

Detailed Information and Preliminary Planning Analysis**Owner: Dezen Realty Company Limited****64 and 66 Thomas Street and 65 Tannery Street****Table of Contents**

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1. Site History

- November 2, 2006 – The Streetsville District Plan Review redesignated the lands from **Mixed Commercial/Industrial** and **Greenlands** to **Residential Medium Density II** and **Greenlands**
- June 20, 2007 – Zoning By-law 0225-2007 came into force. The subject lands were zoned **D** (Development) and **G1** (Greenlands – Natural Hazards)
- November 14, 2012 – Mississauga Official Plan (MOP) came into force. The property was designated **Residential Medium Density – Special Site 2** and **Greenlands** in the Streetsville Community Node Character Area

2. Site and Neighbourhood Context

Site Information

The property is located on the east side of Joymar Drive between Tannery Street and Thomas Street in an area in transition from mixed commercial and industrial uses with outside storage to residential uses within the Streetsville Community Node Character Area. Joymar Street is a local road that serves the neighbourhood. Thomas Street connects Queen Street South to Erin Mills Parkway to the west. The property is located within 30 m (98 ft.) of the Streetsville GO Station. There is a multi-tenant commercial/industrial building occupied by an automobile collision centre, automobile repair, a landscaping business and personal services located on the

property. The east side of the subject property includes a portion of Mullet Creek.

The property is regulated by Credit Valley Conservation (CVC) due to the location of Mullet Creek and its associated valley system. A portion of the property is classified as Natural Hazard as it contains floodplain and erosion hazards. The City's Natural Area Survey (NAS) identifies a portion of the property as Natural Green Space (SV10) which is part of Mississauga's Natural Heritage System. The proposed rezoning will be reviewed against MOP policies which are discussed further in Section 6. The redevelopment area will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of the CVC and City.



Aerial Photo of 64 and 66 Thomas Street and 65 Tannery Street



Image of existing conditions on-site facing south

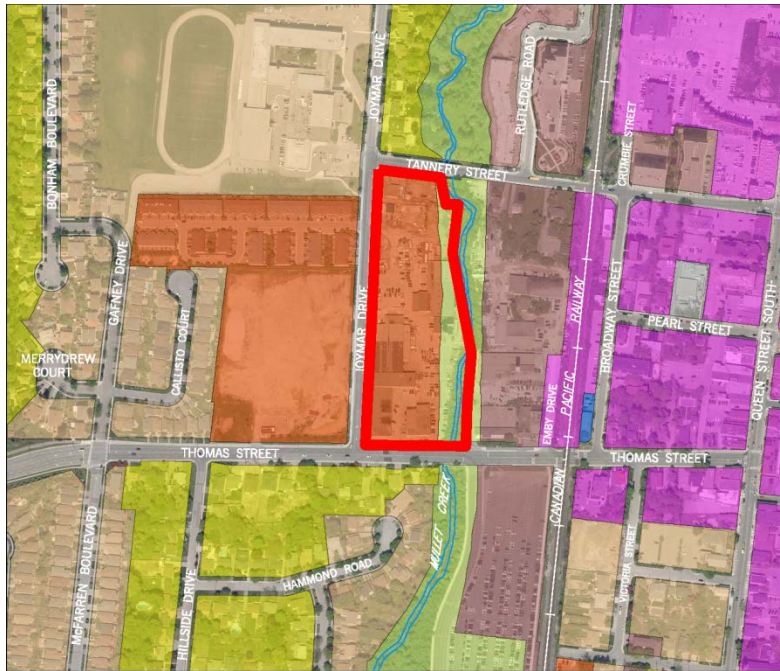
Property Size and Use

Frontages:	293.3 m (962.3 ft.) on Joymar Drive 66.5 m (218.2 ft.) on Tannery Street 106.9 m (350.7 ft.) on Thomas Street
Depth:	106.5 m (349.4 ft.)
Gross Lot Area:	2.77 ha (6.8 ac.)
Existing Uses:	1 and 2 storey multi-tenant commercial/industrial buildings with an automobile collision centre, automobile repair, landscaping business and personal services

Surrounding Land Uses

The surrounding land uses are:

- North: One storey single detached home
- East: One and two storey commercial/industrial buildings (car wash, towing company, automobile repair, construction supplies) and detached homes
- South: One and two storey detached homes (across Thomas Street)
- West: Vacant lands to be developed for two to three storey townhomes on a private road off Joymar Drive



Aerial Photo of 64 & 66 Thomas Street & 65 Tannery Street

The Neighbourhood Context

The subject property is located within the Streetsville Community Node Character Area. The Streetsville Community Node is a historic area, incorporated in 1858 as a village. The immediate and broader area is undergoing transition and growth. The immediate area has a mix of former industrial uses, and more recent medium and high density residential developments. The node contains a variety of housing forms, stores, restaurants, personal services, and places of religious assembly along the Queen Street South mainstreet.

Demographics

Based on the 2016 census, the existing population of the Streetsville Community Node is 1,265 with a median age of 47 (compared to the City's median age of 40). Sixty-four percent of the neighbourhood population are of working age (15 to 64 years of age), with 11% children (0 to 14 years) and 24% seniors (65 years and over). By 2031 and 2041, the population for this area is forecasted to be 2,100 and 2,800 respectively. The average household size is 2 persons with 54% of people living in apartments in buildings that are five storeys or more. The mix of housing tenure for the node is 160 units (23%) owned and 550 units (77%) rented with a vacancy rate of approximately 0.7%. In addition, the number of jobs within this character area is 1,653. Total employment combined with population results in a population and jobs (PPJ) for Streetsville Community Node of 54 persons plus jobs per hectare.

Other Development Applications

There are active development applications directly to the east and west of the subject property. There is a development application for 156 stacked back to back townhomes at 51 and 57 Tannery Street and 208 Emby Drive (NYX Capital Corp.), located east of the site. There is also a development application for 201 stacked back to back townhomes at 80 Thomas Street (Dunpar), located west of the site. The Dunpar applications were appealed to the Local Planning Appeal Tribunal (LPAT), and the applicant is working to complete remaining technical requirements on-site prior to its final approval.

Community and Transportation Services

Streetsville Library is approximately 1 000 m (3,281 ft.) northeast of the site. John Clipperton Park and Streetsville Rotary Park are approximately 500 m (1,640 ft.) and 600 m (1,968 ft.) northeast of the site. Vic Johnston Arena, Streetsville Memorial Park and the Credit River are located approximately 1000 m (3,281 ft.) to the southeast of the subject property. This application is anticipated to have minimal impact on existing services in the community. Additional comments from Community Services can be reviewed within Section 9 of this Appendix.

As mentioned, the site is within 30 m (98.4 ft.) of the Streetsville GO station, which provides two-way, all day bus or train service every 30 minutes during peak period, and every hour outside of peak period. The following major MiWay bus routes currently service the site:

- Route 306 – Streetsville Secondary – Terry Fox South
- Route 305 – Streetsville Falconer
- Route 67 – Streetsville GO
- Route 9 – Rathburn -Thomas
- Route 49 – McDowell
- Route 313 – Streetsville Secondary – Meadowvale Town Centre

There is a signed bicycle route on Joymar Drive.

3. Project Details

The applications are to permit 239 back to back and stacked townhomes.

Development Proposal		
Applications submitted:	Received: July 5, 2019 Deemed incomplete: August 6, 2019 Deemed complete: September 17, 2019	
Developer/ Owner/Applicant:	Dezen Realty Company Limited (Dezen)	
Number of units:	239 back to back and stacked townhomes	
Existing Gross Floor Area:	6 242.3 m ² (67,191.6 ft ²)	
Proposed Gross Floor Area:	24 470 m ² (263,392 ft ²)	
Height:	3 storeys	
Lot Coverage:	43.7%	
Floor Space Index:	1.47	
Landscaped Area:	21%	
Net Density:	144 units/ha (356 units/ac)	
Road Type:	Condominium private road	
Anticipated Population:	741* *Average household sizes for all units (by type) based on the 2016 Census	
Parking:	Required	Provided
resident spaces		363
2 bed/2 bed+den	335	
3 bed	28	
visitor spaces	60	60
Total	423	423
Green Initiatives:	<ul style="list-style-type: none"> • Stormwater retention features • Some permeable pavement • Native vegetation replacement 	

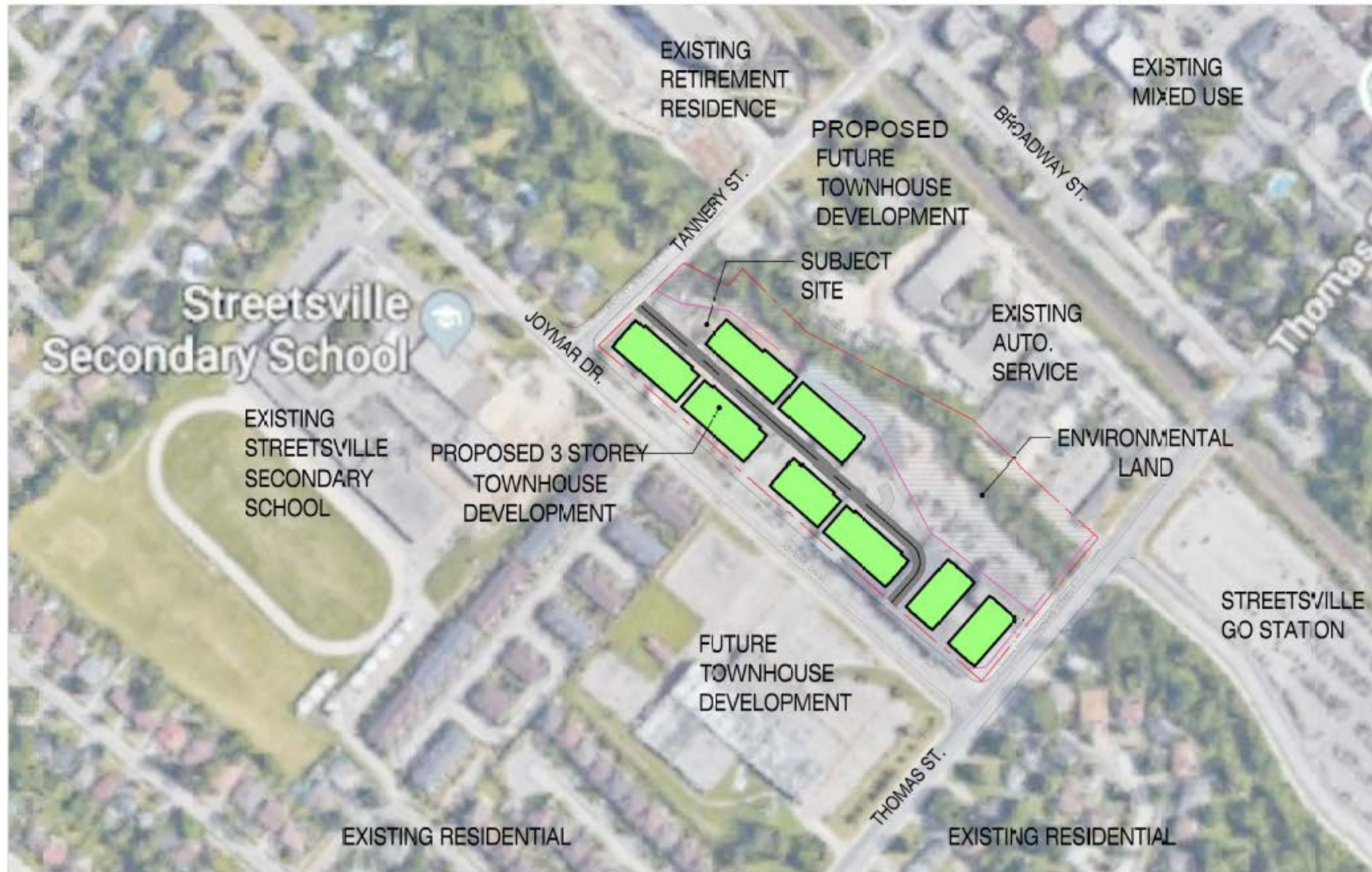
Supporting Studies and Plans

The applicant has submitted the following information in support of the applications which can be viewed at <http://www.mississauga.ca/portal/residents/development-applications>:

- Planning Justification Report
- Concept Plan/Map, Floor Plans, Elevations and Sections
- Urban Design Study
- Underground Parking Plan
- Conceptual Restoration/Enhancement Plan
- Traffic Impact Assessment and Transportation Demand Management Strategy
- Acoustical Feasibility Study
- Arborist Report
- Tree Preservation Plan
- Archaeological Assessment
- Draft Official Plan and Zoning By-law Amendments
- Functional Servicing and Stormwater Management Report
- Environmental Impact Study
- Phase I Environmental Report
- Hydraulic Assessment Memorandum
- Survey Plan and Parcel Register
- Preliminary Grading and Servicing Plans
- Slope Stability Study and Geotechnical Report
- List of Low Impact Design Features



LEGEND	
	PROPERTY LINE
	PROPOSED PROPERTY LINE
	PROPOSED SETBACK LINE
	EXISTING REGIONAL FLOODLINE (CFCA 2018)
	PROPOSED REGIONAL FLOODLINE (CFCA 2018)
	LONG-TERM STABLE TOP OF SLOPE (SIRATI 2018)
	LONG-TERM STABLE TOP OF SLOPE -10m SETBACK (SIRATI 2018)
	PROPOSED DEVELOPMENT LIMIT
	PROPOSED 3:1 SLOPE

**Context Plan/Map**



Sample Elevations (Eight Different Unit Designs Proposed)

4. Land Use Policies, Regulations & Amendments

Mississauga Official Plan

Existing Designation

The site is designated **Residential Medium Density – Special Site 2** and **Greenlands**. The **Residential Medium Density – Special Site 2** designation permits all forms of townhouse dwellings and a maximum floor space index (FSI) of 1.0. The **Greenlands** designation is generally associated with natural hazards and natural areas to provide for the protection, enhancement and restoration of the Natural Heritage System. A portion of the site (lands with cross-hatching) is identified as containing **Natural Hazards**. Permitted uses on lands designated **Natural Hazards** are limited to conservation, flood and/or erosion control, essential infrastructure and passive recreation.

Proposed Designation

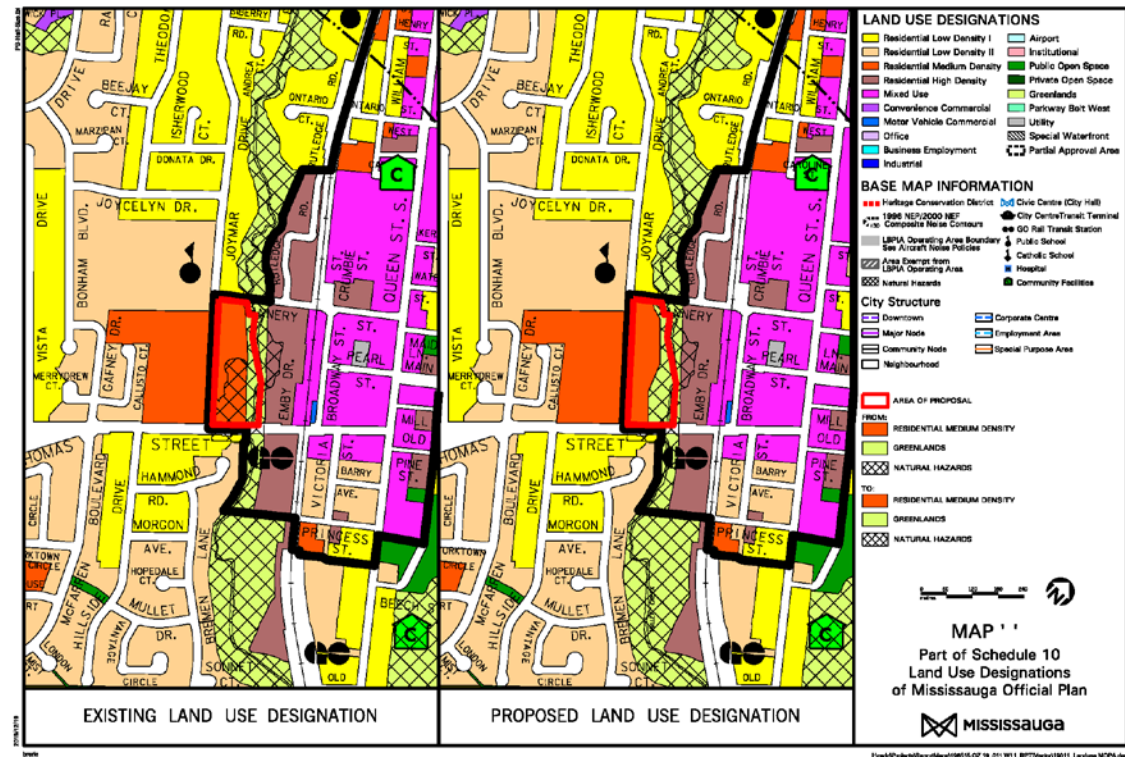
Amendments to the **Residential Medium Density – Special Site 2** designation are proposed for the portion of the site fronting onto Joyomar Drive to permit back to back and stacked townhomes with a maximum FSI of 1.47.

The limits of the **Greenlands** designation will also need to be amended to recognize the lands subject to flood hazards and protect the Mullet Creek valleylands. The Natural Hazards overlay may also be amended.

Through the processing of the applications, staff may recommend a more appropriate designation to reflect the proposed development in the Recommendation Report.

Note: Detailed information regarding relevant Official Plan policies are found in Section 5.

Excerpt of Streetsville Community Node Land Use



Mississauga Zoning By-law

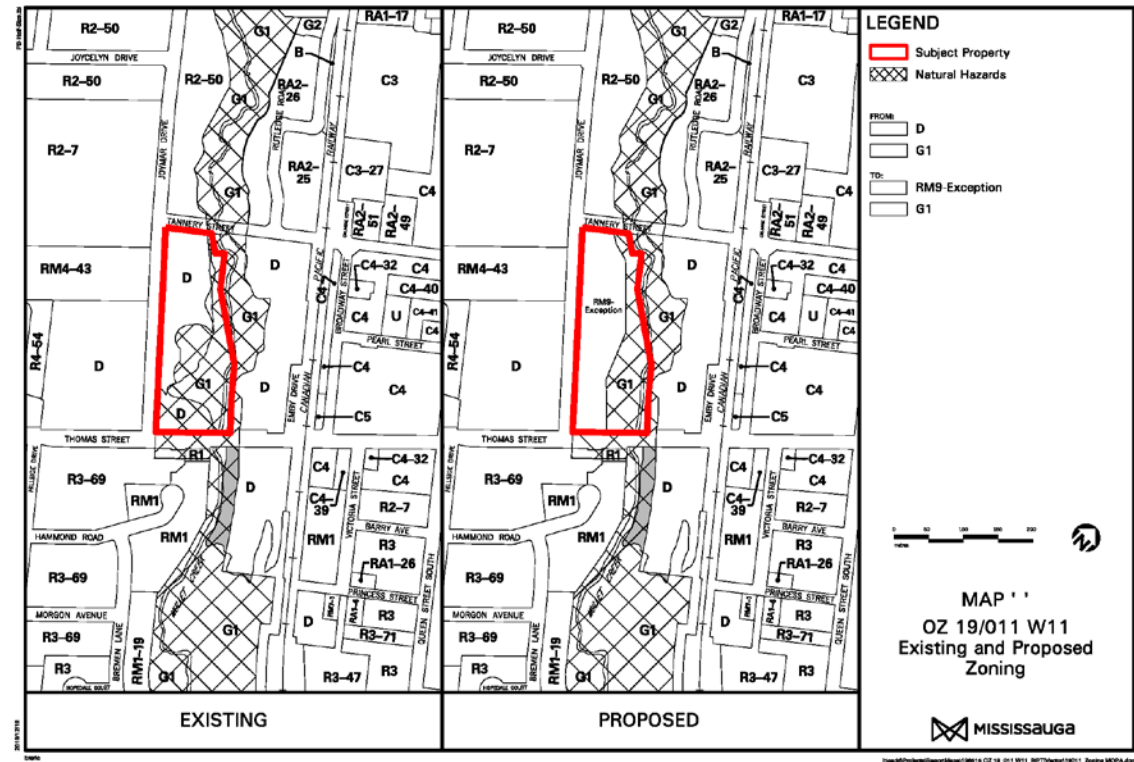
Existing Zoning

The western portion of the site is currently zoned **D** (Development), which permits uses legally existing on the date of the passage of the Zoning By-law.

The remaining portion of the site is zoned **G1** (Greenlands), which permit natural heritage protection and conservation, flood, erosion and stormwater management related uses.

Proposed Zoning

The applicant is proposing **RM9 – Exception** (Stacked Townhouse – Exception) and **G1 (Greenlands)** to permit 239 back to back and stacked townhomes, and to recognize the Mullet Creek valley and floodplains.



Proposed Zoning Regulations

Zone Regulations	RM9 Zone Regulations	Proposed RM9-Exception Zone Regulations
Maximum Floor Space Index (FSI)	1.0	1.47
Maximum Dwelling Height – Flat roof	13.0 m (43 ft.) and 4 storeys	15.0 m (49.2 ft.) and 4 storeys
Minimum Front and Exterior Yard	7.5 m (24.6 ft.)	3.0 m (9.8 ft.)
Minimum Rear Yard	7.5 m (24.6 ft.)	5.0 m (16.4 ft.)
Minimum Setback to a Sight Triangle	N/A	1.0 m (3.3 ft.)
Minimum Internal Setbacks: From the front wall of a building to a condominium road, sidewalk, walkway or parking space not located on a driveway	4.5 m (14.8 ft.)	4.2 m (13.8 ft.)
From a porch, exclusive of stairs, located at and accessible from the first storey or below the first storey, to a condominium road, sidewalk, walkway or parking space	2.5 m (8.2 ft.)	2.0 m (6.6 ft.)
From a rear wall of a building to a rear wall of another building on the same lot	15.0 m (49.2 ft.)	14.0 m (45.9 ft.)
From a side wall of a building to a condominium road, sidewalk or parking space	3.0 m (9.8 ft.)	1.8 m (5.9 ft.)
Minimum setback from an amenity area to a building to any type of road	3.0 m (9.8 ft.)	0.0 m (0.0 ft.)

Zone Regulations	RM9 Zone Regulations	Proposed RM9-Exception Zone Regulations
Minimum setback from an underground parking area to any structure or sidewalk to which it is not attached	N/A	1.0 m (3.3 ft.)
Minimum Landscape Area	40% of lot area	35% of lot area
Minimum Contiguous Amenity Area	5% of the lot area = 1 385 m ² (14,908 sq. ft.)	2.8 m ² (9.3 ft ²) per dwelling unit = 669.2 m ² (7,203.2 sq. ft.)
Minimum Contiguous Private Outdoor Space Per Unit When Located on a Balcony	6.0 m ² (64.6 ft ²)	2.0 m ² (21.5 ft ²)
Note: The provisions listed are based on information provided by the applicant, which is subject to revisions as the applications are further refined.		

5. Summary of Applicable Policies

The *Planning Act* requires that Mississauga Official Plan be consistent with the Provincial Policy Statement and conform with the applicable provincial plans and Regional Official Plan. The policy and regulatory documents that affect these applications have been reviewed and summarized in the table below. Only key policies relevant to the applications have been included. The table should be considered a general summary

of the intent of the policies and should not be considered exhaustive. In the sub-section that follows, the relevant policies of Mississauga Official Plan are summarized. The development application will be evaluated based on these policies in the subsequent recommendation report.

Policy Document	Legislative Authority/Applicability	Key Policies
Provincial Policy Statement (PPS)	<p>The fundamental principles set out in the PPS apply throughout Ontario. (PPS Part IV)</p> <p>Decisions of the council of a municipality shall be consistent with PPS. (PPS 4.2)</p>	<p>Settlement areas shall be the focus of growth and development. (PPS 1.1.3.1)</p> <p>Land use patterns within settlement areas will achieve densities and a mix of uses that efficiently use land, resources, infrastructure, public service facilities and transit. (PPS 1.1.3.2.a)</p>

Policy Document	Legislative Authority/Applicability	Key Policies
	The Official Plan is the most important vehicle for implementation of the Provincial Policy Statement (PPS 4.7)	<p>Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment. (PPS 1.1.3.3)</p> <p>Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected needs of current and future residents of the regional market area. (PPS 1.4.3)</p> <p>Natural features and areas shall be protected for the long term. (PPS 2.1.1)</p> <p>Development shall generally be directed to areas outside of hazardous lands. (PPS 3.1.1)</p> <p>Sites with contaminants in land or water shall be assessed and remediated. (PPS 3.2.2)</p>
Growth Plan for the Greater Golden Horseshoe (Growth Plan)	The Growth Plan applies to the area designated as the Greater Golden Horseshoe growth plan area. All decisions made on or after May 16, 2019 in respect of the exercise of any authority that affects a planning matter will conform with this Plan, subject to any legislative or regulatory provisions providing otherwise. (Growth Plan 1.2.2)	<p>Within settlement areas, growth will be focused in delineated built-up areas; strategic growth areas; locations with existing or planned transit; and, areas with existing or planned public service facilities. (Growth Plan 2.2.1.2 c)</p> <p>Complete communities will feature a diverse mix of land uses; improve social equity and quality of life; provide a range and mix of housing options; provide convenient access to a range of transportation options, public service facilities, open spaces and parks, and healthy, local and affordable food options; provide a more compact built form; mitigate and adapt to climate change impacts; and, integrate green infrastructure. (Growth Plan 2.2.1.4)</p> <p>Municipalities will continue to protect any natural heritage features and areas in a manner that is consistent with the PPS and may continue to identify new systems in a manner that is consistent with the PPS. (Growth Plan 4.2.2.6)</p> <p>To achieve minimum intensification and density targets, municipalities will develop and implement urban design and site design official plan policies and other supporting documents that direct the development of high quality public realm and compact built form. (Growth Plan 5.2.5.6)</p>
Region of Peel Official Plan (ROP)	<p>The Region of Peel approved MOP on September 22, 2011, which is the primary instrument used to evaluate development applications.</p> <p>The existing policies of MOP are consistent with the ROP.</p>	<p>The ROP identifies the subject lands as being located within Peel's Urban System.</p> <p>General objectives of ROP, as outlined in Section 5.3, include conserving the environment, achieving sustainable development, establishing healthy complete communities, achieving intensified and compact form and mix of land uses in appropriate areas that efficiently use land, services, infrastructure and public finances, while taking into account the characteristics of existing communities and services, and achieving an urban form and densities that are pedestrian-friendly and transit supportive.</p>

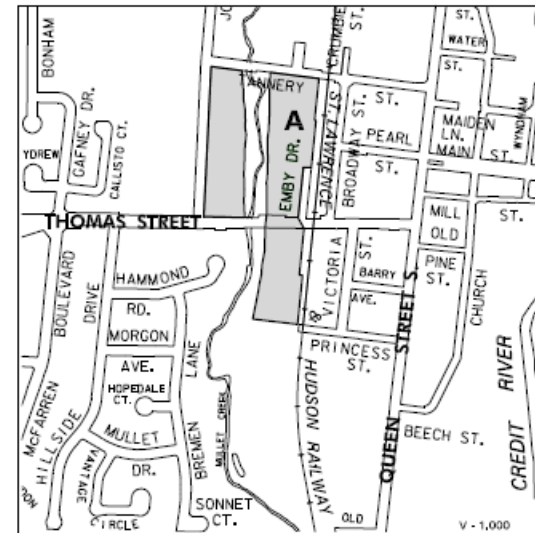
Policy Document	Legislative Authority/Applicability	Key Policies
D-6 Compatibility between Industrial Facilities (Ministry of the Environment, Conservation and Parks)	The D-6 Guideline is intended to be applied in the land use planning process to prevent or minimize future land use conflicts due to the encroachment of sensitive land uses and industrial land uses on one another.	The applications will be reviewed against the D-6 guidelines.

Relevant Mississauga Official Plan Policies

The policies of Mississauga Official Plan (MOP) implement provincial directions for growth. MOP is generally consistent with the PPS and conforms with the Growth Plan, Greenbelt Plan, PBWP and ROP. An update to MOP is currently underway to ensure MOP is consistent with and conform to changes resulting from the recently released Growth Plan, 2019, including those for Major Transit Station Areas (MTSAs).

Notwithstanding that the subject site is in proximity to the Streetsville GO station, the Growth Plan 2019 does not identify it as an MTSA and therefore the MTSA policies of the GP do not currently apply for the site.

The lands are located within the Streetsville Community Node and are designated **Residential Medium Density – Special Site 2** and **Greenlands**. The **Residential Medium Density** designation permits all forms of townhomes. Notwithstanding the provisions of MOP, the following additional policy also applies:



14.11.6.2.1 The lands identified as Special Site 2 are located south of Tannery Street, north of Old Station Road, east of Joymar Drive and west of the St. Lawrence and Hudson Railway.

14.11.6.2.2 Notwithstanding the provisions of the Residential High Density, Residential Medium Density and Greenlands designations, the following additional policies will apply:

- a. the determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City; and
- b. building forms should consist of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks.

14.11.6.2.3 Redevelopment of Area A should include provision for a public road connecting Thomas Street and Tannery Street west of the St. Lawrence and Hudson Railway right-of-way.

The **Greenlands** designation is generally associated with natural hazards and natural areas. A portion of the site is identified as containing Natural Hazards. Permitted uses on lands designated Natural Hazards are limited to conservation, flood and/or erosion control, essential infrastructure and passive recreation.

The applicant is proposing to change the designation to **Residential Medium Density – Exception** to permit 236 back to back and stacked townhomes, with a maximum floor space index of 1.47. The applicant will need to demonstrate consistency with the intent of MOP.

The limits of the **Greenlands** designation will also need to be amended to recognize the lands subject to flood hazards and protect the Mullet Creek valleylands. The Natural Hazards overlay may also be amended.

The following policies are applicable in the review of these applications. In some cases the description of the general intent summarizes multiple policies.

	Specific Policies	General Intent
Chapter 4 Vision		Mississauga will provide the guiding principles that are to assist in implementing the long-term land use, growth and development plan for Mississauga and sets out how the City will achieve these guiding principles.
Chapter 5 Direct Growth	Section 5.1.4 Section 5.1.6 Section 5.1.9 Section 5.2.1 Section 5.3.3.3 Section 5.3.3.4	Mississauga encourages compact, mixed use development that is transit supportive, in appropriate locations, to provide a range of local live/work opportunities. Mississauga will establish strategies that protect, enhance and expand the Green System. Community Nodes will provide for a similar mix of uses as in Major Nodes, but with lower densities

	Specific Policies	General Intent
	Section 5.3.3.6 Section 5.3.3.11	<p>and heights.</p> <p>Community Nodes are intensification Areas and will achieve a gross density of between 100 and 200 residents and jobs combined per hectare. They will achieve an average population to employment ratio of between 2:1 and 1:2, measured as an average across the entire area of each node.</p> <p>Development in Community Nodes will be in a form and density that complements the existing character of historical Nodes or that achieves a high quality urban environment within more recently developed Nodes.</p>
Chapter 6 Value The Environment	Section 6.3 Section 6.3.7 Section 6.3.8 Section 6.3.9 Section 6.3.10 Section 6.3.24.a & b Section 6.3.25 Section 6.3.26 Section 6.3.32 Section 6.3.33 Section 6.3.47 Section 6.7 Section 6.7.1	<p>Mullet Creek is identified as a Natural Hazard as per Schedule 3 Natural Systems in MOP.</p> <p>MOP requires protection, restoration and expansion of the natural heritage system through a number of measures.</p> <p>Mississauga's Natural Heritage System is composed of the following: Significant Natural Areas, Natural Green Spaces, Special Management Areas, Residential Woodlands and Linkages.</p> <p>Buffers are vegetated protection areas that provide a physical separation of development from the limits of natural heritage features and Natural Hazard Lands. Buffers will be determined on a site specific basis as part of an Environmental Impact Study to the satisfaction of the City and conservation authority.</p> <p>Environmental Impact Studies will delineate the area to be analysed, describing existing physical conditions, identify environmental opportunities and constraints, and evaluate the ecological sensitivity of the area in relation to a proposal.</p> <p>Natural hazards are generally unsafe for development due to naturally occurring processes such as flooding and erosion.</p> <p>Lands that meet the criteria of Natural Hazard Lands will be designated Greenlands and zoned to ensure their long term protection, life and property. Uses will be limited to conservation, flood and/or erosion control, essential infrastructure and passive recreation.</p> <p>The exact limit of components of the Natural Heritage System will be determined through site specific studies such as an Environmental Impact Study.</p> <p>The Natural Heritage System will be protected, enhanced, restored and expanded by ensuring that development in or adjacent to the Natural Heritage System protects and maintains natural heritage features and their ecological functions and placing those areas into public ownership.</p>

	Specific Policies	General Intent
		<p>Development and site alteration will not be permitted within erosion hazards associated with valleylands and watercourse features. In addition, development and site alteration must provide appropriate buffer to erosion hazards, as established to the satisfaction of the City and conservation authority.</p> <p>Mississauga will consider the potential impacts of climate change and may increase the risk associated with natural hazard lands.</p>
Chapter 7 Complete Communities	Section 7.1.6 Section 7.2.1 Section 7.2.2 Section 7.2.3	<p>Mississauga will ensure that the housing mix can accommodate people with diverse housing preferences and socioeconomic characteristics and needs.</p> <p>Mississauga will ensure that housing is provided in a manner that maximizes the use of community infrastructure and engineering services, while meeting the housing needs and preferences of Mississauga residents.</p> <p>Mississauga will provide opportunities for:</p> <ol style="list-style-type: none"> the development of a range of housing choices in terms of type, tenure and price; the production of a variety of affordable dwelling types for both the ownership and rental markets; and the production of housing for those with special needs, such as housing for the elderly and shelters. <p>When making planning decisions, Mississauga will ensure that housing is provided in a manner that fully implements the intent of the Provincial and Regional housing policies.</p>
Chapter 9 Build A Desirable Urban Form	Section 9.1.1 Section 9.1.10 Section 9.2.1 Section 9.2.1.3 Section 9.2.1.4 Section 9.2.1.21 Section 9.2.3 Section 9.2.3.3 Section 9.3.1.2 Section 9.3.1.4 Section 9.3.5.2 Section 9.3.5.3 Section 9.3.5.6 Section 9.5.1.1 Section 9.5.1.2 Section 9.5.2.4 Section 9.5.1.15	<p>Mississauga will develop an urban form based on the urban system and the hierarchy identified in the city structure as shown on Schedule 1: Urban System.</p> <p>The City vision will be supported by development that:</p> <ol style="list-style-type: none"> Respects the urban hierarchy Utilizes best sustainable practices Demonstrates context sensitivity, including the public realm Promotes universal accessibility and public safety; and Employs design excellence <p>Built form should provide for the creation of a sense of place through, among other matters, distinctive architecture, and streetscaping.</p> <p>Development will contribute to pedestrian oriented streetscapes and have an urban built form that is attractive, compact and transit supportive.</p> <p>Development will be sensitive to the site and ensure that Natural Heritage Systems are protected, enhanced and restored.</p>

	Specific Policies	General Intent
		<p>Mississauga will ensure that urban form, street patterns and public open space systems are coherent orderly and legible.</p> <p>Development will be designed to:</p> <ul style="list-style-type: none"> • Respect the natural heritage features, such as forests, ridges, valleys, hills, lakes, rivers, streams and creeks • Achieve a street network that connects to adjacent streets and neighbourhoods at regular intervals, wherever possible • Meet universal design principles • Be pedestrian oriented and scaled and support transit use • Be attractive, safe and walkable <p>Residential developments of significant size, except for freehold developments, will be required to provide common outdoor on-site amenity areas that are suitable for the intended users.</p> <p>Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by having regard for the Natural Heritage System, natural hazards (flooding and erosion), and ensuring adequate privacy is maintained through appropriate front, side and rear yards.</p> <p>Noise will be mitigated through appropriate built form and site design. Mitigation techniques such as fencing and berms will be discouraged.</p>
Chapter 11 General Land Use Designations	Section 11.2 Section 11.2.3 Section 11.2.5.5	<p>In addition to the Uses Permitted in all Designations, lands designated Residential Medium Density will also permit the following uses:</p> <ul style="list-style-type: none"> • All forms of townhouse dwellings <p>Lands designated Greenlands are associated with natural hazards and/or natural areas where development is restricted. Permitted uses on Greenlands include conservation related uses, including flood control and/or erosion management, passive recreational uses are also permitted.</p>
Chapter 14 Community Nodes	Section 14.1.1.2 Section 14.1.1.3 Section 14.1.2.2 Section 14.10 Section 14.10.1	<p>For lands within a Community Node, a minimum building height of two storeys to a maximum building height of four storeys will apply unless Character Area policies specify alternative building height requirements.</p> <p>Proposals for heights less than two storeys, more than four storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:</p> <ol style="list-style-type: none"> a. An appropriate transition in heights that respects the surrounding context will be achieved; b. The development proposal enhances the existing or planned development; c. The City Structure hierarchy is maintained; and d. The development proposal is consistent with the policies of this plan.

	Specific Policies	General Intent
Chapter 14.10 Streetsville	Section 14.10.6.2	<p>Notwithstanding the Residential Medium Density policies of this plan, the following additional uses will be permitted:</p> <p>Low-rise apartment dwellings.</p> <p>The lands identified as Special Site 2 are located south of Tannery Street, north of Old Station Road, east of Joymar Drive, and west of the St. Lawrence and Hudson Railway.</p> <p>Notwithstanding the provisions of the Residential High Density, Residential Medium Density and Greenlands designations, the following additional policies will apply:</p> <ol style="list-style-type: none"> The determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City; and Building forms should consist of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks.
Chapter 19 Implementation	Section 19.5.1	<p>This section contains criteria which requires an applicant to submit satisfactory planning reports to demonstrate the rationale for the proposed amendment as follows:</p> <ul style="list-style-type: none"> the proposal would not adversely impact or destabilize the following: the overall intent, goals and objectives of the Official Plan; and the development and functioning of the remaining lands which have the same designation, or neighbouring lands; the lands are suitable for the proposed uses, and compatible with existing and future uses of surrounding lands; there are adequate engineering services, community infrastructure and multi-modal transportation systems to support the proposed application; a planning rationale with reference to Mississauga Official Plan policies, other relevant policies, good planning principles and the merits of the proposed amendment in comparison with the existing designation has been provided by the applicant.

Affordable Housing

In October 2017 City Council approved *Making Room for the Middle – A Housing Strategy for Mississauga* which identified housing affordability issues for low and moderate incomes in the city. In accordance with the Provincial Growth Plan (2019), *Provincial Policy Statement* (2014), Regional Official Plan and

Mississauga Official Plan (MOP), the City requests that proposed multi-unit residential developments incorporate a mix of units to accommodate a diverse range of incomes and household sizes.

Applicants proposing non-rental residential developments of 50 units or more – requiring an official plan amendment or

rezoning for additional height and/or density beyond as-of-right permissions – will be required to demonstrate how the proposed development is consistent with/conforms to Provincial, Regional and City housing policies. The City's official plan indicates that the City will provide opportunities for the provision of a mix of housing types, tenures and at varying price points to accommodate households. The City's annual housing targets by type are contained in the Region of Peel Housing and Homelessness Plan 2018-2028 <https://www.peelregion.ca/housing/housinghomelessness/pdf/plan-2018-2028.pdf>.

To achieve these targets, the City is requesting that a minimum of 10% of new ownership units be affordable. The 10% contribution rate will not be applied to the first 50 units of a development. The contribution may be in the form of on-site or off-site units, land dedication, or financial contributions to affordable housing elsewhere in the city.

6. School Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<p>Student Yield:</p> <p>42 Kindergarten to Grade 5 18 Grade 6 to Grade 8 20 Grade 9 to Grade 12</p> <p>School Accommodation:</p> <p>Ray Underhill Public School</p> <p>Enrolment: 269 Capacity: 350 Portables: 1</p> <p>Dolphin Senior Public School</p> <p>Enrolment: 597 Capacity: 555 Portables: 1</p> <p>Streetsville Secondary School</p> <p>Enrolment: 885 Capacity: 1,088 Portables: 0</p>	<p>Student Yield:</p> <p>17 Junior Kindergarten to Grade 6 14 Grade 9 to Grade 12</p> <p>School Accommodation:</p> <p>St. Joseph (Streetsville) Elementary School</p> <p>Enrolment: 329 Capacity: 478 Portables: 0</p> <p>St. Aloysius Gonzaga Secondary School</p> <p>Enrolment: 1,746 Capacity: 1,656 Portables: 0</p>

7. Community Comments

A community meeting was held by Ward 11 Councillor, George Carlson on Thursday November 7, 2019. Approximately 50 people were in attendance. Approximately 18 written submissions were received.

The following comments made by the community as well as any others raised at the public meeting will be addressed in the Recommendation Report, which will come at a later date.

- The proposed density is too high and there may be impacts on traffic and school capacity
- Hillside Drive has too much traffic. There should be two

exits or traffic lights should be installed at the intersection of Thomas Street and Hillside Drive

- Traffic is especially bad at Hillside Drive and Thomas Street during the morning and evening rush to and from the Streetsville Go Train Station and before and after school at Joymar Drive and Thomas Street due to traffic from Streetville Secondary School
- Provide a pedestrian access from Tannery Street to Mullet Creek and to Thomas Street
- The City should provide additional lighting on local streets
- A grocery store is needed in the vicinity of the subject site

8. Development Issues

The following is a summary of comments from agencies and departments regarding the applications:

Agency / Comment Date	Comments
Region of Peel (October 11, 2019)	<p>An existing 300 mm (11.8 in.) diameter water main is located on Joymar Drive, and an existing 300 mm (11.8 in.) diameter water main is located on Thomas Street. There is an existing 600 mm (23.6 in.) diameter sanitary sewer located on Joymar Drive, and an existing 600 mm (23.6 in.) diameter sanitary sewer located on Thomas Street.</p> <p>Servicing of the site may require municipal and/or private easements and the construction, extension, twinning or upgrading of municipal services. All works associated with the servicing of the site will be at the applicant's expense. The applicant will also be responsible for the payment of applicable fees, development charges, legal costs and all other costs associated with the development of this site. Lines of influence of the proposed underground parking garage shall not encroach into the existing sanitary sewer infrastructure, located within the Region of Peel easement. Private Servicing Easements may be required prior to Regional Servicing Approval.</p> <p>The Region received a Functional Servicing Report (FSR) dated June 21, 2019 by C.F. Crozier and Associates. The report is complete, and will be sent for modelling once a digital copy is provided. A satisfactory FSR is required prior to Recommendation Report.</p> <p>The Region will be required to be party to the Development Agreement. A revised waste management plan and mechanical drawings are required.</p>

Agency / Comment Date	Comments
Dufferin-Peel Catholic District School Board and the Peel District School Board (October 3, 2019 and November 25, 2019)	<p>Dufferin-Peel Catholic District School Board responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by the City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for this development application.</p> <p>Both School Boards require their standard warning clauses to be placed within the Development Agreement to advise that some of the children from the development may have to be accommodated in temporary facilities or bused to schools.</p> <p>In addition, if approved, the Peel District School Board and the Dufferin-Peel Catholic District School Board also require certain conditions to be added to the applicable Development Agreements and to any purchase and sale agreements.</p>
City Community Services Department – Park Planning Section (November 25, 2019)	<p>Future residents of the proposed development will be served by John Clipperton Park (P-512), zoned OS1, which is 0.19 ha (0.46 ac) and contains a playground that is located approximately 500 m (1,640.4 ft.) from the subject property. Streetsville Rotary Park (P-375), zoned C4-51 contains a playground and parking lot, is 0.17ha (0.41ac) and located 600 m (1,968 ft.) from the subject lands.</p> <p>The lands to the rear of the subject property are identified as Natural Hazard Lands within the City's Natural Heritage System as Natural Green Space. Section 6.3.24 of the Mississauga Official Plan states that the Natural Heritage System will be protected, enhanced, restored and expanded through the following measures:</p> <p>a) ensuring that development in or adjacent to the Natural Heritage System protects and maintains the natural heritage features and their ecological functions through such means as tree preservation, appropriate location of building envelopes, grading, landscaping;</p> <p>b) placing those areas identified for protection, enhancement, restoration and expansion in public ownership, where feasible.</p> <p>It is recommended that the identified hazard lands are deeded gratuitously to the City as greenbelt for conservation purposes and appropriately zoned. Should this application be approved, hoarding and fencing will be required along the boundary of the Greenlands. Additionally, securities will be required for greenbelt clean-up, restoration, parkland protection, hoarding, and fencing. Street tree contributions to cover the cost of planting street trees, up to 60mm (2.36 in) caliper, will be required for every 10 m (32.8 ft.) of public street frontage in accordance with current City standards.</p> <p>Furthermore, prior to the issuance of building permits, for each lot or block, cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the <i>Planning Act</i> (R.S.O. 1990, c.P. 13, as amended) and in accordance with City Policies and By-laws.</p>
Credit Valley Conservation (November 25, 2019)	<p>The appropriate restrictive Greenbelt Zoning should be placed over all lands beyond the approved limit of development (natural heritage features and hazards) including buffers and be placed into public ownership for long term protection and maintenance.</p> <p>The proposed limits of development should be revised to reflect this. Furthermore, an updated Hydraulic Assessment</p>

Agency / Comment Date	Comments
<p>Canadian Pacific Railway (November 26, 2019)</p>	<p>Memorandum, Stormwater Management Report, Topographic Map, Servicing and Grading Plans are required.</p> <p>The proposed development is located in close proximity to CPR's Galt Subdivision, which is classified as a principal main line. CPR is not in favour of residential developments adjacent or near their right of way, as this land use is not compatible with railway operations. The health, safety and welfare of future residents could be adversely affected by railway activities.</p> <p>However, to ensure the comfort of adjacent residents and to mitigate as much as possible the inherent adverse environmental factors, CPR has requested that the following be included as Conditions of Approval:</p> <ol style="list-style-type: none"> 1. Dwellings must be constructed such that the interior noise levels meet Ministry of Environment criteria. This will be reviewed by CPR 2. In addition to the warning clauses indicated in the noise report, a warning clause should be inserted in all offers of purchase and sale
<p>Metrolinx (December 6, 2019)</p>	<p>The subject lands are adjacent to CPR's Galt Subdivision, which carries Milton GO Service, and is within 300 m (984.3 ft.) of Streetsville GO Station. As the owner of the railway, CPR is the primary commenting agency in this regard. However, Metrolinx does have interests with respect to noise abatement for the subject development. A detailed noise study as well as an environmental easement for operational emissions, registered on title against the subject residential building in favour of Metrolinx, will be required prior to Site Plan Approval. Warning clauses will be required in the Development Agreement.</p>
<p>City Transportation and Works Department (December 16, 2019)</p>	<p>The Transportation and Works Department has received drawings and reports in support of the above noted application and the owner has been requested to provide additional technical details and revisions in support of the application, prior to Recommendation Report, as follows:</p> <p>Municipal Works - Municipal Works will be required to support this development, including the construction of the municipal boulevards within the Joymar Drive, Tannery Street and Thomas Street right-of-ways and associated road restoration. These works shall form part of the Development Agreement. Detailed design, securities and insurance will be addressed through the Engineering Submission review and approval process.</p> <p>Supporting Drawings - An Underground Parking Plan is to be submitted. Drawings are to be revised to clearly delineate the underground parking structure. All surface works within right-of-ways. ie roadway reinstatement, and details for proposed services to service this site are to be shown. All proposed right-of-way widenings and other land dedications are to be shown on all plans.</p> <p>Noise Study - The preliminary Noise Study is to be updated to provide additional analysis for the private below grade patios. Clarification is required regarding mitigation for the two options presented in the report, one of which requires off-site noise mitigation.</p> <p>Functional Servicing and Stormwater Management Report (FSR) – The FSR is to be updated to address inconsistencies in the stormwater catchment area details. Confirmation that this site will have no impact on the existing storm sewer capacity on Thomas Street through a sewer design sheet is required. Confirmation that there will be no upstream or downstream impact on Mullet Creek is also required.</p> <p>Slope Stability - The Slope Stability Report is to be updated to address any potential impact of the proposed drainage of Catchment 201 to the long term slope stability of Mullet Creek.</p>

Agency / Comment Date	Comments
	<p>Environmental – A Letter of Reliance from a Qualified Person is required for the Phase I Environmental Site Assessment (ESA). Additionally, as lands are to be dedicated to the City, conveyance lands shall be clearly shown in all the supporting reports and drawings.</p> <p>The Phase One ESA indicates potential for contamination. Therefore, a Phase Two Environmental Site Assessment (ESA), including a Letter of Reliance, is required to be submitted to T&W for review.</p> <p>As the proposed land use is changing from a less sensitive (commercial/industrial) to a more sensitive (residential) use, the applicant is required to submit a complete Record of Site Condition (RSC), including all supporting documents to the Transportation and Works Department for review. The RSC must be posted to the Ministry of the Environment, Conservation and Parks Environmental Site Registry (ESR).</p> <p>If dewatering is required as part of development, approval from the Transportation and Works Department to discharge to the City's storm sewer network will be required.</p> <p>Traffic – A road widening will be required over parts of the property and reference plans must be provided for review. These parts are to be clearly shown on all drawings. The proposed site access via Joymar Drive must align with the access to 80 Thomas Street, the neighbouring property located to the west of the site. Drawings should be updated to reflect this.</p> <p>The Traffic Impact Study is to be updated to include signal warrant analysis for the intersection of Thomas Street and Joymar Drive.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> - City Planning Strategies – Healthy By Design - City Community Services Department, Heritage Planning Section - City Community Services Department, Arborist – City & Private Property - City Community Services Department, Public Art - Canada Post Corporation - Enbridge/Consumers Gas - Greater Toronto Airport Authority - Alectra - Mississauga Transit - Planning Services Centre - Trillium Health Partners
	<p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"> - Realty Services - Conseil Scolaire de District Catholique Centre-Sud - Conseil Scolaire Viamonde - Bell Canada - Rogers Cable

Based on the comments received and the applicable Mississauga Official Plan policies, the following matters will have to be addressed:

- Are the policies and principles of Mississauga Official Plan maintained by this project?
- Is the proposal compatible with the existing and planned character of the area given the project's land use, massing, setbacks, building configuration, site access, pedestrian connections and grading?
- Are the proposed zoning by-law exception standards appropriate?
- Is the proposed amount of amenity space appropriate?
- Resolution of the proposed waste collection area
- Resolution of the proposed fire route
- Can the contamination issues be addressed?
- Arrangements to address the City's affordable housing policies
- Delineation of the underground parking garage to ensure it doesn't encroach into hazard area
- Is the sun shadow study acceptable?

Development Requirements

There are engineering matters including: grading, environmental, engineering, noise, servicing and stormwater management that will require the applicant to enter into agreements with the City. Prior to any development proceeding on-site, the City will require the submission and review of an application for site plan approval.

9. Section 37 Community Benefits (Bonus Zoning)

Should these applications be approved by Council, staff will report back to Planning and Development Committee on the provision of community benefits as a condition of approval.

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