realm, and a network of connected parks and open spaces;
2. Plan for more housing and people – provide new housing and jobs with the greatest heights and densities located near the Cookville GO Station; and
3. Achieve a walkable, connected community - pedestrian-oriented main street character along Hurontario and Dundas streets, improved pedestrian and cycling infrastructure, public art and beautification.
4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and higher-order transit service.

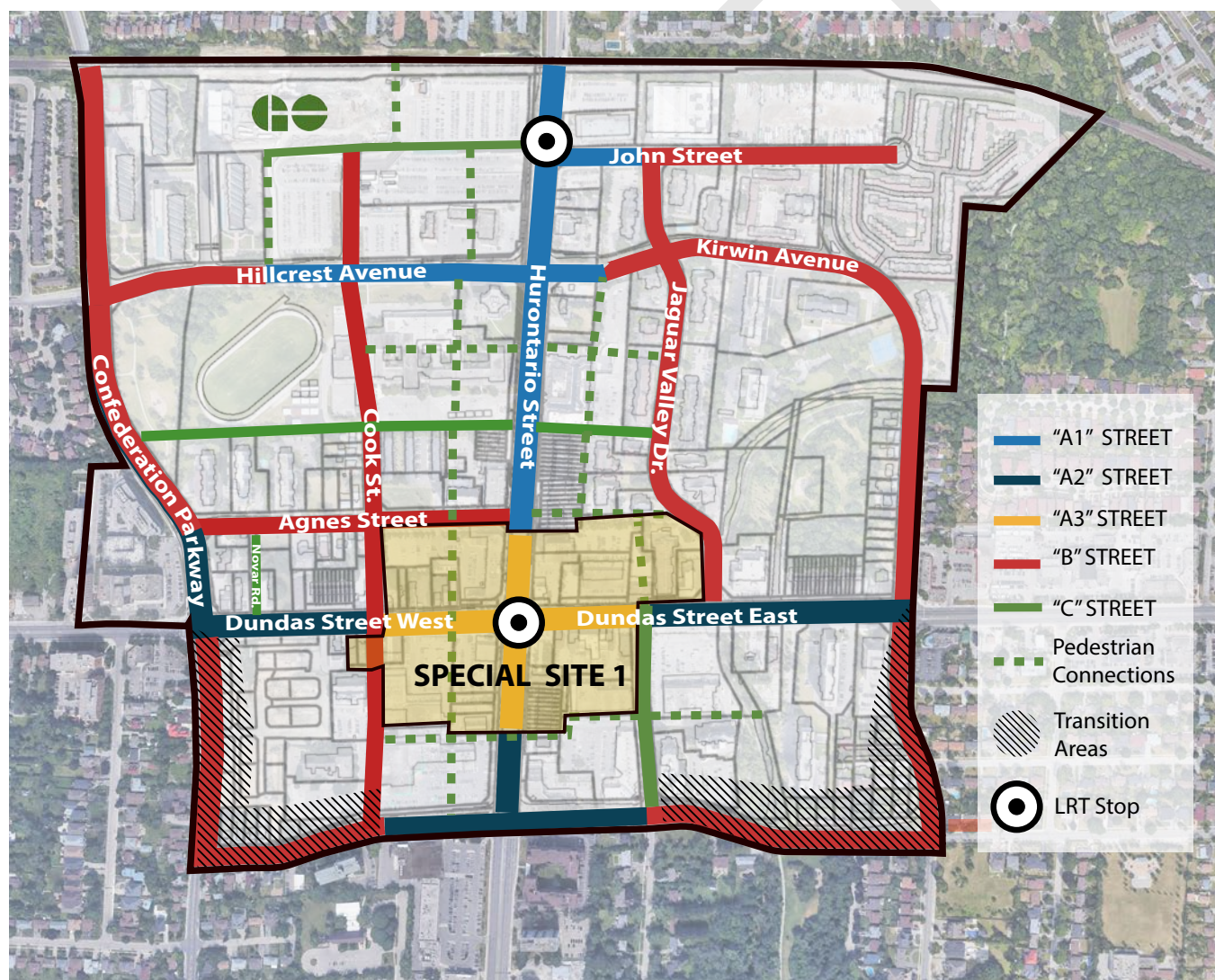


Figure 9: Street Types in the Downtown Cooksville Character Area

3.1.3 Downtown Hospital

Introduction

The focal point of Downtown Hospital continues to be the Trillium Mississauga Hospital, serving as one of Canada's leading health centres with specialized regional programs. Downtown Hospital consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial is clustered at three main intersections along Hurontario at King Street, Queensway and North Service Road.

connections to Hurontario Street and Queensway and incorporate street related retail and service commercial uses along these corridors.

Cooksville Creek is a significant natural feature and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. The open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, additional parkland and improvements to existing parks are planned.

Vision

Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Trillium Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

A transit-oriented approach will support compact and street-related development along Hurontario and Queensway.

Guiding Principles

1. Support health care services – promote a range of health care services, supportive uses and research and development; and
2. Create a more walkable and transit-oriented community – provide additional pedestrian

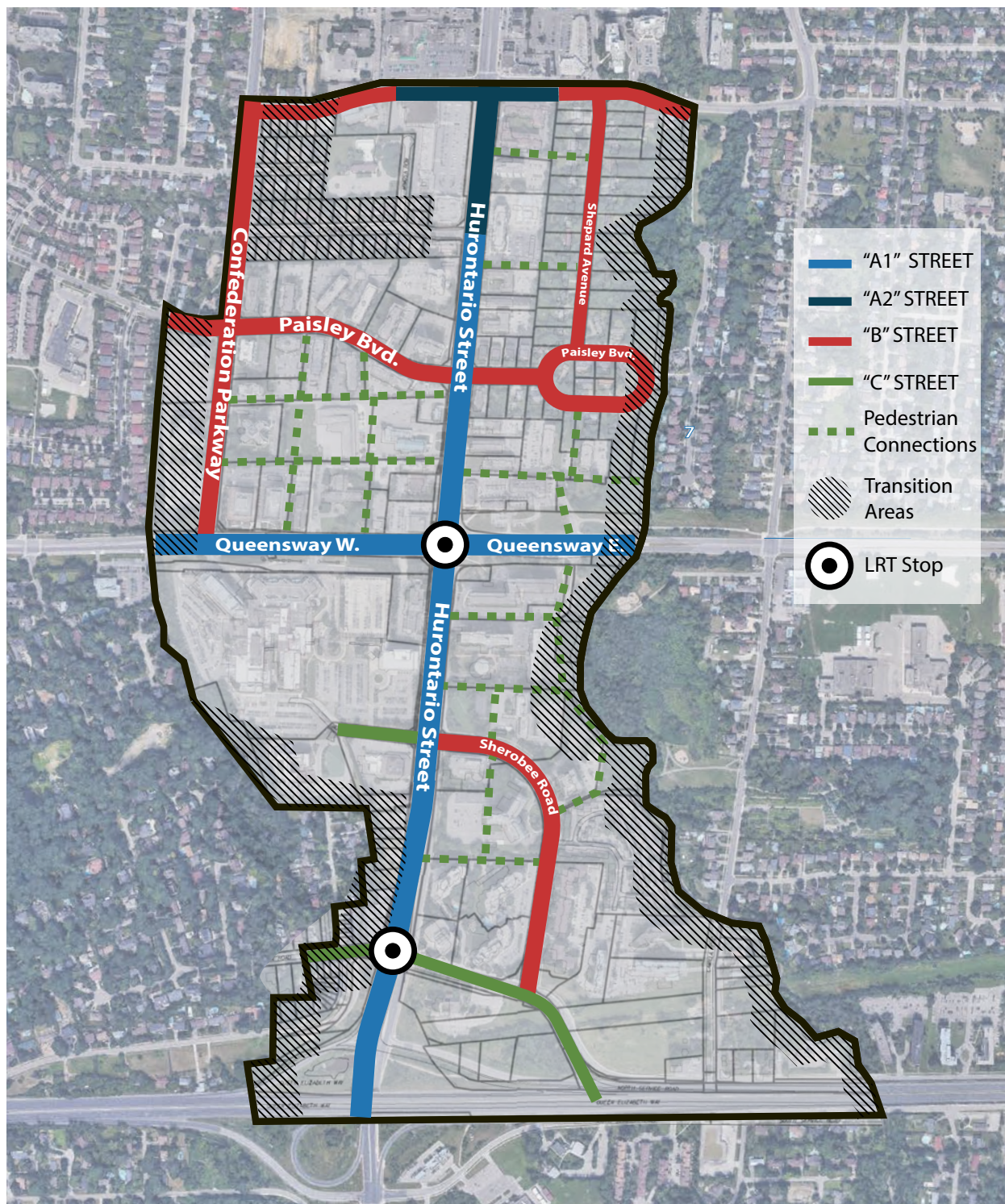


Figure 10: Street Types in the Downtown Hospital Character Area

3.2 A-Streets

A-streets are considered to be primary arteries and must incorporate at grade commercial and/or retail uses at grade. The design of A-Streets are critical to develop a lively urban environment that fosters active uses and a pleasant pedestrian environment.

A-Streets are distinct from secondary B-Streets, that are residential in character. B-Street provisions are discussed in Section 3.3

3.2.1 General Standards for A-Streets

The following standards will apply to all streets A-Streets in the three Downtown Character Areas:

3.2.1.1 Provide continuous buildings along 'A' Streets to maintain street continuity. A minimum of 90% of a property's frontage is required to be occupied by the streetwall that is located within 2.0 m. to 4.0 m from the property line. Up to 25% of the building frontage can be further setback to allow for outdoor patios, recessed entries and landscaped areas;

3.2.1.2 Ground floor heights will be a minimum of 4.5 metres (floor-to-floor, measured from established grade) to accommodate retail and commercial uses with windows that correspond to the height of ground floors;

3.2.1.3 Retail entrances should be located on primary frontages along A-Streets and provide a variability in scale of retail spaces to contribute to a healthy retail economy along A-Streets.

3.2.1.4 Where retail units occupy prominent corner locations, units should be designed to:

- address the corner with well designed storefronts that wrap the corner; and
- incorporate corner entrances.

3.2.1.5 Entrances to retail and commercial tenant spaces will be operational, flush and directly at the grade of the public sidewalk;

3.2.1.6 Storefronts are articulated externally to breakdown the apparent width with a frequent rhythm that supports the pedestrian experience at the street level;

3.2.1.7 Entrance doors to retail units built to the property line should be recessed in order to minimize conflicts between door swings and pedestrians;

3.2.1.8 Ensure that ground floors containing storefronts articulate:

- a strong sense of rhythm and pattern collectively across frontages;
- subtle recesses and reveals to create depth and visual interest; and
- assist pedestrians with spatial perception, orientation and accessibility along retail frontages.

3.2.1.9 At grade retail and commercial uses will incorporate the highest standard of storefront design consisting of:

- Durable, high quality materials such as metal, steel, glass, natural stone and brick;
- Vision glazing and doors that allow for views into and out of storefronts;
- Elements such as mullions, glazing bars and transoms to help frame, divide and define storefront window sections and apertures;and
- Dark tinted, reflective or opaque glazing placed on storefront windows are discouraged.

3.2.1.10 Consistent signage and lighting that integrates with the storefront design, and which complements or acts as an extension of the buildings architectural character;

3.2.1.11 The incorporation of architectural cantilevers, fixed canopies, awnings or similar features into a proposed development are encouraged to achieve pedestrian scale, comfort and weather sheltered pedestrian routes. These should complement the architectural character of the building and be made of highly durable materials;

3.2.1.12 Where appropriate, patios should be provided adjacent to retail/commercial frontages or entrances to promote activation of the street and vibrancy; and

3.2.1.13 Patios should have a depth of at least 3.5 m and can be recessed into the building as part of the permitted setback from the build-to line, or placed beyond the build-to line along the spill-out zone providing that there is sufficient space. A clear sidewalk width is required to ensure pedestrian traffic flow is not disrupted.

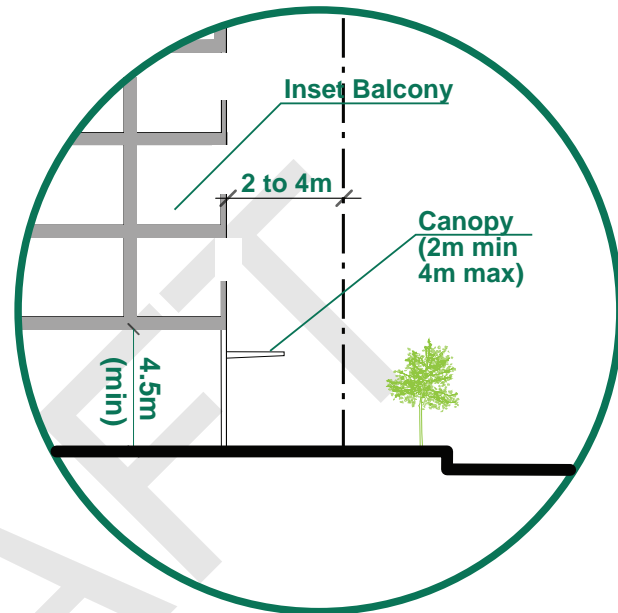


Figure 4: Ground floor conditions on all A - Streets. Balconies should be inset and any canopy projections should be between 2 and 4 meters.

Figure 11: Storefronts when well executed, promote a vibrant street life and the pedestrian experience.
Downtown Mississauga, Living Arts Dr & Square One Dr



Figure 12: Canopies help create a sense of enclosure and frame entrances.
Downtown Mississauga, Confederation Pkwy & Arbutus Way



3.2.2 A-Streets Subcategories

A-Streets are further divided into A1, A2 and A3 Streets with each sub-category having a variation in the built form, the required setback, podium height, step back and angular plane requirements.

These dimensions should always be revised to consider context. For example, where trees cannot be accommodated on boulevards, they will be required to be on private property, as determined during the application review process.

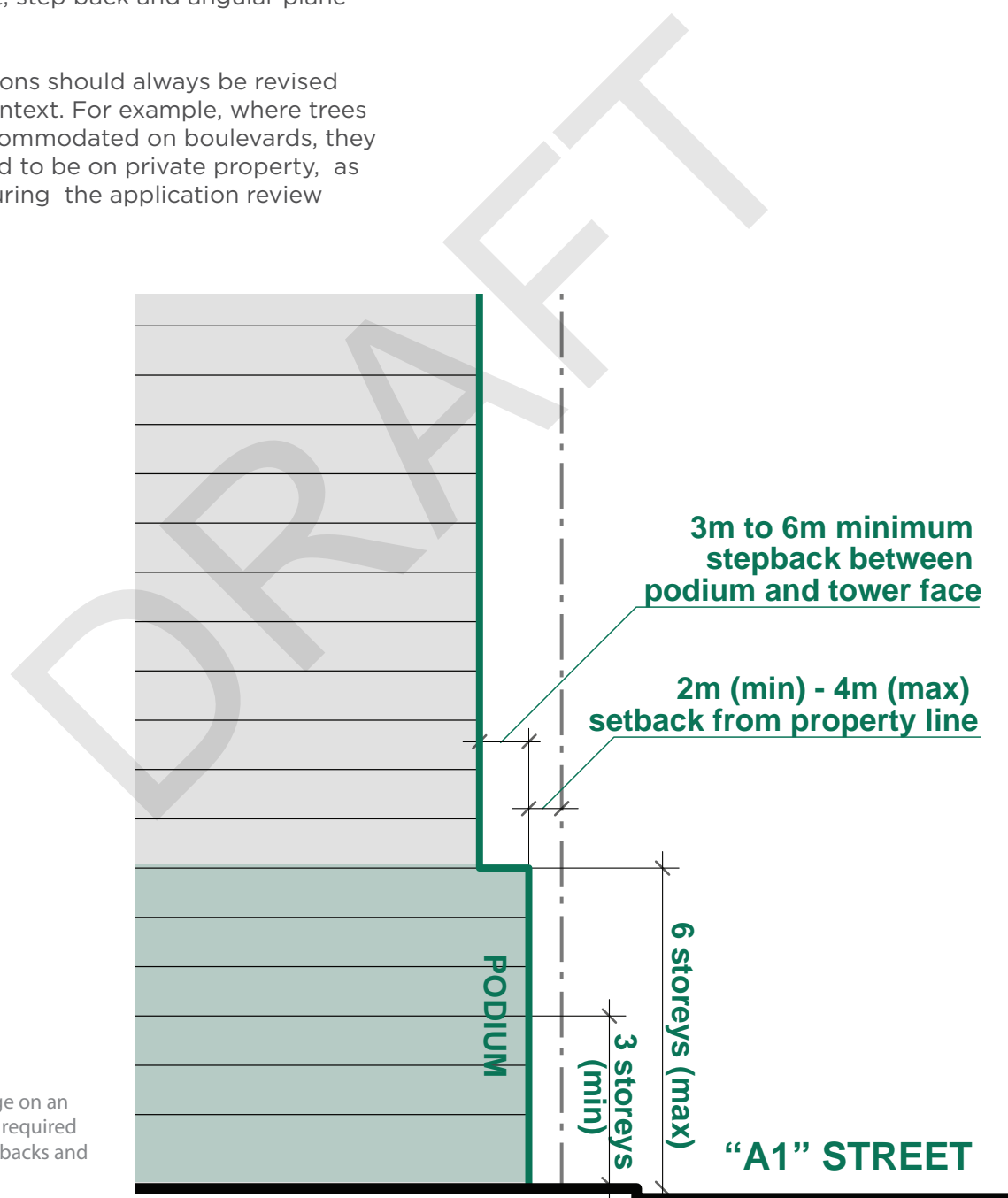


Figure 13: Frontage on an A1- Street with the required podium height, setbacks and stepback

3.2.2 A-Streets Subcategories

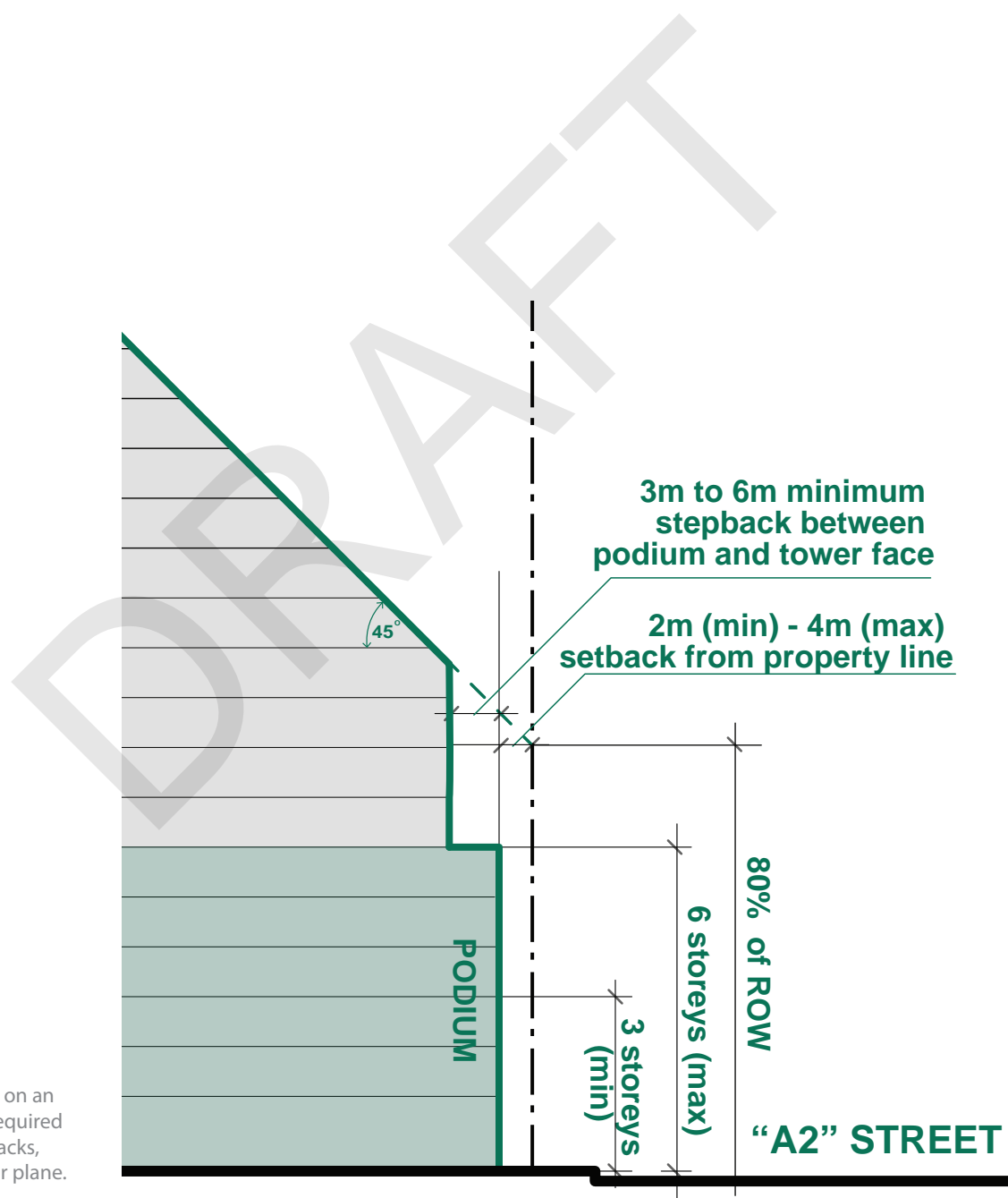


Figure 14: Frontage on an A2- Street with the required podium height, setbacks, setback and angular plane.

3.2.2 A-Streets Subcategories

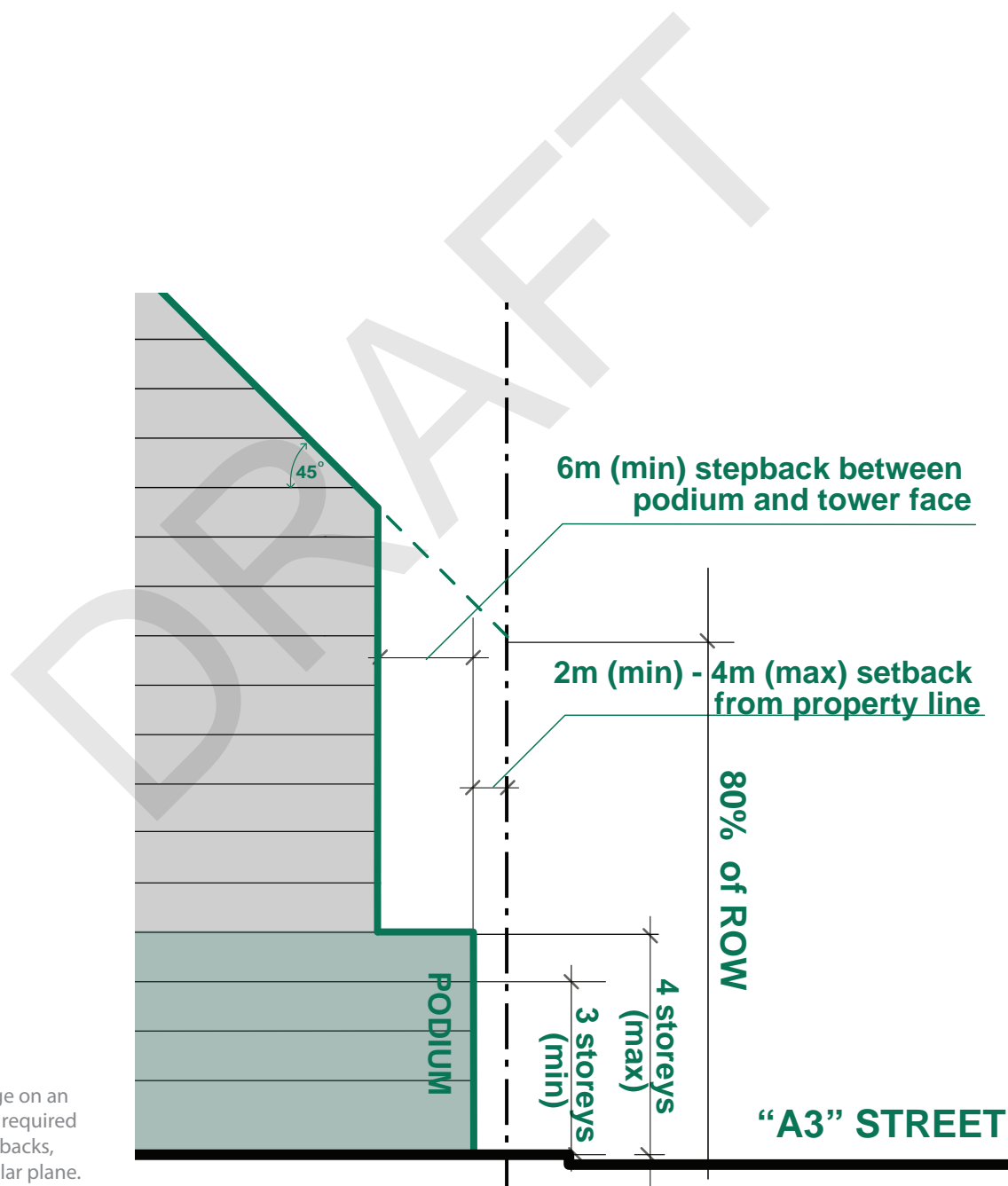


Figure 15: Frontage on an A3- Street with the required podium height, setbacks, setback and angular plane.

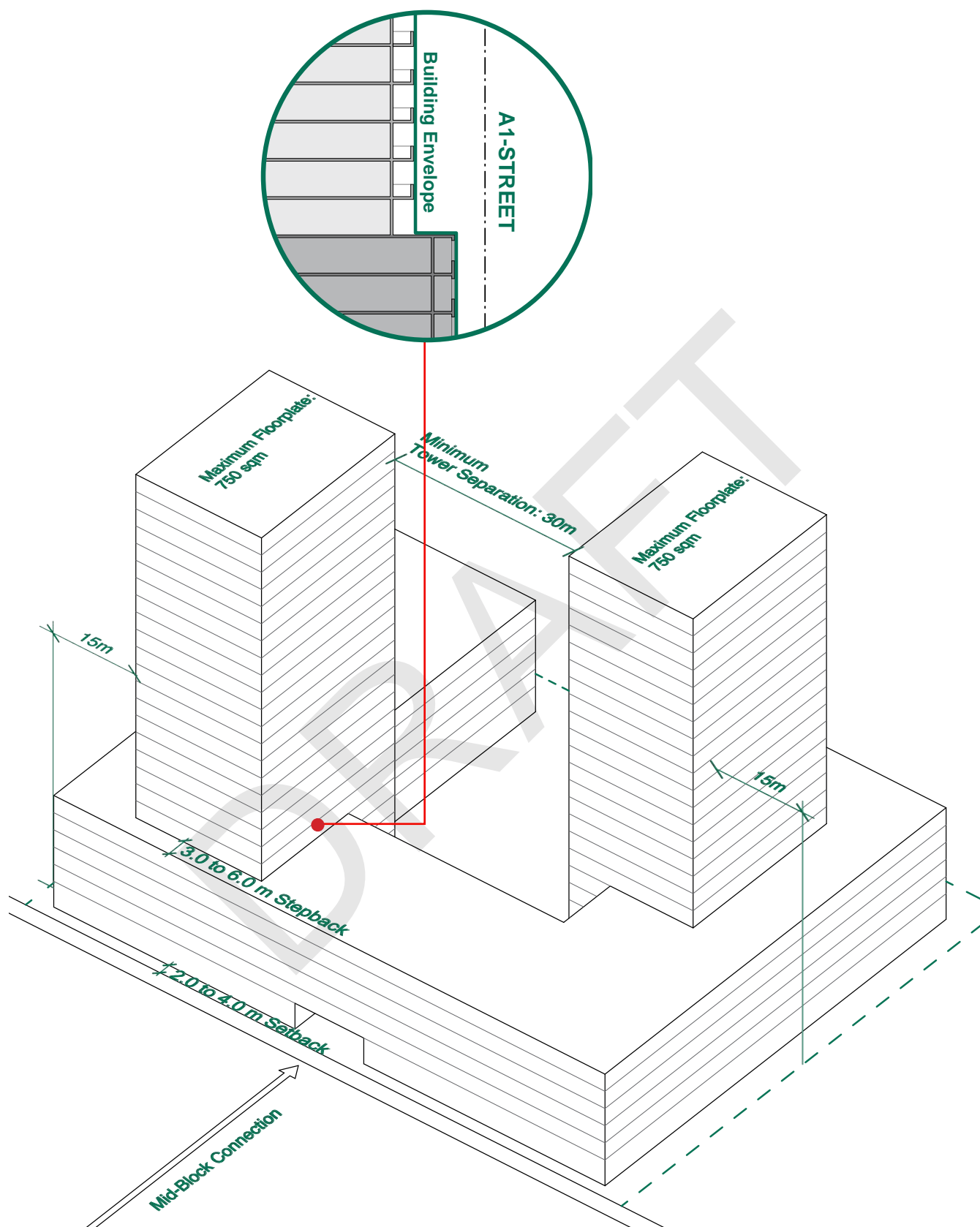


Figure 16: Frontage on an A1 Street. Tower separations, setbacks and floor plate sizes are also indicated.

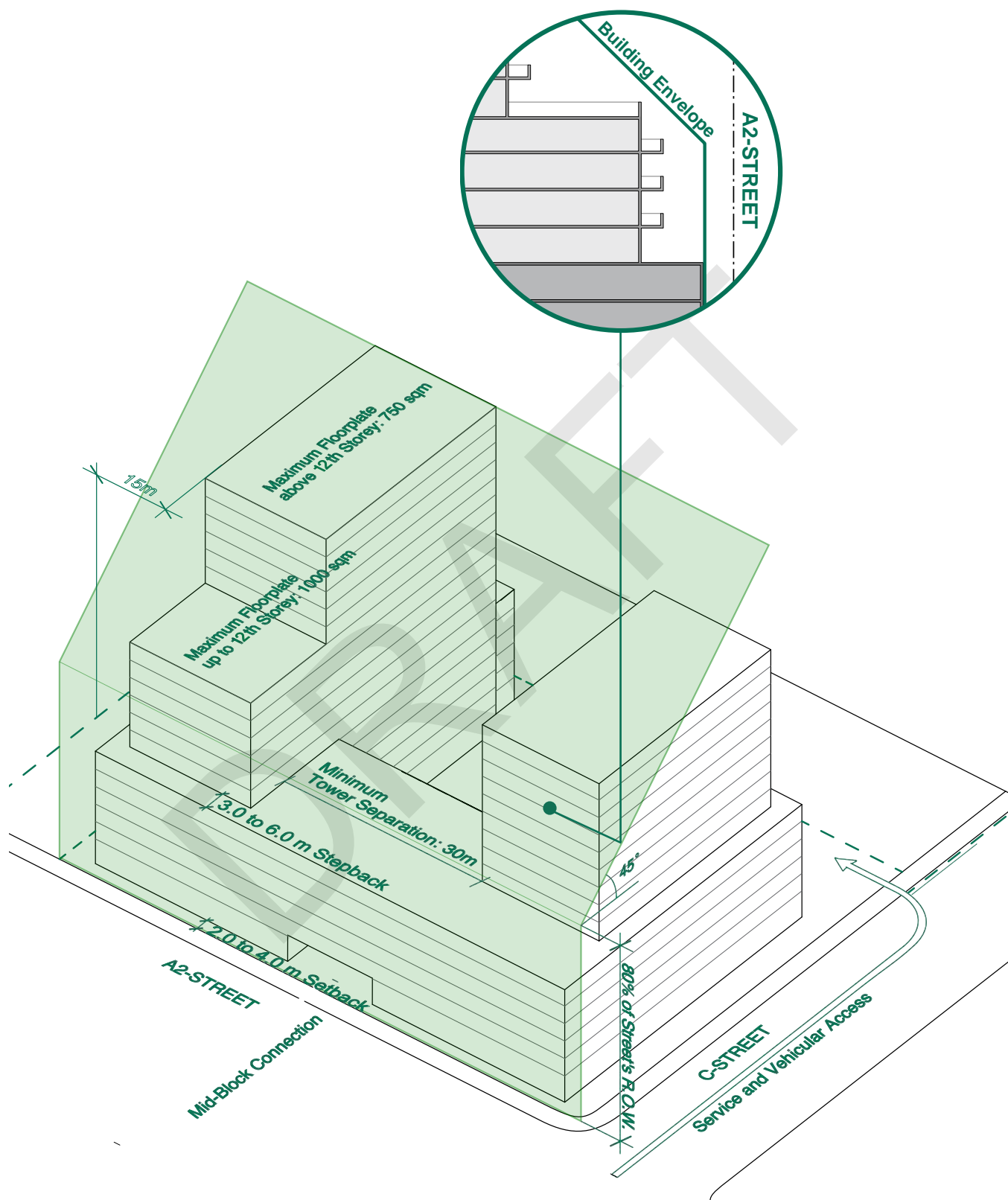


Figure 17: Frontage on an A2 Street with tower separations, setbacks, angular plane and floor plate size requirements indicated.

3.3 B-Streets

B-streets are secondary streets and are more commonly residential in nature. When the ground floor does not have retail uses, entrances should be raised from the sidewalk between 0.6 meters and 1.2 meters above grade and setbacks and landscaping should be designed in a manner that creates a buffer between the public and the private realms and maintain the residential characteristic of these streets.

These guidelines ensure that B-streets have a unified residential character, adequate access to light and sky views, and a building massing that respects the scale of the corresponding context.

3.3.1 General Standards for B-Streets

The following standards apply to residential frontages on B-Streets:

3.3.1.1 Create a high quality seamless interface and transition between the public sidewalk and front door to ensure adequate separation, definition and privacy;

3.3.1.2 Provide individual unit entrances from the sidewalk when possible and animate the frontage with windows on the ground floor that look out onto the street or open space;

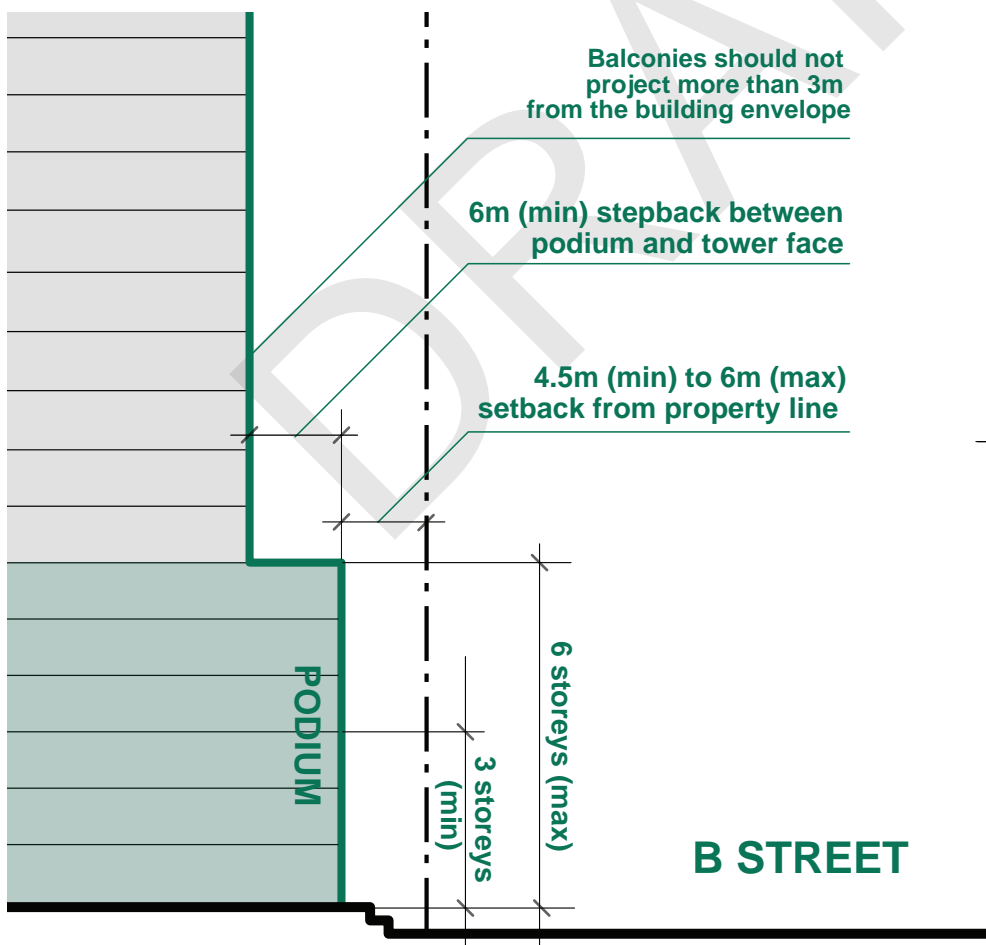


Figure 18: Frontage on a B Street with the required setbacks and stepbacks.

3.3.1.3 Provide high quality landscape treatments within frontages, transition zones and setbacks;

3.3.1.4 Incorporate a minimum setback of 3.0 metres and a maximum of 6.0 meters. This should include front steps and a porch;

3.3.1.5 The ground floor should be raised between 0.6 – 1.2 metres above the sidewalk level;

3.3.1.6 Residential uses should link to the public sidewalk through individual points of access serving each unit.

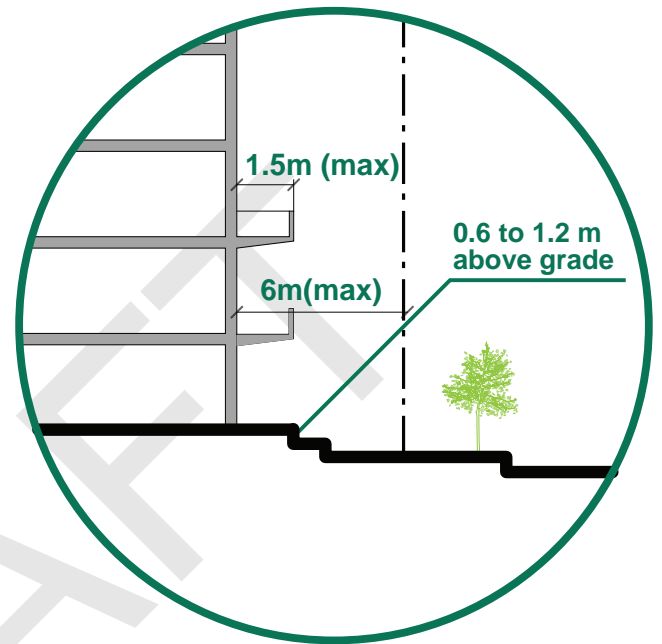


Figure 13: Ground floor conditions on B-Streets. Residential uses are elevated above the ground floor, but directly linked to the public sidewalk through individual points of access serving each unit.



Figure 19: Ground floor conditions on B-Streets. Residential uses are elevated above the ground floor, but directly linked to the public sidewalk through individual points of access serving each unit.

3.3.2 Intersection of A and B Streets

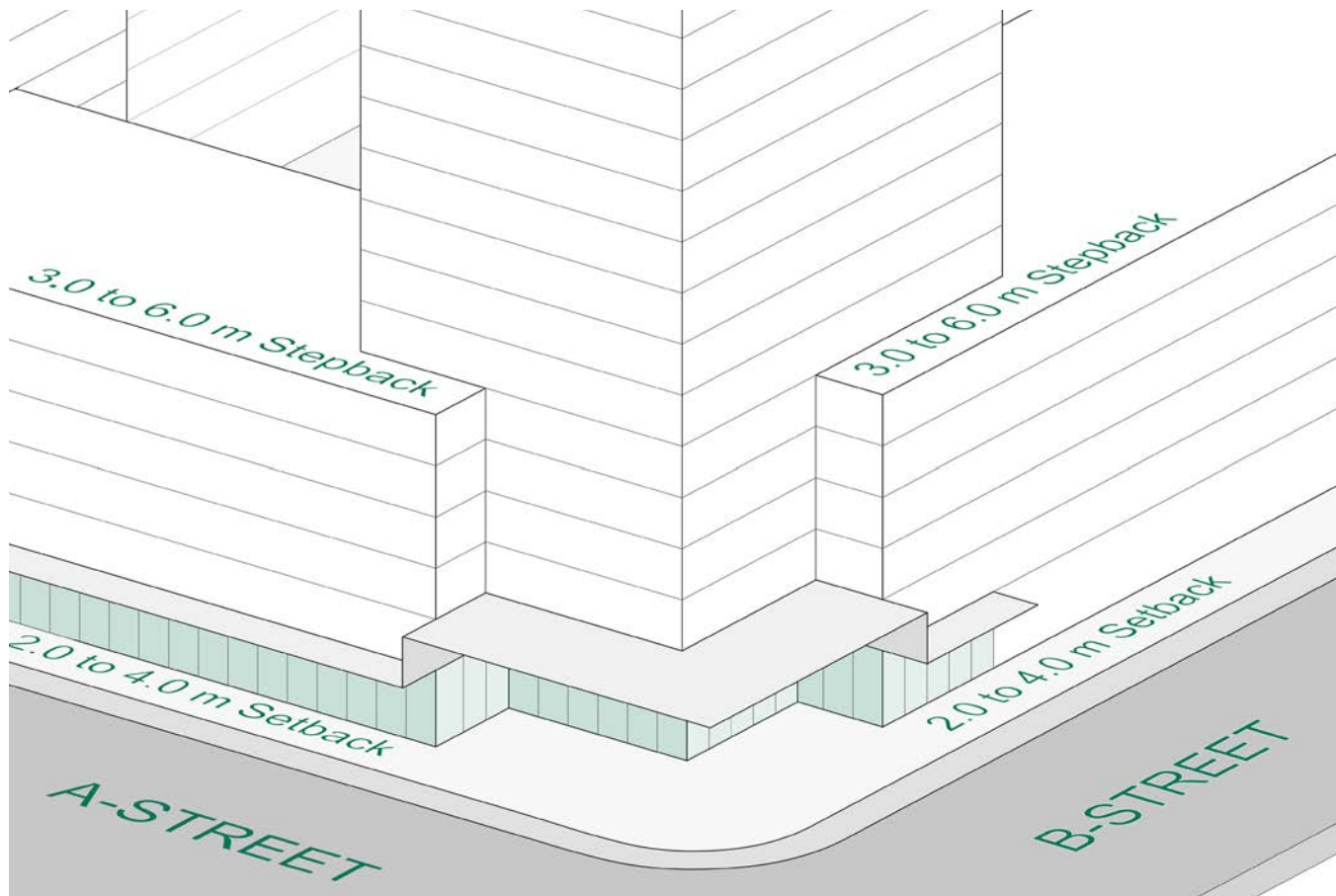


Figure 20: When A and B Streets intersect, commercial uses should wrap around the corner while the tower setback should step back

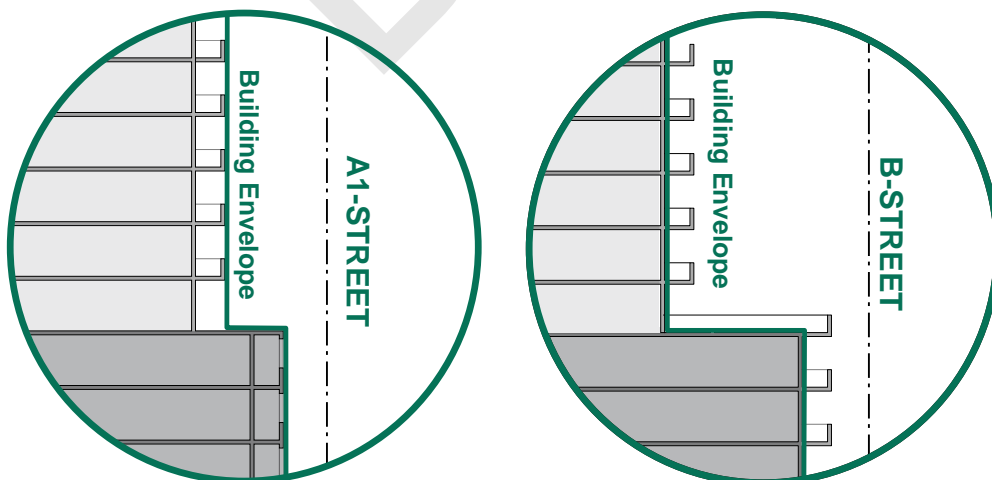


Figure 21: Balconies are only allowed to project when a building fronts a B-Street. On A-Streets, balconies cannot project from the building envelope.

3.4 C-Streets and Pedestrian Connections

3.4.1 General Standards for C-Streets

C-Streets are tertiary streets that connect A and B Streets. In contrast to 'A' and 'B' Streets, C-Streets provide development blocks with access for deliveries, garbage pick-up, service and loading, including vehicular access to structured and off-street parking within development sites.

'C' Streets are also intended to support a pedestrian environment, by integrating a high standard of urban design to support street activity. C-Streets could also accommodate residential and retail uses in some circumstances.

3.4.1.1 Ground floor elevations along 'C' frontages will have a minimum of 50% vision glazing with views into the building.

3.4.1.2 Provide functioning main front entrances to buildings on 'C' Street frontages when there are no 'A or B - Street frontages;

3.4.1.3 Where appropriate, provide functioning secondary entrance(s) for additional pedestrian access; and

3.4.1.4 Service, parking and loading should be

consolidated to serve a number of buildings simultaneously from one area;

3.4.1.5 Loading, garbage and service spaces will be located internal to the building to avoid noise and visual impacts;

3.4.1.6 Prioritize pedestrian safety and the appeal of the public realm by incorporating special architectural and landscape treatments to screen servicing areas and providing a safe level of illumination.

3.4.1.7 The height required for overhead loading for bulk refuse within a collection area should conform to the Region of Peel's standards.



Figure 22: Provision for vehicular access to off-street parking, access for deliveries, garbage pick-up, servicing and loading.

Downtown Mississauga, Webb Dr & Duke of York Blvd

3.4.2 General Standards for Pedestrian Connections

Mid-block and pedestrian connections can build upon the existing road network in certain locations, by providing improved permeability. Additional linkages and access between multiple development blocks and open space should be incorporated. They are intended to have strong civic quality and robust urban character that is inviting, comfortable, accessible and easy to use for pedestrians.

Mid-block pedestrian connections will:

3.4.2.1 Be coordinated across adjacent blocks and generally located mid-block providing direct connectivity between existing and future streets, connections and open space;

3.4.2.2 Be continuous, direct and barrier-free to be pedestrian and transit supportive.

3.4.2.3 Have a minimum overall width of 12.0 metres; with a minimum 4 metre wide walkway zone.

3.4.2.4 Incorporate a strong urban expression and civic presence through design and choice of materials;

3.4.2.5 Be perpendicular to the public sidewalk, providing clear sightlines from one end to the other;

3.4.2.6 Provide minimal grade changes to maximize accessibility;

3.4.2.7 Provide a comfortable year-round enclosure that is open to the sky;

3.4.2.8 Incorporate architectural and landscape treatments to mitigate any tunnel effect and is proportional to the built form;

3.4.2.9 Provide a minimum of 50% vision glazing along the at grade condition to ensure informal surveillance along the entirety of the walkway and incorporate active uses where feasible.

3.4.3 Landscape and Amenities

To promote the pedestrian experience, the following landscape treatments will be provided within mid-block pedestrian connections:

3.4.3.1 A combination of soft and hard landscape treatments to define the walkway edges and amenity areas such as trees, pavers and planters;

3.4.3.2 Landscape species that create visual and seasonal interest;

Figure 23: Provide a minimum of 50% vision glazing along the at grade condition to ensure informal surveillance along the entirety of the walkway (i.e. "eyes on the walkway").

Downtown Mississauga, a pedestrian connection between Prince of Wales Dr & Square One Dr



3.4.3.3 Continuous soil trenches that provide a minimum 20.0 m³ per tree (note: structural soil will be discouraged);

3.4.3.4 Provide a minimum clearance of 1.5 meters between the finished grade of the mid-block connection and the top deck of any below-grade structure;

3.4.3.5 Sod will generally be discouraged;

3.4.3.6 Integrate benches and/or built-in seating spaced at regular intervals;

3.4.3.7 Provide pedestrian scale lighting, bike racks and waste and recycling receptacles;

3.4.3.8 Where desirable, patios internal to the walkway may be incorporated to support active uses, provided they do not obstruct the required (4.0 metre) walkway width; and

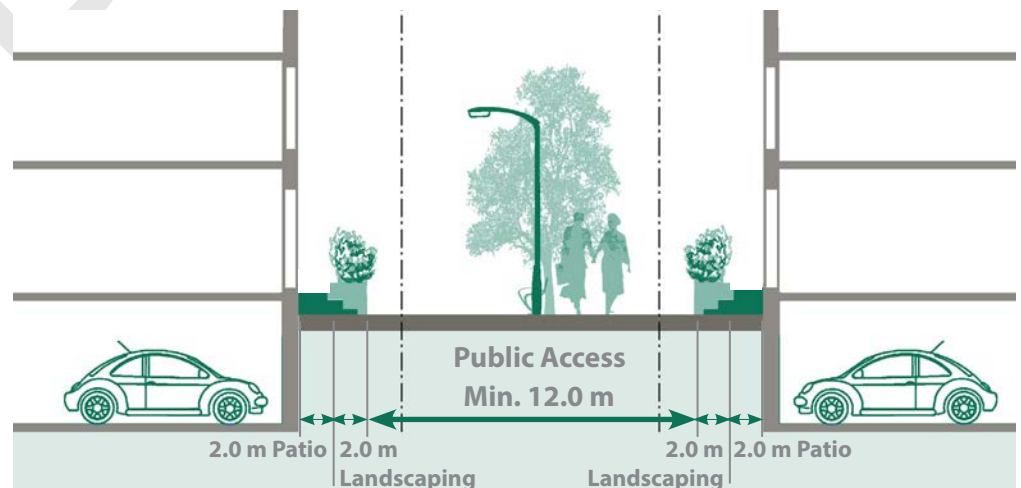
3.4.3.9 High-quality paving with minimal grade transitions and materials that have a strong civic presence that also satisfy the AODA standards.

Figure 24: To promote the pedestrian experience, provide Landscape species that create visual and seasonal interest.

Downtown Mississauga, 352 Princess Royal Dr



Figure 25: Section of mid-block connection.



3.5 Transition from Established Neighbourhoods

To create an appropriate built-form and to protect established neighbourhoods, parks and open spaces, the following standards provide setback and angular plane provision for new developments to create adequate transitions.

The following standards should be used wherever appropriate and could replace the other street frontage standards if deemed necessary by City staff or the applicant.

Figure 27: Transition from an established residential area separated by a road

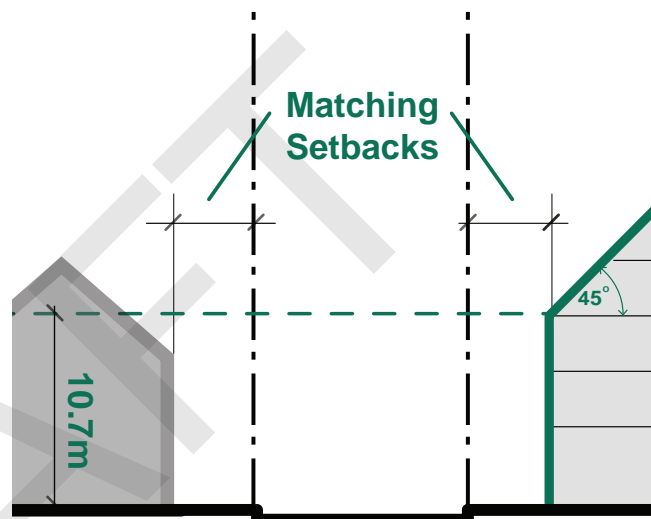


Figure 27: Transition from the rear property line abutting an apartment building in an established neighbourhoods.

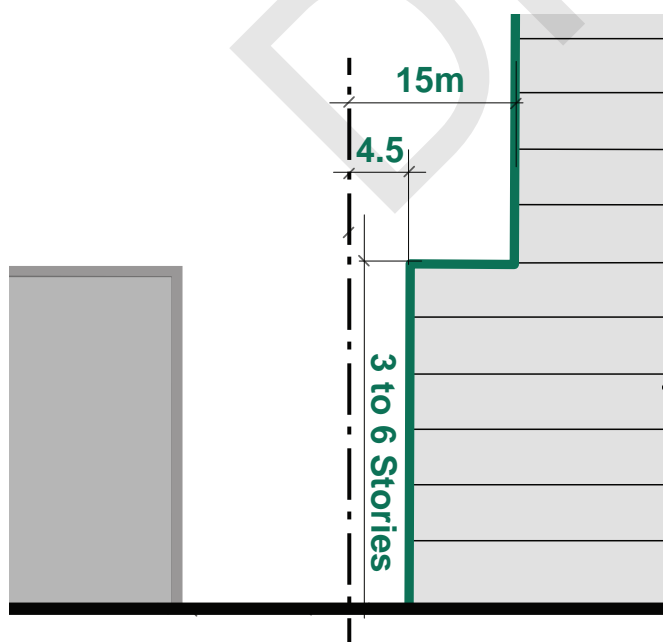
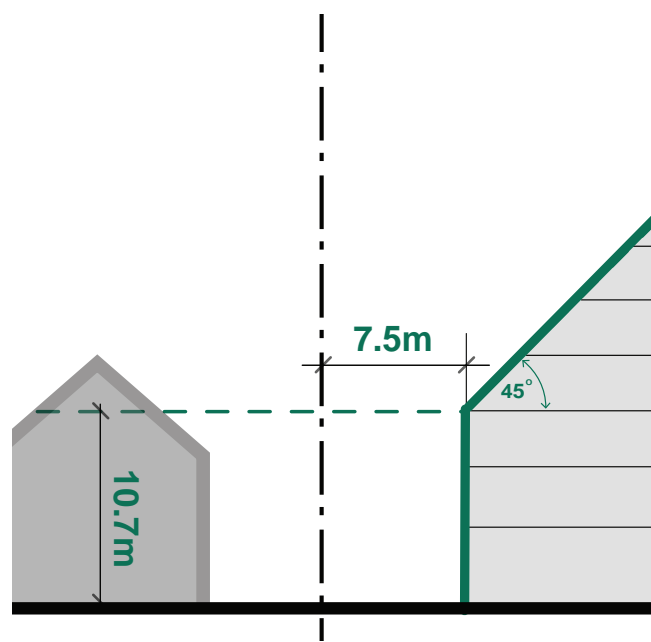


Figure 28: Transition from the rear property line abutting an established residential area or an open space.



Glossary

Articulation - architectural detail that refines and gives a building interest and added richness.
At grade - refers to the uses located at the ground storey, and generally the manner in which they are expressed or articulated to positively support the public realm.

Build-to line - a designated line placed within the build-to area on a development block which informs the placement and orientation of the streetwall or building.

Built form - buildings and structures.

Frontage Design - refers to the built form urban design requirements associated with a category of frontage such as 'A' or 'B' frontage.

Glazing - clear or transparent glass windows whose physical attribute allows light to pass through completely. In reference to at grade uses, it enables views towards the inside of a building space from the exterior at the sidewalk level.

Patio - an outdoor space generally used for dining that adjoins a restaurant, grocery store, coffee shop or café.

Pedestrian scale - a size of building, or space that a pedestrian perceives as not dominating or overpowering.

Main Building Entrance - the primary means of access which serves pedestrians, patrons and/or users of a building through a common entrance, internal lobby or vestibule that provides access to uses other than retail or commercial uses that are located at grade.

Podium - means the base of the building, structure or part thereof, located at or above established grade that project from the tower portion of the building.

Public realm - the streets, parks and open spaces that are available for any member of the public to access, use and enjoy.

Scale - the size of a building or an architectural feature in relation to its surroundings and to the size of a person.

Sidewalk - unobstructed pedestrian travel route generally situated within the public right-of-way.

Streetscape - means the character of the street, including the street right-of-way, adjacent properties between the street right-of-way and building faces. Thus, the creation of a streetscape is achieved by the development of both public and private lands and may include planting, furniture, paving, etc.

Street frontage - the front and/or side of a property that faces an 'A' or 'B' street.

Step-back - a required articulation of the building massing that helps establish the streetwall; it serves to reduce the appearance and bulk of the podium, mitigate the perception of height from the street and reduce shadow and wind impacts.

Storefront - the front of a store, or a room at the ground floor of a building that contains a display window at its front designed for commercial and retail uses.

N.B. Definitions in the Zoning By-law should be consulted and will take precedence over this Glossary.