

**Detailed Information and Preliminary Planning Analysis**

**Owner: Edenshaw SSR Developments Ltd.**

**49 South Service Road**

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## 1. Proposed Development

The applicant proposes to develop the property with a 26 storey apartment building containing 352 units. Official plan amendment and rezoning applications are required to permit the proposed development (refer to Section 4 for details concerning the proposed amendments).

Development Proposal		
Applications submitted:	Received: November 2, 2022 Deemed complete: November 3, 2022	
Developer/ Owner:	Edenshaw SSR Developments Ltd.	
Applicant:	Sajecki Planning Ltd.	
Number of units:	352 units	
Existing Gross Floor Area:	Vacant non-residential building	
Proposed Gross Floor Area:	21 348 m <sup>2</sup> (229,788 ft <sup>2</sup> )	
Height:	26 storeys / 90.6 m (297.2 ft.)	
Lot Coverage:	24.8%	
Floor Space Index:	4.9 FSI	
Landscaped Area:	54.7%	
Road Type:	Public	
Anticipated Population:	780* *Average household sizes for all units (by type) based on the 2016 Census	
Parking:	Required	Provided
Resident spaces	317	102
Visitor spaces	70	35
Total	387	137
Green Initiatives:	<ul style="list-style-type: none"> <li>Potential LEED Certification</li> <li>Stormwater Reuse On-Site (Full List Provided on Active Development Webpage)</li> </ul>	

## Supporting Studies and Plans

The applicant has submitted the following information in support of the applications which can be viewed at <http://www.mississauga.ca/portal/residents/development-applications>:

- Planning Justification Report
- Air Quality Study
- Archaeological Assessment
- Geotechnical Investigation
- Context Plan
- Site Plan
- Landscape Plan
- Floor Plans, Roof Plan and Underground Parking Plans
- Elevations, Sections and Renderings
- Housing Report
- Hydrogeological Assessment
- List of Low Impact Design Features
- Noise and Vibration Feasibility Study
- Sun/Shadow Study
- Archaeological Assessment
- Draft Official Plan and Zoning By-law Amendments
- Functional Servicing Report
- Stormwater Management Report
- Traffic Impact Study
- Phase I and II Environmental Site Assessments
- Pedestrian Wind Study
- Survey and Site Statistics
- Grading, Utilities and Servicing Plans and Sections
- Streetscape Feasibility Plan and Sections

- Multi-Use Demand Table
- Waste Management Plan
- Tree Inventory and Preservation Plan Report

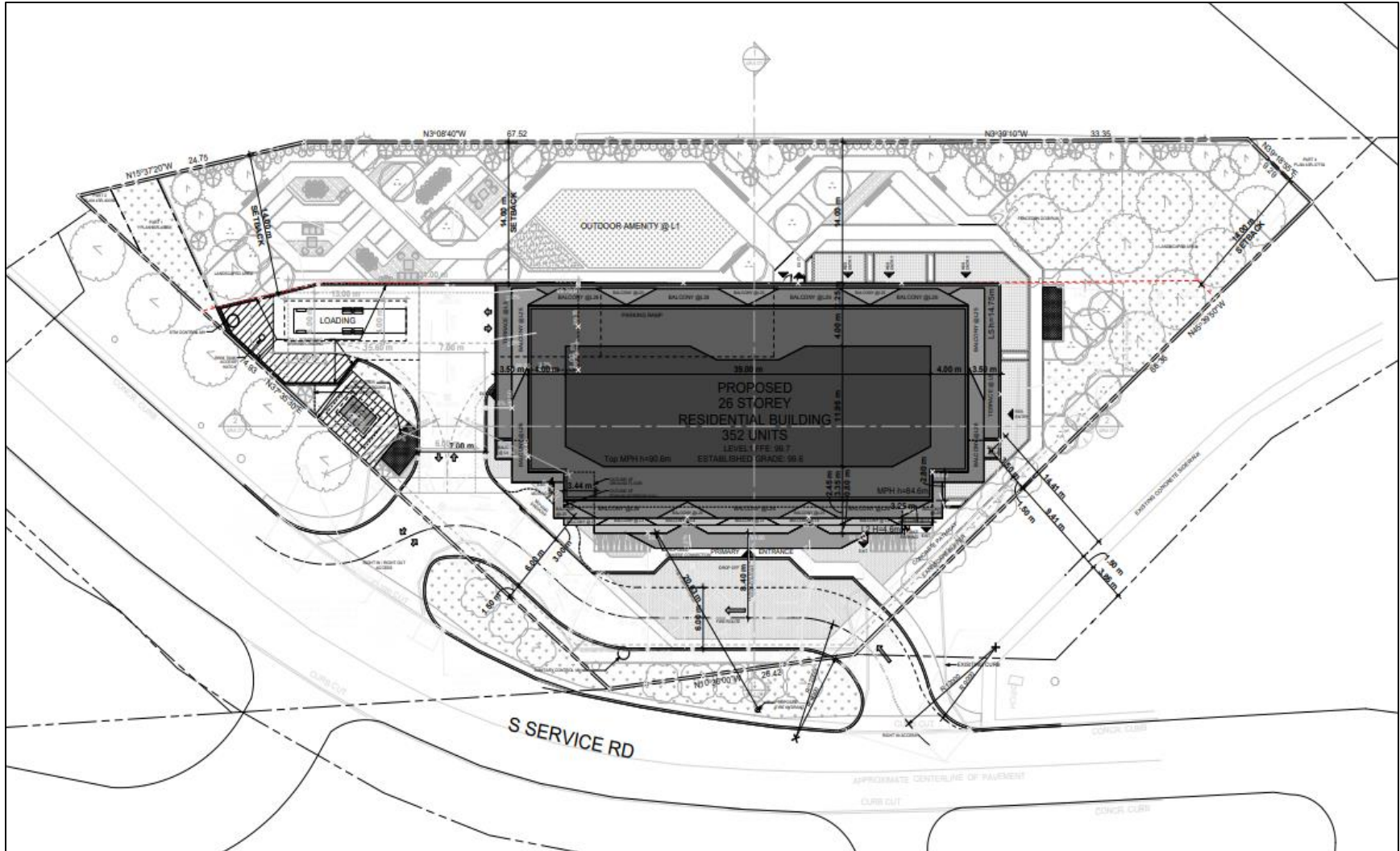
The applications will be reviewed by the Mississauga Urban Design Advisory Panel once the proposal is at a stage where concerns have been addressed in regards to built form. The Urban Design Advisory Panel is an advisory body and makes recommendations to staff for consideration.

### **Application Status**

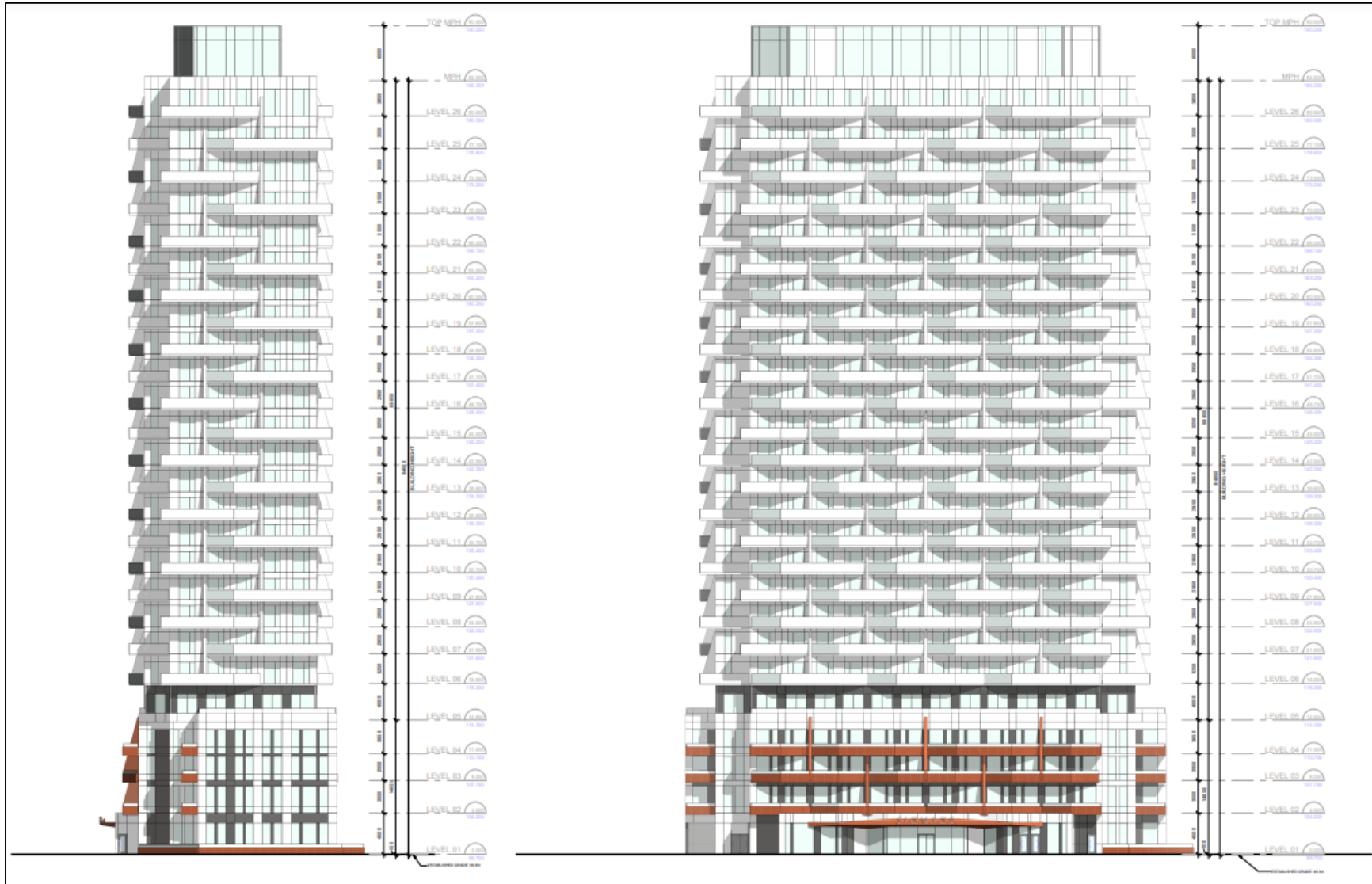
Upon deeming the applications complete, the supporting studies and plans were circulated to City departments and external agencies for review and comment. These comments are summarized in Section 7 of this appendix and are to be addressed in future resubmissions of the applications.

A community meeting was held by Ward 1 Councillor, Stephen Dasko, on February 7, 2023. Refer to Section 6 of this appendix for a summary of comments received at the community meeting and from written submissions received about the applications.

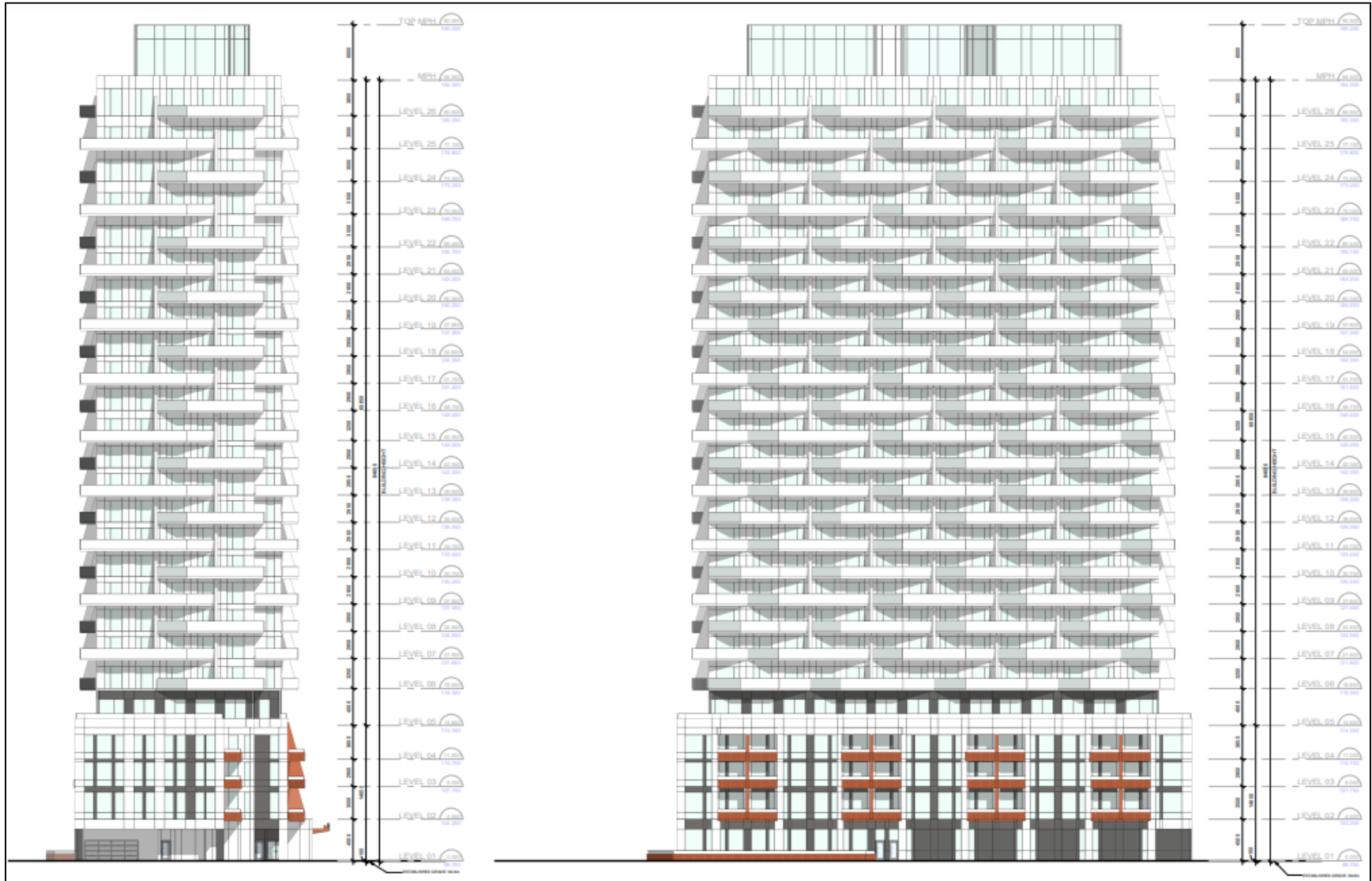
### Site Plan, Elevations and Renderings



Site Plan



North and East Apartment Elevations



South and West Apartment Elevations





Renderings

## 2. Site Description

### Site Information

The property is located on the north side of South Service Road and east of Hurontario Street within the Mineola Neighbourhood Character Area. The site is currently occupied by a vacant two storey non-residential building (former Ontario Provincial Police (OPP) station) and an accessory structure.



Aerial photo of 49 South Service Road

Property Size and Use	
Frontage:	+/- 26.4 m (86.6 ft.)
Gross Lot Area:	+/- 0.4 ha (1.0 ac.)
Existing Uses:	Vacant



Image of existing conditions facing northwest from South Service Road

### Site History

- June 20, 2007 – Zoning By-law 0225-2007 came into force which zoned the property **R1** (Detached Dwellings – Typical Lots)
- November 14, 2012 – Mississauga Official Plan (MOP) came into force which designated the property **Residential Low Density II**
- November 2, 2022 – Submission of official plan amendment and rezoning applications to permit a 26 storey apartment building containing 352 units



### 3. Site Context

#### Surrounding Land Uses

The immediate surrounding area includes a three storey non-residential building known as the New Port Credit Corporate Centre, Queen Elizabeth Sr. Public School and the Queen Elizabeth Way (QEW). The broader surrounding area includes a place of religious assembly and residential uses in the form of townhomes and detached dwellings.

The surrounding land uses are:

North: QEW  
East: South Service Road and Queen Elizabeth Sr. Public School  
South: South Service Road and New Port Credit Corporate Centre  
West: Hurontario Street

#### Neighbourhood Context

The property is located at the periphery of the Mineola Neighbourhood Character Area. The neighbourhood is characterized predominantly by residential uses in the form of detached dwellings, office uses within detached dwellings and higher density uses such as townhomes in select locations. The Downtown Hospital Character Area is located north of the QEW and the Mineola Neighbourhood Character Area.

West of the property is Hurontario Street, which is identified as an Intensification Corridor in MOP. Hurontario Street is comprised of residential and non-residential uses in low-rise buildings, primarily detached dwellings, within the Mineola Neighbourhood Character Area and buildings with various heights within the Downtown Hospital Character Area.

The Hurontario Light Rail Transit System (LRT), also known as the Hazel McCallion LRT, is currently under construction along Hurontario Street. The North Service LRT stop is located approximately 400 m (1,312 ft.) from the property at the intersection of North Service Road and Hurontario Street.



Aerial photo of 49 South Service Road

## Demographics

Based on the 2016 census, the existing population of the Mineola Neighbourhood area is 9,590 with a median age of this area being 44 (compared to the City's median age of 40). 66% of the neighbourhood population are of working age (15 to 64 years of age), with 18% children (0-14 years) and 16% seniors (65 years and over). By 2031 and 2041, the population for this area is forecasted to be 10,500 and 10,700 respectively. The average household size is 3 persons with 0% of people living in apartments in buildings that are five storeys or more. The mix of housing tenure for the area is 2,990 units (91%) owned and 315 units (10%) rented with a vacancy rate of approximately 0.8%\*. In addition, the number of jobs within this Character Area is 932. Total employment combined with the population results in a people plus jobs per hectare (PPJ) for Mineola Neighbourhood of 20 persons plus jobs per ha.

\*Please note that vacancy rate data does not come from the census. This information comes from CMHC which demarcates three geographic areas of Mississauga (Northeast, Northwest, and South). This specific Character Area is located within the South geography. Please also note that the vacancy rate published by CMHC is ONLY for apartments.

## Other Development Applications

The following development applications are in process or were recently approved in the immediate vicinity of the property within the Mineola Neighbourhood Character Area:

- OZ/OPA 14/004 W1 – 1640 Crestview Road – applications approved for twenty (20) townhomes on a condominium

road

- OZ/OPA 17/021 W1 – 1575 Hurontario Street – applications approved for eighteen (18) townhomes on a condominium road
- OZ 18/001 W1 – 200 South Service Road and 201 Radley Road – application in process for five (5) detached dwellings

## Community and Transportation Services

The property is located in proximity to Mary Fix Park. At a larger distance, Kenollie Park and Spruce Park provide additional park options.

MiWay Route 2 (Hurontario Street) currently services the property. Once construction is complete, the Hazel McCallion LRT will also service the property. A multi-use trail is provided along Hurontario Street. A multi-use trail is also provided east of the property which connects South Service Road to Hurontario Street through a QEW underpass.



#### 4. Summary of Applicable Policies, Regulations and Proposed Amendments

The *Planning Act* requires that MOP be consistent with the Provincial Policy Statement and conform with the applicable provincial plans and Regional Official Plan. The policy and regulatory documents that affect these applications have been reviewed and summarized in the table below. Only key policies relevant to the applications have been included. The table

should be considered a general summary of the intent of the policies and should not be considered exhaustive. In the subsection that follows, the relevant policies of MOP are summarized. The development application will be evaluated based on these policies in the subsequent recommendation report.

Policy Document	Legislative Authority/Applicability	Key Policies
<b>Provincial Policy Statement (PPS)</b>	<p>The fundamental principles set out in the PPS apply throughout Ontario. (PPS Part IV)</p> <p>Decisions of the council of a municipality shall be consistent with PPS. (PPS 4.1)</p> <p>The Official Plan is the most important vehicle for implementation of the Provincial Policy Statement (PPS 4.6)</p>	<p>Settlement areas shall be the focus of growth and development. (PPS 1.1.3.1)</p> <p>Land use patterns within settlement areas will achieve densities and a mix of uses that efficiently use land, resources, infrastructure, public service facilities and transit. (PPS 1.1.3.2.a)</p> <p>Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment. (PPS 1.1.3.3)</p> <p>Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected needs of current and future residents of the regional market area. (PPS 1.4.3)</p> <p>Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified. New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities. (PPS 1.6.8.3)</p>
<b>Growth Plan for the Greater Golden Horseshoe (Growth Plan)</b>	<p>The Growth Plan applies to the area designated as the Greater Golden Horseshoe growth plan area. All decisions made on or after May 16, 2019 in respect of the exercise of any authority that affects a</p>	<p>Within settlement areas, growth will be focused in delineated built-up areas; strategic growth areas; locations with existing or planned transit; and, areas with existing or planned public service facilities. (Growth Plan 2.2.1.2 c)</p>



Policy Document	Legislative Authority/Applicability	Key Policies
	<p>planning matter will conform with this Plan, subject to any legislative or regulatory provisions providing otherwise. (Growth Plan 1.2.2)</p>	<p>Complete communities will feature a diverse mix of land uses; improve social equity and quality of life; provide a range and mix of housing options; provide convenient access to a range of transportation options, public service facilities, open spaces and parks, and healthy, local and affordable food options; provide a more compact built form; mitigate and adapt to climate change impacts; and, integrate green infrastructure. (Growth Plan 2.2.1.4)</p> <p>To achieve minimum intensification and density targets, municipalities will develop and implement urban design and site design official plan policies and other supporting documents that direct the development of high quality public realm and compact built form. (Growth Plan 5.2.5.6)</p>
<p><b>Region of Peel Official Plan (ROP)</b></p>	<p>The Region of Peel approved MOP on September 22, 2011, which is the primary instrument used to evaluate development applications. The proposed development applications were circulated to the Region who has advised that in its current state, the applications meet the requirements for exemption from Regional approval. Local official plan amendments are generally exempt from approval where they have had regard for the <i>Provincial Policy Statement</i> and applicable Provincial Plans, where the City Clerk has certified that processing was completed in accordance with the <i>Planning Act</i> and where the Region has advised that no Regional official plan amendment is required to accommodate the local official plan amendment. The Region provided additional comments which are discussed in Section 8 of this Appendix.</p>	<p>The ROP identifies the subject lands as being located within Peel's Urban System.</p> <p>General objectives of ROP, as outlined in Section 5.3, include conserving the environment, achieving sustainable development, establishing healthy complete communities, achieving intensified and compact form and mix of land uses in appropriate areas that efficiently use land, services, infrastructure and public finances, while taking into account the characteristics of existing communities and services, and achieving an urban form and densities that are pedestrian-friendly and transit supportive.</p>

### **Mississauga Official Plan**

The policies of MOP implement provincial directions for growth. MOP is generally consistent with the PPS and conforms with the Growth Plan, Greenbelt Plan, PBWP and ROP. An update to MOP is currently underway to ensure MOP is consistent with and conform to changes resulting from the recently released Growth Plan, 2019 and Amendment No. 1 (2020).

### **Existing Designation**

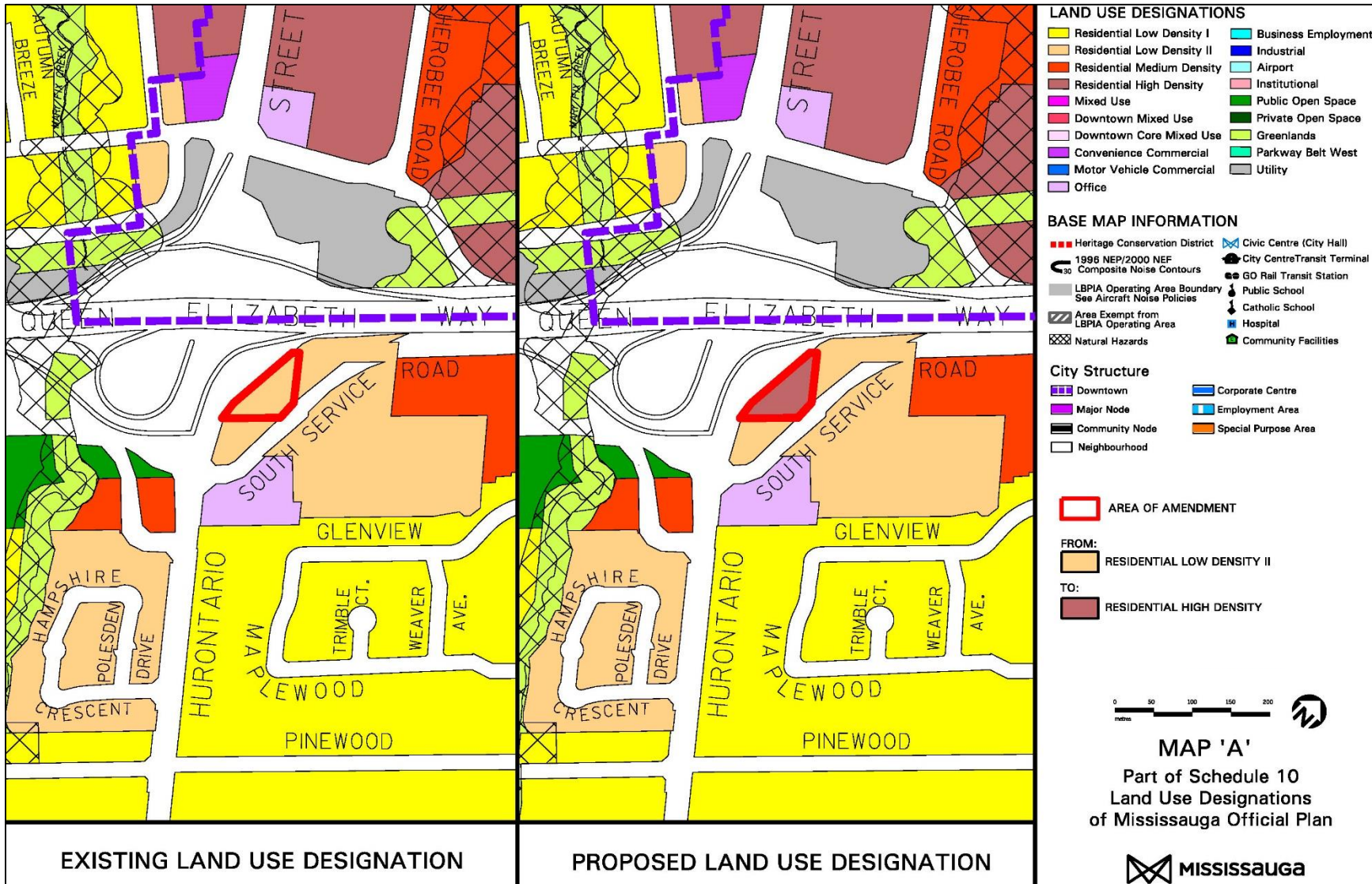
The property is located within the Mineola Neighbourhood Character Area and is designated **Residential Low Density II**. The **Residential Low Density II** designation permits only detached dwellings.

The property is located within the Mineola Major Transit Station Area (MTSA) associated with the Hazel McCallion LRT.

### **Proposed Designation**

The applicant is proposing to change the designation to **Residential High Density** to permit the 26 storey apartment building containing 352 units. The applicant will need to demonstrate consistency with the intent of MOP and shall have regards for the appropriateness of the proposed built form in terms of compatibility with the surrounding context and character of the area.

Through the processing of the applications, staff may recommend a more appropriate designation to reflect the proposed development in the Recommendation Report.



Excerpt of Mineola Neighbourhood Character Area

### Relevant Mississauga Official Plan Policies

The following policies are applicable in the review of these applications. In some cases the description of the general intent summarizes multiple policies.

	<b>General Intent</b>
<p><b>Chapter 5 Direct Growth</b></p>	<p>Most of Mississauga's future growth will be directed to Intensification Areas. (Section 5.1.4)</p> <p>Mississauga encourages compact, mixed use development that is transit supportive, in appropriate locations, to provide a range of local live/work opportunities. (Section 5.1.6)</p> <p>Mississauga will protect and conserve the character of stable residential Neighbourhoods. (Section 5.1.7)</p> <p>Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved. (Section 5.3.5.1)</p> <p>Residential intensification within Neighbourhoods will generally occur through infilling and the development of existing commercial sites as mixed use areas. (Section 5.3.5.2)</p> <p>Where higher density uses are proposed, they should be located on sites identified by a local area review, along Corridors or in conjunction with existing apartment sites or commercial centres. (Section 5.3.5.3)</p> <p>Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan. (Section 5.3.5.5)</p> <p>Development will be sensitive to the existing and planned context and will include appropriate transition in use, built form, density and scale. (Section 5.3.5.6)</p> <p>Development on Corridors should be compact, mixed use and transit friendly and appropriate to the context of the surrounding Neighbourhood. (Section 5.4.4)</p> <p>Where higher density uses within Neighbourhoods are directed to Corridors, development will be required to have regard for the character of the Neighbourhoods and provide appropriate transitions in height, built form and density to the surrounding lands. (Section 5.4.5)</p> <p>Hurontario Street and Dundas Street have been identified as Intensification Corridors. These are Intensification Areas. Additional Intensification Corridors may be identified in the future. (Section 5.4.11)</p> <p>Not all segments of Intensification Corridors are appropriate for intensification. Planning studies for Intensification Corridors will identify appropriate locations for intensification and the appropriate densities, land uses and building heights. (Section 5.4.12)</p>



	<b>General Intent</b>
	<p>Low density residential development will be discouraged from locating within Intensification Corridors. (Section 5.4.13)</p> <p>The Hurontario Street Intensification Corridor is comprised of lands along Hurontario Street from Lake Ontario in Port Credit to the city's limit to the north as shown on Map 5-1: Hurontario Street Intensification Corridor. (Section 5.4.14)</p> <p>A number of Light Rail Transit Stations, which will be located along the Hurontario Street Intensification Corridor to serve the proposed light rail transit system are a form of Major Transit Station Areas. The Major Transit Station Areas are identified on Schedule 2: Intensification Areas and the Light Rail Transit Stations are identified on Schedule 6: Long Term Transit Network. (Section 5.4.15)</p> <p>The focus for intensification will be Intensification Areas, which are the Downtown, Major Nodes, Community Nodes, Corporate Centres, Intensification Corridors and Major Transit Station Areas, as shown on Schedule 2: Intensification Areas. (Section 5.5.1)</p> <p>Planning studies will delineate the boundaries of Intensification Corridors and Major Transit Station Areas and identify appropriate densities, land uses and building heights. (Section 5.5.3)</p> <p>Intensification Areas will be planned to reflect their role in the City Structure hierarchy. (Section 5.5.4)</p> <p>Residential and employment density should be sufficiently high to support transit usage. Low density development will be discouraged. (Section 5.5.8)</p> <p>Intensification Areas will be planned to maximize the use of existing and planned infrastructure. (Section 5.5.9)</p> <p>Pedestrian movement and access from major transit routes will be a priority in Intensification Areas. (Section 5.5.14)</p> <p>Intensification Areas will be served by transportation Corridors containing transit and active transportation and may contain higher order transit facilities. (Section 5.5.15)</p> <p>Major Transit Station Areas will be planned and designed to provide access from various transportation modes to the transit facility, including consideration of pedestrians, bicycle parking and commuter pick-up/drop-off areas. (Section 5.5.16)</p>
<b>Chapter 6 Value The Environment</b>	<p>Mississauga will require that development applications be supported by stormwater best management practices in accordance with relevant plans, studies, development standards and policies. Additional measures may be specified by the City based on known concerns related to storm sewer capacity, pollution prevention, flood risk and erosion, and protection of the city's Natural Heritage System, including its ecological function. Stormwater best management practices must be approved by the city, appropriate conservation authority and Provincial Government, where applicable. (Section 6.4.2.2)</p> <p>Appropriate techniques to mitigate odour and dust will be encouraged to be incorporated in development. (Section 6.5.4)</p> <p>When determining land use compatibility, regard will be given to odours, air particulates, noise and other contaminants, which may impact adjacent or nearby land uses and natural areas. Incompatible land uses such as sensitive land uses and those uses that are sources of noise, odour and dust will be separated and/or the nuisances will be mitigated, so they do not interfere with each other. (Section 6.5.5)</p>

	<p><b>General Intent</b></p> <p>Where residential and other land uses sensitive to noise are proposed in close proximity to Provincial Highways, it may be necessary to mitigate noise impact, in part, by way of building and site design. A feasibility noise impact study will be submitted prior to approval in principle of such land uses located within 50 m of arterial and major collector rights-of-way and within 100 m of a Provincial Highway right-of-way, or as required by the City or Region. (Section 6.10.3.1)</p> <p>Residential development or development that includes outdoor living areas will not be permitted in locations where the mitigated outdoor noise levels are forecast to exceed limits specified by the applicable Provincial Government environmental noise guideline. A detailed noise impact study will be required to demonstrate that every effort has been made to achieve the sound level limits specified by the applicable Provincial Government environmental noise guideline, for an outdoor living area (55 dBA or less). Only in cases where the required noise attenuation measures are not feasible for technical, economic, aesthetic or administrative reasons would excess noise above the limit (55 dBA) be acceptable, with a warning clause to prospective purchasers, consistent with the applicable Provincial Government environmental noise guideline. In these situations, any excess noise above the limit will not be acceptable if it exceeds 60 dBA. (Section 6.10.3.2)</p> <p>Development with a residential component such as dwellings, or any development which includes bedrooms, sleeping quarters, living rooms or reading rooms which will be subject to high levels of traffic noise, will only be permitted if it includes structural features which result in interior noise levels that comply with the indoor standards specified by the applicable Provincial Government environmental noise guideline. (Section 6.10.3.3)</p> <p>Where residential and other land uses sensitive to noise are proposed within 500 m of a freeway, 250 m of a provincial highway or 100 m from other roads, development proponents will be required to submit detailed noise studies delineating mitigative noise measures required to meet Provincial Government and Region of Peel noise guidelines. The recommendations of the approved reports are to be implemented as conditions of development. (Section 6.10.3.4)</p>
<p><b>Chapter 7 Complete Communities</b></p>	<p>Mississauga will ensure that the housing mix can accommodate people with diverse housing preferences and socioeconomic characteristics and needs. (Section 7.1.6)</p> <p>Mississauga will ensure that housing is provided in a manner that maximizes the use of community infrastructure and engineering services, while meeting the housing needs and preferences of Mississauga residents. (Section 7.2.1)</p> <p>Mississauga will provide opportunities for:</p> <ol style="list-style-type: none"> <li>a. the development of a range of housing choices in terms of type, tenure and price;</li> <li>b. the production of a variety of affordable dwelling types for both the ownership and rental markets; and</li> <li>c. the production of housing for those with special needs, such as housing for the elderly and shelters. (Section 7.2.2)</li> </ol> <p>When making planning decisions, Mississauga will ensure that housing is provided in a manner that fully implements the intent of the Provincial and Regional housing policies. (Section 7.2.3)</p>
<p><b>Chapter 9 Build A Desirable Urban Form</b></p>	<p>Mississauga will develop an urban form based on the urban system and the hierarchy identified in the city structure as shown on Schedule 1: Urban System. (Section 9.1.1)</p>

	<b>General Intent</b>
	<p>Infill and redevelopment within Neighbourhoods will respect the existing and planned character. (Section 9.1.3)</p> <p>Development on Corridors will be consistent with existing or planned character, seek opportunities to enhance the Corridor and provide appropriate transitions to neighboring uses. (Section 9.1.5)</p> <p>Built form should provide for the creation of a sense of place through, among other matters, distinctive architecture, streetscaping, public art and cultural heritage recognition. (Section 9.2.1.3)</p> <p>Mississauga will encourage a high quality, compact and urban built form to reduce the impact of extensive parking areas, enhance pedestrian circulation, complement adjacent uses, and distinguish the significance of the Intensification Areas from surrounding areas. (Section 9.2.1.4)</p> <p>Mississauga will encourage the consolidation of access points and shared parking, service areas and driveway entrances. (Section 9.2.1.6)</p> <p>The preferred location of tall buildings will be in proximity to existing and planned Major Transit Station Areas. (Section 9.2.1.8)</p> <p>Appropriate height and built form transitions will be required between sites and their surrounding areas. (Section 9.2.1.10)</p> <p>Tall buildings will be appropriately spaced to provide privacy and permit light and sky views. (Section 9.2.1.13)</p> <p>In appropriate locations, tall buildings will be required to incorporate podiums to mitigate wind impacts on the pedestrian environment and maximize sunlight on the public realm. (Section 9.2.1.14)</p> <p>Tall buildings will minimize adverse microclimatic impacts on the public realm and private amenity areas.(Section 9.2.1.16)</p> <p>Development will create a sense of gateway to the Intensification Area with prominent built form and landscaping. (Section 9.2.1.27)</p> <p>Buildings and streetscapes will be situated and designed so as to encourage pedestrian circulation. (Section 9.2.1.35)</p> <p>Neighbourhoods are stable areas where limited growth is anticipated. Where increases in density and a variety of land uses are considered in Neighbourhoods, they will be directed to Corridors. Appropriate transitions to adjoining areas that respect variations in scale, massing and land uses will be required. (Section 9.2.2)</p> <p>Heights in excess of four storeys will be required to demonstrate that an appropriate transition in height and built form that respects the surrounding context will be achieved. (Section 9.2.2.1)</p> <p>Tall buildings will generally not be permitted in Non-Intensification Areas. (Section 9.2.2.3)</p> <p>While new development need not mirror existing development, new development in Neighbourhoods will:</p> <ol style="list-style-type: none"> <li>a. Respect existing lotting patterns;</li> <li>b. Respect the continuity of front, rear and side yard setbacks;</li> </ol>

	<b>General Intent</b>
	<ul style="list-style-type: none"> <li>c. Respect the scale and character of the surrounding area;</li> <li>d. Minimize overshadowing and overlook on adjacent neighbours;</li> <li>e. Incorporate stormwater best management practices;</li> <li>f. Preserve mature high quality trees and ensure replacement of the tree canopy; and</li> <li>g. Be designed to respect the existing scale, massing, character and grades of the surrounding area. (Section 9.2.2.3)</li> </ul> <p>Development on Corridors will be encouraged to:</p> <ul style="list-style-type: none"> <li>a. Assemble small land parcels to create efficient development parcels;</li> <li>b. Face the street, except where predominate development patterns dictate otherwise;</li> <li>c. Not locate parking between the building and the street;</li> <li>d. Site buildings to frame the street;</li> <li>f. Support transit and active transportation modes;</li> <li>h. Provide concept plans that show how the site can be developed with surrounding lands. (Section 9.2.2.6)</li> </ul> <p>Private open space and/or amenity areas will be required for all development. (Section 9.3.5.5.)</p> <p>Residential developments of significant size, except for freehold developments, will be required to provide common outdoor on-site amenity areas that are suitable for the intended users. (Section 9.3.5.6)</p> <p>Residential developments will provide at grade amenity areas that are located and designed for physical comfort and safety. In Intensification Areas, alternatives to at grade amenities may be considered. (Section 9.3.5.7)</p> <p>The design of all development will foster the improvement of connections and accessibility for transit users and promote active transportation modes. (Section 9.4.1.1)</p> <p>Developments will provide a transition in building height and form between Intensification Areas and adjacent Neighbourhoods with lower density and heights. (Section 9.5.1.5)</p> <p>Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by ensuring adequate privacy, sunlight and sky views are maintained. (Section 9.5.1.9)</p> <p>Noise will be mitigated through appropriate built form and site design. Mitigation techniques such as fencing and berms will be discouraged. (Section 9.5.1.12)</p> <p>Buildings with exposure to Provincial Highways or public streets in areas of site plan control will be subject to a higher standard of design to achieve upgraded building elevations and landscaping, including principal doors and window fenestration. (Section 9.5.1.13)</p> <p>Development proponents may be required to upgrade the public boulevard and contribute to the quality and character of streets and open spaces by providing:</p> <ul style="list-style-type: none"> <li>a. street trees and landscaping, and relocating utilities, if required; [...] (Section 9.5.2.5)</li> </ul> <p>Tall buildings will minimize undue physical and visual negative impact relating to:</p>



	<p><b>General Intent</b></p> <ul style="list-style-type: none"> <li>a. microclimatic conditions, including sun, shadow and wind;</li> <li>b. noise;</li> <li>c. views;</li> <li>d. skyview;[...] (Section 9.5.3.9)</li> </ul> <p>Service, loading and garbage storage areas should be internal to the building or located at the rear of the building and screened from the public realm. (Section 9.5.5.7)</p>
<p><b>Chapter 11          General Land Use          Designations</b></p>	<p>Lands designated Residential Low Density II will permit the following uses:</p> <ul style="list-style-type: none"> <li>a. detached dwelling;</li> <li>b. semi-detached dwelling;</li> <li>c. duplex dwelling; and</li> <li>d. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages. (Section 11.2.5.4)</li> </ul> <p>Lands designated Residential High Density will permit the following use:</p> <ul style="list-style-type: none"> <li>a. apartment dwelling;</li> <li>b. uses permitted in the Residential Medium Density designation, accessory to apartment dwellings on the same property; and</li> <li>c. uses permitted in the Convenience Commercial designation are permitted at grade in apartment dwellings, except for commercial parking facilities, gas bars, and drive-through facilities. (Section 11.2.5.6)</li> </ul>
<p><b>Chapter 16          Neighbourhoods</b></p>	<p>For lands within Neighbourhoods, a maximum building height of four storeys will apply unless Character Area policies specify alternative building height requirements. (Section 16.1.1.1)</p> <p>Proposal for heights more than four storeys or different than established in the Character Area policies, will only be considered where it can be demonstrated to the City's satisfaction, that:</p> <ul style="list-style-type: none"> <li>a. an appropriate transition in heights that respects the surrounding context will be achieved;</li> <li>b. the development proposal enhances the existing or planned development;</li> <li>c. the City Structure hierarchy is maintained; and</li> <li>d. the development proposal is consistent with the policies of this Plan.</li> </ul> <p>On lands adjacent to Hurontario Street, the existing mature vegetation, well landscaped appearance and generous setbacks will be maintained to reflect area character. As Hurontario Street is a gateway to the Character Area, as well as to Port Credit, consideration should be given to: additional tree planting, a sodded boulevard, a bicycle route and a right-of-way design that is sympathetic to the character of the area. (Section 16.18.1.2)</p> <p>Notwithstanding the Residential Low Density I and Residential Low Density II policies of this Plan, the Residential Low Density I and Residential Low Density II designations permit only detached dwellings. (Section 16.18.2)</p>

	<b>General Intent</b>
<b>Chapter 19 Implementation</b>	<p>City Council will consider applications for site specific amendments to this Plan within the context of the policies and criteria set out throughout this Plan. The proponent of an official plan amendment will be required to submit satisfactory reports to demonstrate the rationale for the amendment; including, among other matters:</p> <ol style="list-style-type: none"> <li>a. that the proposed redesignation would not adversely impact or destabilize the following:                             <ul style="list-style-type: none"> <li>• the achievement of the overall intent, goals, objectives, and policies of this Plan; and</li> <li>• the development or functioning of the remaining lands that have the same designation, or neighbouring lands; and</li> </ul> </li> <li>b. that a municipal comprehensive review of land use designations or a five year review is not required;</li> <li>c. that the lands are suitable for the proposed use, and a planning rationale with reference to the policies of this Plan, other applicable policies, and sound planning principles is provided, setting out the merits of the proposed amendment in comparison with the existing designation;</li> <li>d. land use compatibility with the existing and future uses of surrounding lands; and</li> <li>e. the adequacy of engineering services, community infrastructure and multi-modal transportation systems to support the proposed application.</li> </ol> <p>Mississauga may apply a holding provision to lands within Intensification Areas to ensure that the policies of this Plan are implemented. The removal of the holding provision will be conditional on the applicant satisfying the requirements of the policies of this Plan. (Section 19.7.4)</p> <p>Mississauga may require development proponents to submit their application to the Design Review Panel. (Section 19.17.3)</p>

**Mississauga Zoning By-law**

**Existing Zoning**

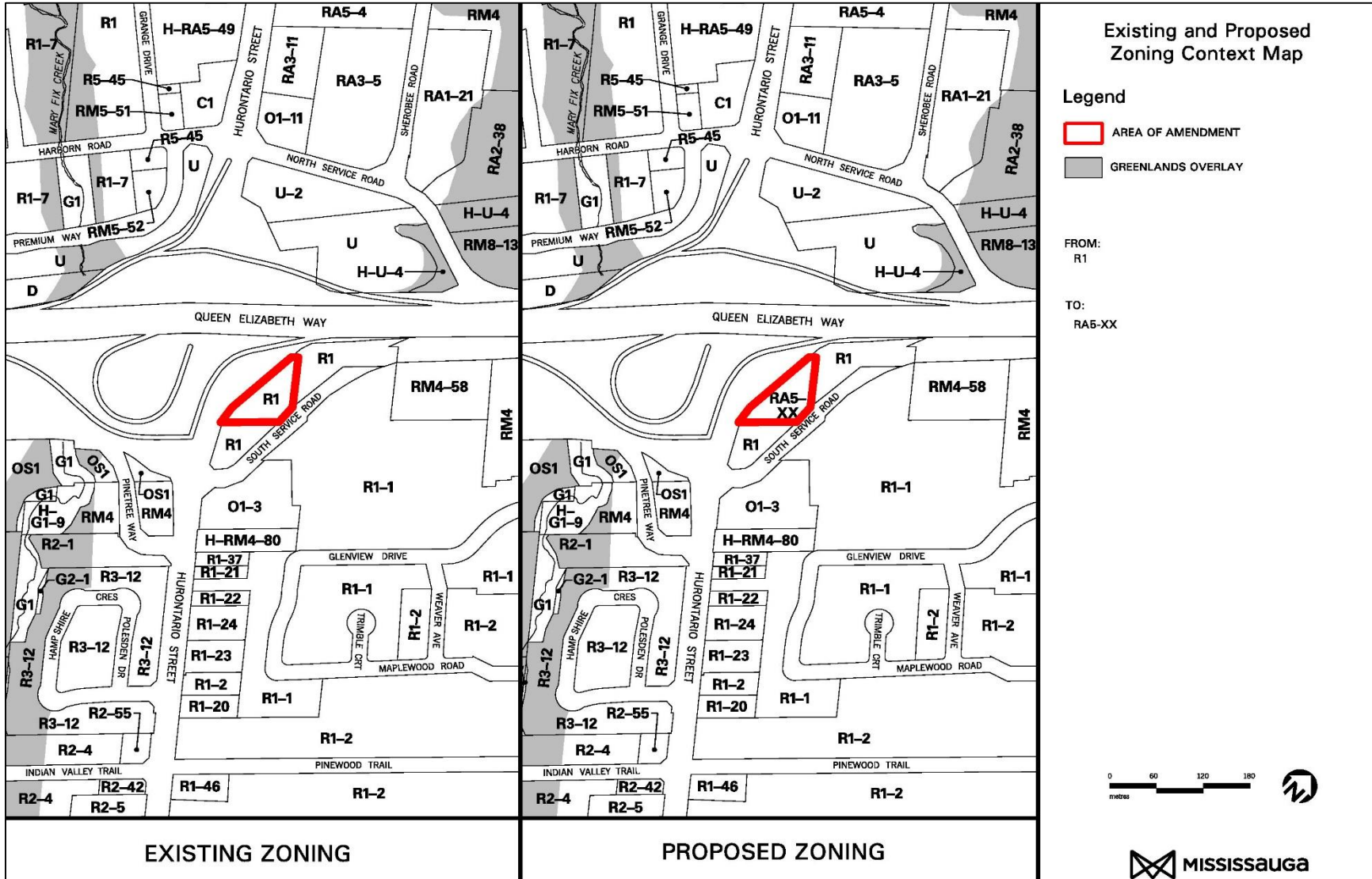
The property is currently zoned **R1** (Detached Dwellings – Typical Lots) which only permits detached dwellings.

**Proposed Zoning**

The applicant is proposing to zone the property **RA5-Exception**

(Apartments – Exception) to permit the 26 storey apartment building containing 352 units.

Through the processing of the applications staff may recommend a more appropriate zone category for the development in the Recommendation Report.



Excerpt of Zoning Map 7

### Proposed Zoning Regulations

Zone Regulations	Existing R1 Zone Regulations	Proposed RA5 Base Zone Regulations	Proposed RA5-Exception Zone Regulations
Maximum <b>Floor Space Index (FSI)</b>	-	2.9	4.9
Maximum <b>Height</b>	10.7 m (35.1 ft.)	77.0 m (252.6 ft.) / 25 storeys	91.0 m (298.6 ft.) / 26 storeys (inclusive of mechanical penthouse)
Minimum <b>Interior Side Yard</b>  <b>Corner Lot</b>  For that portion of the dwelling with a height greater than 13.0 m (42.7 ft.) and less than or equal to 20.0 m (65.6 ft.)  For that portion of the dwelling with a height greater than 20.0 m (65.6 ft.) and less than or equal to 26.0 m (85.3 ft.)  For that portion of the dwelling with a height greater than 26.0 m (85.3 ft.)	3.0 m (9.8 ft.)  -  -  -	6.0 m (19.7 ft.)  7.5 m (24.6 ft.)  9.0 m (29.5 ft.)	5.0 m (16.4 ft.)
Minimum <b>Parking Spaces</b>	-	0.9 resident spaces per unit – 317 spaces 0.2 visitor spaces per unit – 70 spaces	0.29 resident spaces per unit – 102 spaces 0.1 visitor spaces per unit – 35 spaces
Minimum Setback From A <b>Parking Structure</b> Completely Below Finished Grade, Inclusive Of External Access Stairwells, To Any <b>Lot</b>	-	3.0 m (9.8 ft.)	1.0 m (3.3 ft.)



Zone Regulations	Existing R1 Zone Regulations	Proposed RA5 Base Zone Regulations	Proposed RA5-Exception Zone Regulations
<b>Line</b>			
Minimum Setback From A Waste Enclosure/Loading Area To A <b>Street Line</b>	-	10.0 m (32.8 ft.)	0.0 m (0.0 ft.)
Minimum Depth Of A <b>Landscaped Buffer</b> Abutting A <b>Lot Line</b> That Is A <b>Street Line</b> And/Or Abutting Lands With An Open Space, Greenlands And/Or A Residential Zone With The Exception Of An <b>Apartment Zone</b>	-	4.5 m (14.8 ft.)	0.0 m (0.0 ft.)
Minimum Depth Of A <b>Landscaped Buffer</b> Along Any Other <b>Lot Line</b>	-	3.0 m (9.8 ft.)	1.5 m (4.9 ft.)
Minimum <b>Amenity Area</b>	-	The greater of 5.6 m <sup>2</sup> (60.3 ft <sup>2</sup> ) per <b>dwelling unit</b> or 10% of the site area	3.5 m <sup>2</sup> (37.7 ft <sup>2</sup> ) per <b>dwelling unit</b>
Minimum Percentage Of Total Required <b>Amenity Area</b> To Be Provided In One Contiguous Area	-	50% (985.6 m <sup>2</sup> (10,608.9 ft <sup>2</sup> ))	46% (902.0 m <sup>2</sup> (9,709.1 ft <sup>2</sup> ))
	<p>Note: The provisions listed are based on information provided by the applicant, which is subject to revisions as the applications are further refined. In addition to the regulations listed, other minor and technical variations to the implementing by-law may also apply, including changes that may take place before Council adoption of the by-law, should the application be approved.</p>		

## Affordable Housing

In October 2017 City Council approved *Making Room for the Middle – A Housing Strategy for Mississauga* which identified housing affordability issues for low and moderate incomes in the City. In accordance with the Provincial Growth Plan (2019) and Amendment No. 1 (2020), *Provincial Policy Statement (2020)*, Regional Official Plan and Mississauga Official Plan (MOP), the City requests that proposed multi-unit residential developments incorporate a mix of units to accommodate a diverse range of incomes and household sizes.

Applicants proposing non-rental residential developments of 50 units or more requiring an official plan amendment or rezoning for additional height and/or density beyond as-of-right permissions will be required to demonstrate how the proposed development is consistent with/conforms to Provincial, Regional and City housing policies. The Official Plan indicates that the City will provide opportunities for the provision of a mix of housing types, tenures and at varying price points to accommodate households. The City's annual housing targets by type are contained in the Region of Peel Housing and Homelessness Plan 2018 - 2028 <https://www.peelregion.ca/housing/housinghomelessness/pdf/plan-2018-2028.pdf>.

To achieve these targets, the City is requesting that a minimum of 10% of new ownership units be affordable. The 10% contribution rate will not be applied to the first 50 units of a development. The contribution may be in the form of on-site or

off-site units, land dedication, or financial contributions to affordable housing elsewhere in the City.

## 5. School Accommodation

### The Peel District School Board

Student Yield	School Accommodation			
33 Kindergarten to Grade 6 8 Grade 7 to Grade 8 17 Grade 9 to Grade 12	Forest Avenue Public School	Mineola Public School	Queen Elizabeth Sr. Public School	Port Credit Secondary School
	Enrolment: 191 Capacity: 199 Portables: 0	Enrolment: 309 Capacity: 429 Portables: 0	Enrolment: 300 Capacity: 262 Portables: 2	Enrolment: 1,314 Capacity: 1,203 Portables: 7

### The Dufferin-Peel Catholic District School Board

Student Yield	School Accommodation	
6 Kindergarten to Grade 8 5 Grade 9 to Grade 12	St. Dominic Elementary School	St. Paul Catholic Secondary School
	Enrolment: 273 Capacity: 271 Portables: 4	Enrolment: 726 Capacity: 807 Portables: 0

## 6. Community Questions and Comments

A community meeting was held by Ward 1 Councillor, Stephen Dasko, on February 7, 2023. 32 people attended the community meeting and one written submission has been received.

The following comments made by the community as well as any others raised at the public meeting will be addressed in the Recommendation Report, which will come at a later date.

- Additional underground parking should be provided
- Family sized units in the form of two-bedroom plus den and three-bedroom units should be incorporated into the unit mix
- The development should provide affordable units to accommodate a diverse range of incomes
- Balconies should be removed given the development's proximity to the QEW
- A connection to the existing multi-use trail east of the property should be explored to ensure the safety of pedestrians and cyclists

## 7. Development Issues

The following is a summary of comments from agencies and departments regarding the applications:

Agency / Comment Date	Comments
Arborist – Private Property (December 2022)	The applicant is advised that tree removal permission is required as per the Private Tree Protection By-law 21-22 to remove one or more trees 15 cm (0.5 ft.) diameter at breast height or greater per lot per calendar year. The tree removal application will be reviewed in conjunction with the future site plan application. The approval of the tree removal permit is required prior to the earliest of the demolition permit, erosion and sediment control permit or site plan approval.
Region of Peel (December 2022)	<p>An existing 300 mm (1 ft.) diameter watermain is located on South Service Road. The proposal requires connection to a minimum municipal watermain size of 300 mm (1 ft.). An existing 250 mm (0.8 ft.) diameter sanitary sewer is located on South Service Road. All unutilized water and sanitary services shall be disconnected and/or abandoned in accordance with Region of Peel standards and specifications.</p> <p>Prior to approval, confirmation that the City will permit shared servicing, if applicable, is required. Regional site servicing connection approvals are required prior to the City issuing full building permit.</p> <p>Servicing of the site may require municipal and/or private easements and the construction, extension, twinning and/or upgrading of municipal services. All works associated with the servicing of this site will be at the applicant's expense. The applicant will also be responsible for the payment of applicable fees, development charges, legal costs and all other costs associated with the development of this site.</p>
Dufferin-Peel Catholic District School Board (November 2022)	<p>Based on the Dufferin-Peel Catholic District School Board's School Accommodation Criteria, the Board is satisfied with the current provision of educational facilities for the catchment area in which the subject application is located. The City of Mississauga school accommodation condition need not be applied.</p> <p>The applicant shall agree in the Development and/or Subdivision Agreement to include the following warning clauses in all offers of purchase and sale of residential lots:</p> <ul style="list-style-type: none"> <li>• Whereas, despite the best efforts of the Dufferin-Peel Catholic District School Board, sufficient accommodation may not be available for all anticipated students from the area, you are hereby notified that students may be accommodated in temporary facilities and/or bussed to a school outside of the neighbourhood, and further, that students may later be transferred to the neighbourhood school.</li> <li>• That the purchasers agree that for the purpose of transportation to school, the residents of the subdivision shall agree that children will meet the bus on roads presently in existence or at another place designated by the Board.</li> </ul>
Peel District School Board	The applicant shall agree to include the following warning clauses be placed in any agreement of purchase and sale and

Agency / Comment Date	Comments
(December 2022)	<p>entered into with respect to any lots on this plan, within a period of five years from the date of registration of the development agreement:</p> <ul style="list-style-type: none"> <li>• Whereas, despite the efforts of the Peel District School Board, sufficient accommodation may not be available for all anticipated students in the neighbourhood schools, you are hereby notified that some students may be accommodated in temporary facilities or bused to schools outside of the area, according to the Board's Transportation Policy. You are advised to contact the School Accommodation department of the Peel District School Board to determine the exact schools.</li> <li>• The purchaser agrees that for the purposes of transportation to school the residents of the development shall agree that the children will meet the school bus on roads presently in existence or at another designated place convenient to the Board.</li> </ul> <p>The developer shall agree to erect and maintain signs at the entrances to the development which shall advise prospective purchasers that due to present school facilities, some of the children from the development may have to be accommodated in temporary facilities or bused to schools, according to the Peel District Board's Transportation Policy.</p>
<p>City Community Services            Department – Park Planning            Section            (February 2023)</p>	<p>The property is located on the north side of South Service Road abutting the QEW and within the Mineola Neighbourhood Character Area.</p> <p>The 2022 Parks Plan notes that the Mineola Neighbourhood Character Area meets the minimum parkland provision requirement of 1.2 ha (3 ac.) of parkland for 1,000 people. In addition to the minimum parkland provision, the Parks Plan and Future Directions Master Plan also establish a maximum walking distance of 800 m (2,624.7 ft.) to a City owned Playground. Spruce Park (P-029), zoned <b>OS1</b>, is located approximately 2,164 m (7,099.7 ft.) from the property and contains a playground. Although the park does not meet the 800 m (2,624.7 ft.) walking distance requirement, it still provides some of the parkland needs for future residents. The requirement for a parkland dedication has not been imposed on this development given the site constraint to achieve an unencumbered parkland.</p> <p>Furthermore, it is noted, prior to the issuance of building permits for each lot or block cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the Planning Act (R.S.O. 1990, c.P. 13, as amended) and in accordance with approved City's Policies and By-laws.</p>
<p>City Planning Strategies –            Housing            (January 2023)</p>	<p>Additional information is required on the provision of affordable housing options. The applicant is strongly encouraged to contribute to the range and affordability of housing options. Options include middle-income affordable ownership units, affordable rental units, off-site land contributions, or financial contributions to affordable middle income housing elsewhere.</p>
<p>City Transportation and Works            Department            (January 2023)</p>	<p>Technical reports and drawings have been submitted and are under review to ensure that engineering matters related to noise, grading, servicing, stormwater management, traffic and environmental compliance can be satisfactorily addressed to confirm the feasibility of the project, in accordance with City requirements.</p> <p>Based on a review of the materials submitted to date, the owner has been requested to provide additional technical details</p>



Agency / Comment Date	Comments
	<p>and revisions prior to the City making a recommendation on the application, as follows:</p> <p><b><u>Stormwater</u></b></p> <p>A Stormwater Management Report prepared by WSP Canada Inc. dated October 2022, was submitted in support of the proposed development. The purpose of the report is to evaluate the proposed development impact on the municipal drainage system (e.g. storm sewers, watercourses, etc.) and to mitigate the quality and quantity impacts of stormwater run-off generated from the site. Mitigation measures may include new infrastructure and/or on-site stormwater management controls.</p> <p>The applicant is proposing to construct a private storm sewer system to service the development lands, with a connection to the City of Mississauga's stormwater infrastructure on South Service Road, as well as on-site stormwater management controls for the post-development discharge.</p> <p>The applicant is required to provide further technical information to:</p> <ul style="list-style-type: none"> <li>• demonstrate the feasibility of the proposed storm sewer system;</li> <li>• demonstrate that the 5 mm (0.02 ft.) water balance through Low Impact Development (LID) will be achieved; and</li> <li>• demonstrate that there will be no impact on the existing drainage system including how groundwater will be managed on-site</li> </ul> <p><b><u>Traffic</u></b></p> <p>A Traffic Impact Study prepared by GHD Limited dated October 2022, was submitted in support of the proposed development. A full review and audit was completed by Transportation and Works staff. Based on the information provided to date, staff are not satisfied with the study and require further clarification on the information provided.</p> <p>The applicant is required to provide the following information as part of subsequent submissions:</p> <ul style="list-style-type: none"> <li>• an updated Traffic Impact Study addressing all staff comments;</li> <li>• a review of the driveway access to ensure both South Service Road and the internal driveway can operate efficiently;</li> <li>• turning movement diagrams to evaluate the internal site circulation and access points;</li> <li>• obtain Ministry of Transportation (MTO) approval as South Service Road is under their jurisdiction; and,</li> <li>• address any traffic concerns from the Community related to the proposed development</li> </ul> <p><b><u>Environmental Compliance</u></b></p> <p>Phase One Environmental Site Assessment (ESA) and Phase Two ESA reports prepared by Palmer Environmental Consulting Group Inc. dated October 2022, were submitted in support of the proposed development. The reports indicate that no further environmental investigation is required. However, the following documents are to be submitted for further review:</p>

Agency / Comment Date	Comments
	<ul style="list-style-type: none"> <li>• a written document prepared by a Professional Engineer that includes a plan to decommission the wells and aboveground storage tanks, or proof of decommissioning;</li> <li>• a written documentation prepared by a Qualified Person (as defined in Section 5 of Ontario Regulation 153/04, as amended) stating the geotechnical and environmental suitability of the fill materials located on-site; and,</li> <li>• a completed Storm Sewer Use By-law Acknowledgement form.</li> </ul> <p>As the land use is changing from a less sensitive to a more sensitive use, a Record of Site Condition (RSC) is required to be filed in accordance with O. Reg. 153/04 prior to enactment of the rezoning by-law. A copy of the RSC and all supporting documentation must be provided to the City once it has been acknowledged by the Ontario Ministry of the Environment, Conservation and Parks</p> <p><b><u>Noise</u></b></p> <p>An Environmental Noise &amp; Vibration Assessment prepared by J.E. Coulter associates Limited dated October 2022 was received for review. The study evaluates the potential impact to and from the development, and recommends mitigation measures to reduce any negative impacts. Noise sources that may have an impact on this development include road traffic from the QEW and Hurontario Street including the nearby LRT service. Noise mitigation will be required and a revised study is required as part of the next submission to address staff comments.</p> <p><b><u>Engineering Plans/Drawings</u></b></p> <p>The applicant has submitted a number of technical plans and drawings, which are to be revised as part of subsequent submissions, in accordance with City standards.</p>
Heritage Planning (November 2022)	<p>The property has archaeological potential due to its proximity to a present or past watercourse or known archaeological resource. No grading or other soil disturbances shall take place on the subject property prior to the approval authority and the Ministry of Citizenship and Multiculturalism confirming that all archaeological resource concerns have met licensing and resource conservation requirements. Letters to this effect from said Ministry corresponding to each archaeological assessment report and activity are required to be submitted to the Culture Division for review.</p>
Ministry of Transportation (January 2023)	<p>The site is within the Ministry's permit control area. A building and land use permit is required prior to construction. Any signs visible from the highway require a sign permit.</p> <p>Additional information is required regarding the Traffic Impact Study.</p> <p>All buildings, structures, essential site features, storm water management facilities, including ponds and associated berms, storages, and noise walls should be setback a minimum of 14 m (45.9 ft.) from the Ministry's right-of-way. If any part of the development is integral to the operation of the site (i.e. compliance with zoning by-law standards, conditions of site plan approval including minimum parking requirements, fire lanes, drive-through lanes, delivery areas, loading dock areas, etc.), these parts of the development must be setback a minimum of 14 m (45.9 ft.) from the Ministry's right-of-way.</p>

Agency / Comment Date	Comments
	Rooftop storage and unconventional underground storage such as chambers and infiltration systems are not permitted. Underground storage provided in manholes, storm sewers, super pipes or storage tanks are permitted if such storages are accessible through a manhole and can be easily inspected for their continued functionality.
Parking (December 2022)	A satisfactory Parking Utilization Survey is required to support the proposed parking deficiency. Additional information regarding the transportation demand management strategy is required.
Public Art (December 2022)	The City strongly encourages the inclusion of public art in developments that are greater than 10,000 m <sup>2</sup> (107,639 ft <sup>2</sup> ) in gross floor area, with the exception of non-profit organizations and social housing. Developers are encouraged to include public art as part of their development and/or contribute an agreed upon amount of their gross construction costs to the City's Public Art Reserve Fund for the inclusion of public art near the subject site.
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> <li>• Alectra Utilities</li> <li>• Light Rail Transit Office</li> <li>• Fire Prevention Plan Examination</li> <li>• Transit Reviewer</li> <li>• Transit Infrastructure</li> <li>• GO Transit – Metrolinx</li> <li>• Rogers Cable</li> </ul>
	<p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"> <li>• Arborist – Streetscape</li> <li>• Arborist – City Property</li> <li>• Trillium Health Partners</li> <li>• Canada Post</li> <li>• CS Viamonde</li> <li>• Enbridge Gas Inc.</li> <li>• TransCanada Pipelines Ltd.</li> <li>• Greater Toronto Airport Authority</li> <li>• Trans Northern Pipeline</li> </ul>

## Development Requirements

There are engineering matters that might require the applicant to enter into agreements with the City. Prior to any development proceeding on-site, the City will require the submission and review of an application for site plan approval.

## 8. Community Benefits Charge

Schedule 17 of Bill 197, *COVID-19 Economic Recovery Act*, 2020, amended the *Planning Act*. The Section 37 Height/Density Bonus provisions are replaced with the Community Benefit Charge (CBC) provisions, implemented by a CBC By-law passed by Council. Section 37 of the *Planning Act* now allows municipalities to impose a CBC on land to fund costs related to growth. Funds collected under CBC will be to fund projects City-wide and Council will be requested at budget time each year to spend or allocate CBC funds to specific projects in accordance with the CBC Strategy and Corporate Policy.

In response to this legislative change, Council passed the City's new CBC By-law on June 22, 2022, which will be administered by the Corporate Services Department, Finance Division. The by-law specifies to which types of development and redevelopment the charge applies, the amount of the charge, exemptions and timing of charge payment. The CBC is 4% of the value of the land. A land appraisal is required in order to determine the applicable CBC in each case.

As the subject proposal is more than 5 storeys and contains 10 or more residential units in total, the CBC is applicable and will be payable at the time of first building permit.

## 9. Next Steps

Based on the comments received and the applicable MOP policies, the following matters will have to be addressed:

- Is the proposed development compatible with the existing and planned character of the area given the proposed massing and building height?
- Have appropriate amenity areas been provided to support the proposed development?
- Are the proposed zoning by-law exception standards including parking supply appropriate?

Upon satisfying the requirements of various City departments and external agencies, the Planning and Building Department will bring forward a recommendation report to a future Planning and Development Committee meeting. It is at this meeting that the members of the Committee will make a decision on the applications.