



**Scoped Heritage Impact Assessment
5150 & 5170 Ninth Line, City of Mississauga
Heritage Impact Assessment
For Adjacent 5104 Ninth Line
City of Mississauga, Region of Peel
(Formerly Town of Milton, in the Regional Municipality of Halton), Ontario**

Prepared for
Mattamy (5150 Ninth Line) Ltd.
7880 Keele Street
Vaughan, Ontario L4K 4G7
Tel: (905) 907 8871

By
Archaeological Research Associates Ltd.
219-900 Guelph Street
Kitchener, ON N2H 5Z6
Tel: (519) 804-2291 Fax: (519) 286-0493

HR-161-2019
Project# 2019-0280-03

July 10, 2020

Revised Report

EXECUTIVE SUMMARY

Under a contract awarded in July 2019 by Mattamy (5150 Ninth Line) Ltd., Archaeological Research Associates Ltd. was retained to complete a scoped Heritage Impact Assessment related to the South Ninth Line development at 5150 and 5170 Ninth Line, City of Mississauga (referred to going forward as 5150 Ninth Line). ARA was further retained in October 2019 to undertake revisions to the Heritage Impact Assessment to reflect revised concept plans for the development. Following submission of this report to the City of Mississauga, staff approved the recommended mitigation measures as outlined in the scoped Heritage Impact Assessment (dated October 29, 2019) on December 20, 2019. ARA was again retained by Mattamy (5150 Ninth Line) Ltd. in July 2020 to undertake revisions to the report to reflect the revised concept as presented in the site plan for the development including: an increased setback adjacent to the subject property; an increase in the unit count from 119 to 178; a change in visitor parking space and amenity area calculations; an increase in visitor parking spaces from 31 to 42; the realignment of the public street slightly further south; and revisions to the design of each product type. Units and driveways were added on the lots, and block numbers and road labels were also added. The HIA has been revised to reflect these updates, namely in Sections 1.0, 8.0, 9.0 and 10.1 of this report.

The adjacent property at 5104 Ninth Line is a listed property on the *City of Mississauga's Municipal Register of Property of Cultural Heritage Value or Interest*. As such, the City of Mississauga requested a scoped Heritage Impact Assessment. The City provided direction regarding completing a scoped assessment in accordance with the *City of Mississauga's Heritage Impact Assessment Terms of Reference* on July 25, 2019, indicating that **Section 2.1 (a detailed site history to include a listing of owners from the land registry office) and Section 2.3 bullets 1 and 2 (internal photographs and measured drawings) were not required.**

The property at 5104 Ninth Line, the subject property, was originally located within the boundaries of the Town of Milton. On January 1, 2010 lands situated east of Highway 407, including the subject property, were annexed to the City of Mississauga. The property is located within the east part of Lot 1, Concession 9, New Survey, in the former Geographic Township of Trafalgar, in the former Halton County, Ontario, currently the City of Mississauga, Regional Municipality of Peel.

The Heritage Impact Assessment approach consisted of the following:

- Background research concerning the project context, natural context and historical context of the study area;
- Consultation with the City of Mississauga's Heritage Planner;
- Identification of any designated or recognized cultural heritage properties within and adjacent to the subject lands;
- On-site inspection and identification of all properties with potential Built Heritage Resources and Cultural Heritage Landscapes within, adjacent to or otherwise in close proximity to the subject lands;
- A description of the location and nature of potential cultural heritage resources;
- Evaluation of potential cultural heritage resources against the criteria set out in Ontario Regulation 9/06 for determining cultural heritage value or interest;
- Evaluation of potential project impacts of the proposed development; and
- Provision of suggested strategies for the future conservation of identified cultural heritage resources.

Research, consultation and field work completed for this report found that 5150 Ninth Line did not possess potential cultural heritage value or interest. Following consultation, historical research, field survey and evaluation against the criteria set out in Ontario Regulation 9/06 of the *Ontario Heritage Act*, the property at 5104 Ninth Line was not found to meet any of the criteria. Modifications have impacted architectural features of 5104 Ninth Line such that it is no longer a representative example of the Neoclassical style, nor is it an early example. The property is associated with the O'Haras, a long-time farming family in the area. Research conducted did not suggest any notable contributions to the community made by the individuals who lived on the property. The contextual value of the property has been diminished through adjacent and proposed contemporary suburban development and the construction of Hwy 407.

The proposed development will have indirect impacts on the subject property. The development entails the demolition and removal of the existing dwelling and outbuildings from the project location at 5150 Ninth Line and the construction of approximately 178 condominium units in medium-density residential buildings to be located along the south side of Ninth Line. The proposed buildings are all attached homes including dual frontage towns, front load towns and back-to-back towns (village homes).

Research and field work completed for this report found that 5150 Ninth Line did not possess potential cultural heritage value or interest. Although the property at 5104 Ninth Line was not found to meet the criteria of Ontario Regulation 9/06, it is an historic farmhouse, as such development considerations that are sympathetic to the existing historic fabric have been suggested. Mitigation measures have been recommended for the development at 5150 Ninth Line that consider the existing historic fabric of the adjacent property at 5104 Ninth Line:

- That slight modifications to the designs of all of the proposed product types be considered, specifically the rear elevation of the front load towns (proposed directly adjacent to the subject property), to draw from and reflect the proportions, materials and design elements reflective of 5104 Ninth Line, notably the red brick cladding, brick voussoirs, medium pitched side gable roof, square window and door openings, and centrally placed front entrance with sidelights and/or transom.
- That to better reflect the adjacent property at 5104 Ninth Line, red brick and white design elements should be considered in the colour palate of each proposed product type.
- That during construction, it is recommended that construction fencing be erected between the property at 5104 Ninth Line and the project location at a sufficient distance to ensure that there will be no direct or indirect impacts to the building. The fencing should be maintained throughout the duration of the construction period.
- That to ensure the adequate screening of the property at 5104 Ninth Line, it is recommended that the proposed options presented by Mattamy (5150 Ninth Line) Ltd., including the erection of a wood privacy fence, the conservation of extant mature trees on the subject property and the planting of rows of vegetation be implemented along the east side of the development, directly adjacent to 5104 Ninth Line.
- That a landscape architect or arborist should be retained to investigate the feasibility of conserving mature trees along the east side of the development, directly adjacent to the subject property to further ensure adequate screening.

The 2020 *Provincial Policy Statement* notes that cultural heritage value or interest is bestowed upon cultural heritage resources by communities. Accordingly, the system by which heritage is governed in this province places emphasis on the decision-making of local municipalities in determining cultural heritage value or interest. It is hoped that the information presented in this report will be useful in those deliberations.

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GLOSSARY OF ABBREVIATIONS

ARA – Archaeological Research Associates Ltd.
 BHR – Built Heritage Resource
 CHL – Cultural Heritage Landscape
 CHVI – Cultural Heritage Value or Interest
 HIA – Heritage Impact Assessment
 MCL – (Former) Ministry of Culture
 MHSTCI – Ministry of Heritage, Sport, Tourism and Culture Industries
 MTC – (Former) Ministry of Tourism and Culture
 MTO – Ministry of Transportation
 OHA – Ontario Heritage Act

OHT – Ontario Heritage Trust
O. Reg. – Ontario Regulation
PPS – Provincial Policy Statement
VLA – Veteran's Land Act

PERSONNEL

Principal: P.J. Racher, MA, CAHP
Heritage Operations Manager: K. Jonas Galvin, MA, RPP, MCIP, CAHP
Project Manager: L. Benjamin MAES, RPP, MCIP, CAHP
Site Visit: K. Jonas Galvin, L. Benjamin
Historical Research: S. Clarke, BA
Photography: K. Jonas Galvin
Cartographer: K. Brightwell (GIS)
Technical Writers: K. Jonas Galvin, L. Benjamin, P. Young, MA, CAHP, J. McDermid, BA

Two-page curriculum vitae (CV) that demonstrate the qualifications and expertise of key team members to perform cultural heritage work in Ontario are provided in Appendix D.

MINIMUM REPORT REQUIREMENTS CHART

City of Mississauga Minimum Requirements	ARA Equivalent
2.1 Site History	3.0 Site History
2.2 Description of Existing Structures	5.0 Description of Buildings and Structures
2.2 Statement of Conclusions (Significance and Heritage Attributes of the Cultural Heritage Resource)	7.0 Heritage Assessment 11.0 Mandatory Statement
2.2 Location Map	1.0 Project Context
2.3 Documentation of Existing Conditions (Current External Photographs)	Appendix A: 5150 and 5104 Ninth Line Images
2.3 Documentation of Existing Conditions (Measured Drawings – Elevations, Floor Plans, and a Site Plan or Survey)	5.0 Description of Buildings and Structures – Project Location at 5150 Ninth Line 6.0 Description of Buildings and Structures – 5104 Ninth Line
2.3 Documentation of Existing Conditions (Historical Photos, Drawings or Other Archival Material)	Appendix B: Historical Photos, Drawings and Other Archival Material
2.4 Outline of the Proposed Development	8.0 Proposed Development Appendix C: Preliminary Building Elevations for Each Product Type
2.5 Full Architectural Drawings	Appendix C: Preliminary Building Elevations for Each Product Type
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4. Mandatory Recommendation	11.0 Mandatory Statement
5. Qualifications	Appendix D: Key Team Member Two-Page Curriculum Vitae

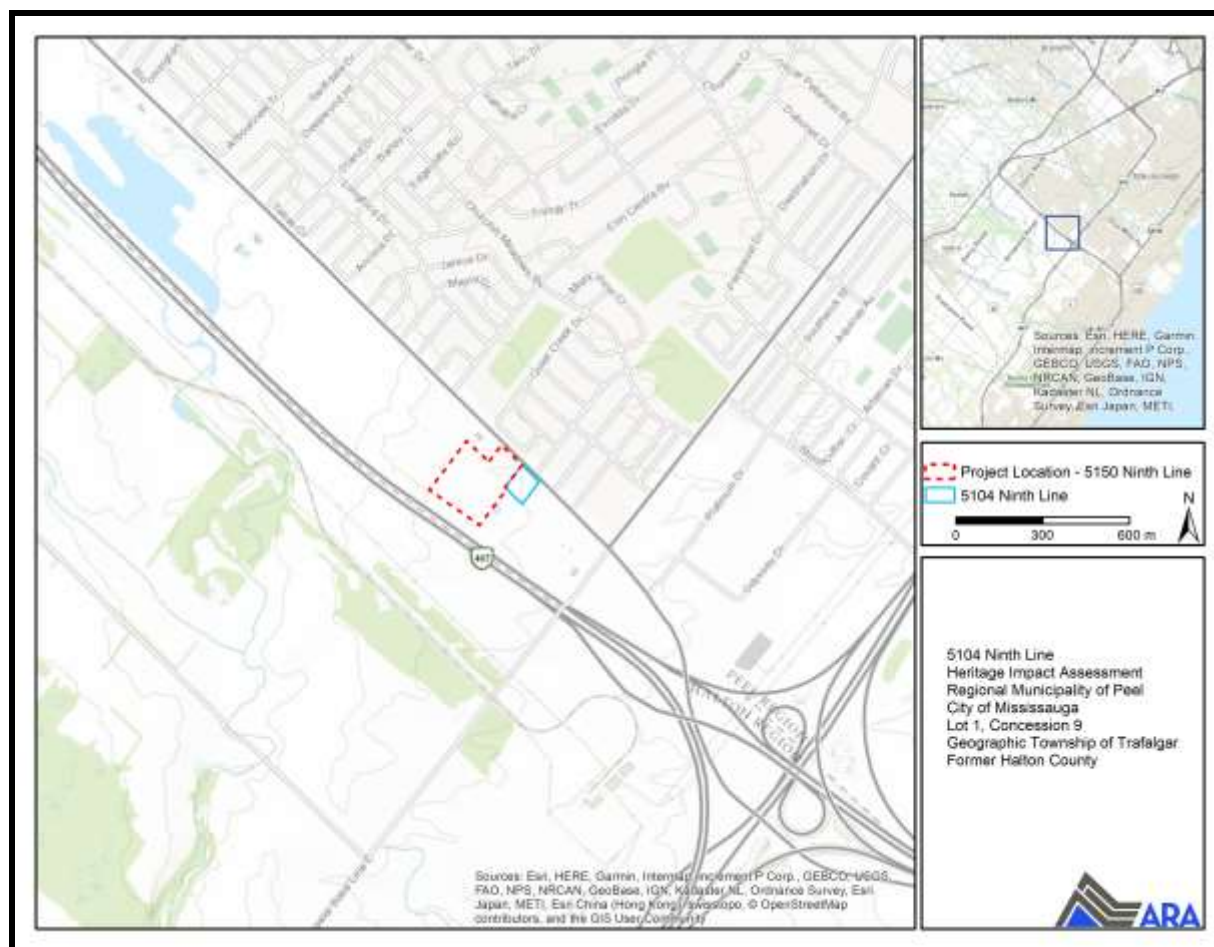
1.0 PROJECT CONTEXT

Under a contract awarded in July 2019 by Mattamy (5150 Ninth Line) Ltd., Archaeological Research Associates Ltd. (ARA) was retained to complete a scoped Heritage Impact Assessment (HIA) related to the South Ninth Line development at 5150 and 5170 Ninth Line, City of Mississauga (referred to going forward as 5150 Ninth Line or the project location). ARA was further retained in October 2019 to undertake revisions to the HIA to reflect revised concept plans for the development. Following submission of this report to the City of Mississauga, staff approved the recommended mitigation measures as outlined in the scoped Heritage Impact Assessment (dated October 29, 2019) on December 20, 2019. ARA was again retained by Mattamy (5150 Ninth Line) Ltd. in July 2020 to undertake revisions to the report to reflect the revised concept as presented in the site plan for the development including: an increased setback adjacent to the subject property; an increase in the unit count from 119 to 178; a change in visitor parking space and amenity area calculations; an increase in visitor parking spaces from 31 to 42; the realignment of the public street slightly further south; and revisions to the design of each product type. Units and driveways were added on the lots, and block numbers and road labels were also added. The HIA has been revised to reflect these updates, namely in Sections 1.0, 8.0, 9.0 and 10.1 of this report.

The adjacent property at 5104 Ninth Line is a listed property on the *City of Mississauga's Municipal Register of Property of Cultural Heritage Value or Interest*. As such, the City of Mississauga requested a scoped HIA. On July 25, 2019, the City provided direction regarding completion of a scoped HIA in accordance with the *City of Mississauga's Heritage Impact Assessment Terms of Reference* and indicated that Section 2.1 (a detailed site history to include a listing of owners from the land registry office) and Section 2.3 bullets 1 and 2 (internal photographs and measured drawings) were not required (2017a). For the purposes of this assessment, the properties at 5150 and 5170 Ninth Line will comprise the study area.

The property located at 5104 Ninth Line, the subject property, was originally located within the boundaries of the Town of Milton. On January 1, 2010 lands situated east of Highway 407, including the subject property, were annexed to the City of Mississauga (City of Mississauga 2009). The property is located within part of Lot 1, Concession 9, New Survey, in the former Geographic Township of Trafalgar, in the former Halton County, Ontario, currently the City of Mississauga, Regional Municipality of Peel (see Map 1).

The purpose of this assessment is to identify and evaluate the cultural heritage resources within the study area that may be impacted by the proposed development. This assessment was conducted in accordance with the aims of the *Planning Act* R.S.O. 1990, c. P.13, *Provincial Policy Statement* (2020), *Ontario Heritage Act*, R.S.O. 1990, c. O.18, *City of Mississauga Official Plan* (2019), and the *City of Mississauga Heritage Impact Assessment Terms of Reference* (2017a).



Map 1: Project Location and 5104 Ninth Line in the City of Mississauga
(Produced by ARA under licence using ArcGIS® software by Esri, © Esri)

2.0 METHOD

The framework for this assessment report is provided by provincial planning legislation and policies as well as municipal Official Plans and guidelines. Section 2 of the *Planning Act* indicates that a council of a Municipality have regard for matters of provincial interest such as: “(d) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest.” Section 3 of the *Planning Act* directs a municipal Council’s decisions to be consistent with the *Provincial Policy Statement* (PPS 2020). Policy 2.6.1 states: “Significant built heritage resources and significant cultural heritage landscapes shall be conserved.”

With respect to cultural heritage, the *Mississauga Official Plan* Policy 7.4.1 states that “Mississauga’s cultural heritage resources reflect the social, cultural and ethnic heritage of the city and, as such, are imperative to conserve and protect” (2019a:7.7). Additionally, Policy 7.4.1.12 states that “the proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource, or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment (HIA), prepared to the satisfaction of the City and other appropriate authorities having jurisdiction” (2019a:7.8). Additionally, the *City of Mississauga Heritage Impact Assessment Terms of Reference* (2017a) outline the required elements for HIAs prepared on properties situated within the City of Mississauga.

2.1 Key Concepts

The following concepts require clear definition in advance of the methodological overview; proper understanding is fundamental for any discussion pertaining to cultural heritage resources:

- **Built Heritage Resource (BHR)** can be defined in the *PPS* as: “a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property’s cultural heritage value or interest as identified by a community, including an Indigenous community. *Built heritage resources* are located on property that may be designated under Parts IV or V of the *Ontario Heritage Act*, or that may be included on local, provincial, federal and/or international registers” (MMAH 2020:41).
- **Cultural Heritage Landscape (CHL)** is defined in the *PPS* as: “a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. *Cultural heritage landscapes* may be properties that have been determined to have cultural heritage value or interest under the *Ontario Heritage Act*, or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms” (MMAH 2020:42).

It is recognized that the heritage value of a CHL is often derived from its association with historical themes that characterize the development of human settlement in an area (Scheinman 2006). In Ontario, typical themes which may carry heritage value within a community include, but are not limited to: 1) Pre-Contact habitation, 2) early European exploration, 3) early European and First Nations contacts, 4) pioneer settlement, 5) the development of transportation networks, agriculture and rural life, 6) early industry and commerce, and/or 7) urban development. Individuals CHLs may be related to a number of these themes simultaneously.

The *Operational Guidelines for the Implementation of the World Heritage Convention* defines several types of CHLs: 1) designed and created intentionally by man, 2) organically evolved landscapes which fall into two-subcategories (relic/fossil or continuing), and 3) associative cultural landscapes (UNESCO 2008:86). MCL (at the time) *Information Sheet #2 Cultural Heritage Landscapes* (MCL 2006c) repeats these definitions to describe landscapes in Ontario.

- **Cultural Heritage Value or Interest (CHVI)**, also referred to as Heritage Value, is identified if a property meets one of the criteria outlined in O. Reg. 9/06 namely historic or associate value, design or physical value and/or contextual value. Provincial significance is defined under *Ontario Heritage Act (OHA)* O. Reg. 10/06.
- **Conserved** means “the identification, protection, management and use of *built heritage resources*, *cultural heritage landscapes* and *archaeological resources* in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision- maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments” (MMAH 2020:41-42).

- **Heritage Attributes** are defined in the *OHA* as: “the principal features or elements that contribute to a protected heritage property’s cultural heritage value or interest, and may include the property’s built or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (including significant views or vistas to or from a protected heritage property means, in relation to real property, and to the buildings and structures on the real property, the attributes of the property, buildings and structures that contribute to their cultural heritage value or interest” (Government of Ontario 2019).
- **Significant** in reference to cultural heritage is defined as: “resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the *Ontario Heritage Act*” (MMAH 2020:51).

Key heritage definitions from the *City of Mississauga Official Plan* are as follows:

- **Heritage Impact Assessment** is defined as “a statement that will identify all heritage resources of a property; describe and evaluate their heritage significance; and, evaluate their sensitivity to a proposed development, use or reuse, including, where possible, measures to mitigate deleterious consequences” (City of Mississauga 2019a:20-4).

2.2 Types of Recognition

BHRs and CHLs are broadly referred to as cultural heritage resources. A variety of types of recognition exist to commemorate and/or protect cultural heritage resources in Ontario.

The National Historic Sites program commemorates important sites, people or events that had a nationally significant effect on, or illustrate a nationally important aspect of, the history of Canada. The Minister of Canadian Heritage on the advice of the Historic Sites and Monuments Board of Canada (HSMBC) makes recommendations to the program. Yet another form of recognition at the federal level is the Canadian Heritage Rivers System program. It is a federal program to recognize and conserve rivers with outstanding natural, cultural and recreational heritage. It is important to note that both of these federal commemoration programs do not offer protection from alteration or destruction.

The Ontario Heritage Trust (OHT) operates the Provincial Plaque Program that has over 1,250 plaques located across the province recognizing key people, places and events that shaped Ontario. Additionally, properties owned by the province may be recognized as a “provincial heritage property” (MTC 2010).

Protected properties are those designated under Part IV (individual properties) or Part V (Heritage Conservation District) of the *OHA*. Once designated, a property cannot be altered or demolished without the permission of the local council. A cultural heritage resource may also be protected through a municipal or OHT easement. Many heritage committees and historical societies provide plaques for local places of interest.

Under *Section 27* of the *OHA*, a municipality must keep a Municipal Heritage Register. A register lists designated properties as well as other properties of cultural heritage value or interest in the municipality. Properties on this list that are not formally designated are commonly referred to as “listed.” Listed properties are flagged for planning purposes and are afforded a 60-day delay in demolition if a demolition request is received.

2.3 Approach

The City of Mississauga's *Heritage Impact Assessment Terms of Reference* outlines the contents required to determine the impacts to known and potential heritage resources within a defined area proposed for future development. This HIA records a detailed site history; a location map; complete listing and full written descriptions of all existing structures on the property; documentation, including current photographs; an outline of the proposed development; an assessment of alternative development options and mitigation measures; a summary of conservation principles; an explanation of proposed demolitions/alterations; and a clear statement of the conclusions regarding the significance and heritage attributes of the cultural heritage resource (City of Mississauga 2017a).

2.3.1 Historical Research

Background information is obtained from aerial photographs, historical maps (i.e., illustrated atlases), archival sources (i.e., historical publications and records), published secondary sources (online and print) and local historical organizations. The 1851 Census for the Township of Trafalgar has not survived (LAC 2018). Given that research is constrained to sources in the public record and conducted in a limited time frame, there is the possibility that additional historical information exists but may not have been identified.

2.3.2 Consultation

Consultation with the local community is essential for determining the community value of cultural heritage resources. At project commencement, ARA contacted the relevant local municipalities to inquire about: 1) protected properties in the study area, 2) properties with other types of recognition in the study area, 3) previous studies relevant to the current study, and 4) other heritage concerns regarding the study area or project. Where possible, information was sought directly from the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) and OHT. In this case, the City of Mississauga was contacted to obtain feedback on the potential cultural heritage resources in the vicinity as well as the scope of the study.

2.3.3 Field Survey

The field survey component of an assessment involves the collection of primary data through systematic photographic documentation of all potential cultural heritage resources within the study area, as identified through historical research and consultation. Additional cultural heritage resources may also be identified during the survey itself. Photographs capturing all properties with potential BHRs and CHLs are taken, as are general views of the surrounding landscape. The site visit also assists in confirming the location of each potential cultural heritage resource and helps to determine the relationship between resources.

2.4 Evaluation of Significance

In order to objectively identify cultural heritage resources, O. Reg. 9/06 made under the *OHA* sets out three principal criteria with nine sub-criteria for determining CHVI (MCL 2006a:20–27). The criteria set out in the regulation were developed to identify and evaluate properties for designation under the *OHA*. Best practices in evaluating properties that are not yet protected employ O. Reg. 9/06 to determine if they have CHVI. These criteria include: design or physical value, historical or associative value and contextual value.

1. The property has design value or physical value because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has contextual value because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark. O. Reg. 9/06, s. 1 (2).

If a potential cultural heritage resource (BHR or CHL) is found to meet any one of these criteria, it can then be considered an identified resource.

2.5 Evaluation of Impacts

Any potential project impacts on identified BHRs or CHLs must be evaluated, including direct and indirect impacts. *InfoSheet #5: Heritage Impact Assessments and Conservation Plans* (2006b:3) provides an overview of several major types of negative impacts, including but not limited to:

- Destruction of any, or part of any, significant heritage attributes;
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance;
- Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;
- Isolation of a heritage attribute from its surrounding environment, context or significant relationship;
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features;
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces; and
- Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.

2.6 Mitigation Strategies

If potential impacts to identified heritage resources are determined, proposed conservation or mitigative/avoidance measures must be recommended. The Ministry of Culture's *InfoSheet #5: Heritage Impact Assessments and Conservation Plans* (2006b:3) lists several specific methods

of minimizing or avoiding a negative impact on a cultural heritage resource, including but not limited to:

- Alternative development approaches;
- Isolating development and site alteration from significant built and natural features and vistas;
- Design guidelines that harmonize mass, setback, setting, and materials;
- Limiting height and density;
- Allowing only compatible infill and additions;
- Reversible alterations; and
- Buffer zones, site plan control, and other planning mechanisms.

Many of these mitigation strategies are echoed in the City of Mississauga's *Heritage Impact Assessment Terms of Reference* that also lists salvage mitigation, relocation, ruinification and symbolic conservation (2017a).

2.7 Summary of Approach

The approach outlined herein is supported by the best practices, guidelines and policies of the following:

- *Provincial Policy Statement* (2020);
- *Ontario Heritage Act* (R.S.O. 1990);
- *Ontario Heritage Tool Kit* series (MCL 2006a);
- *City of Mississauga Official Plan* (2019); and
- *City of Mississauga Heritage Impact Terms of Reference* (2017a).

3.0 SITE HISTORY

The Township of Trafalgar has a long history of Indigenous land use and settlement including Pre-Contact and Post-Contact campsites and villages. It should be noted that the written historical record regarding Indigenous use of the landscape in southern Ontario draws on accounts by European explorers and settlers. As such, this record details only a small period of time in the overall human presence in Ontario. Oral histories and the archaeological record show that Indigenous communities were mobile across great distances, which transcend modern understandings of geographical boundaries and transportation routes.

Based on current knowledge, the cultural heritage resources located within the study area are tied to the history of the initial settlement and growth of Euro-Canadian populations in the Township of Trafalgar. The arrival of European explorers and traders at the beginning of the 17th century triggered widespread shifts in Indigenous lifeways and set the stage for the ensuing Euro-Canadian settlement process. Documentation for this period is abundant, ranging from the first sketches of Upper Canada and the written accounts of early explorers to detailed township maps and lengthy histories. The Post-Contact period can be effectively discussed in terms of major historical events, and the principal characteristics associated with these events are summarized in Table 1.

3.1 Trafalgar Township, Former County of Halton

The early history of the study area can be effectively discussed in terms of major historical events. The principal characteristics associated with these events are summarized in Table 1.

Table 1: County and Township Settlement History
(Wilson's Publishing Co. 2000; Warnock 1862; Cumming 1971; Town of Oakville 2008; City of Mississauga 2009)

Historical Event	Timeframe	Characteristics
Loyalist Influx	Late 18 th century	United Empire Loyalist influx after the American Revolutionary War (1775–1783); British develop interior communication routes and acquire additional lands; <i>Constitutional Act</i> of 1791 creates Upper and Lower Canada.
Peel County Development	Late 18 th and early 19 th century	Area initially adjacent to York County's 'West Riding'; Became part of York County's 'West Riding' in 1798; Southern portion acquired as part of the 'First Purchase of the Mississauga Tract' in 1805; Northern portion acquired as part of the 'Second Purchase' or 'Ajetance Purchase' in 1818; Peel County established after the abolition of the district system in 1849.
Trafalgar Township Formation	Early 19 th century	First settlers arrived in southeastern part of Trafalgar (the 'Old Survey') ca. 1807; Prominent early families in the south included the Sovereigns, Proudfoots, Kattings, Freemans, Posts, Biggars, Mulhollands, Kenneys, Chalmers, Albertsons, Chisholms, Sproats, Browns and Hagars; Population reached 548 by 1817 with four saw mills and one grist mill in operation; the 'New Survey' comprised the northwestern lands acquired in 1818.
Trafalgar Township Development	Mid-19 th and early 20 th century	By 1846, 28,375 ha had been taken up in Trafalgar, with 11,404 ha under cultivation; 23 saw mills and seven grist mills in operation at that time; Population reached 4,513 by 1850; Traversed by the Hamilton & Toronto Branch of the Great Western Railway (1855), the Hamilton & North Western Railway (1877) and the Credit Valley Railway (1877); Communities at Milton, Hornby, Auburn, Boyne, Omagh, Drumquin in the north and Oakville, Bronte, Palermo, Trafalgar, Munn's Corner and Sheridan in the south.
Town of Oakville Amalgamation & Transfer to Peel	Mid-20 th and early 21 st century	In 1962, the Township of Trafalgar and the Town of Oakville amalgamated to form a new Town of Oakville with four wards (Town of Oakville 2008:3); At this time, the Township of Trafalgar was part of the County of Halton; In 2010, lands from the Town of Milton were annexed to become part of the City of Mississauga (City of Mississauga 2009).

3.2 Study Area

As discussed in Section 1.0, 5150 and 5104 Ninth Line fall on the east part of Lot 1, Concession 9 in the Geographic Township of Trafalgar, in the former Halton County, Ontario.

To reconstruct the historic land use of the study area, ARA examined three historical maps that documented past residents, structures (i.e., homes, businesses and public buildings) and features between the early-19th and late-19th centuries. Specifically, the resources outlined in Table 2 were consulted.

Table 2: Maps and Aerial Photographs Consulted

Year	Map Title	Reference
1858	Tremaine's Map of the County of Halton, Canada West	Tremaine
1877	Northern Part of Trafalgar	Walker & Miles
1909	Brampton Sheet No. 35 [030M12] Topographic Map	OCUL

The limits of the study area are shown on georeferenced versions of the consulted historical maps (see Map 2-Map 4).

“Chas O’Hara” is indicated as the occupant of the east half of Lot 1, Concession 9. Structures are not depicted on the 1858 map; however, property occupants are indicated, as are major thoroughfares and waterways (see Map 2).

The 1877 map of Trafalgar Township indicates that William O’Hara was the occupant of the east half of Lot 1, Concession 9, with a farmhouse and orchard depicted on his property fronting Ninth Line (see Map 3).

By 1909, modern road alignments were well established with the project location indicated northwest along Ninth Line. The O’Hara’s red brick residence is indicated on the map in red with the municipal address of 5104 Ninth Line (Map 4).

3.3 Project Location

The project location on part of the east half of Lot 1, Concession 9 in the Township of Trafalgar was severed from the larger 100-acre farm, once belonging to the O’Hara family, in 1953. The O’Hara home remains extant on the property at 5104 Ninth Line.

3.4 5104 Ninth Line

The Crown Patent for the east half (100 acres) of Lot 9, Concession 1 New Survey in the Township of Trafalgar, Halton County went to **Charles O’Hara Sr. in April of 1830. Charles O’Hara, a Catholic farmer from Ireland, appears to have resided on the property prior to receipt** of the Crown Patent. In 1819, O’Hara and early township settler Bartholomew O’Connor persuaded Father O’Reilly who was located 40 miles distant, to attend O’Hara’s cabin on Lot 1, Concession 9 to celebrate mass. Shortly thereafter in 1823 a log church was erected on Lot 6, Concession 1 in the Township of Trafalgar (OGS 2019).

In July of 1830, O’Hara Sr. sold the property to his son, Charles O’Hara Jr. **The O’Haras formed part of an Irish Catholic settlement in the Township of Trafalgar in the vicinity of Ninth Line and Britannia Road known colloquially as the “Catholic Swamp”** for the flooded land conditions and Catholic inhabitants there (Heritage Mississauga 2010:198). An alternative name for the settlement was **Nunan’s Corners** after one of the first settlers to the area, William Nunan (Heritage Mississauga 2010:198). Census records from 1851 have not survived for the Township of Trafalgar, however the 1861 census provides information that further details the O’Haras that resided on the property. Charles O’Hara Jr. (55) and his wife Mary (50) are enumerated in the 1861 census as living in a one-and-a-half storey brick house with their children Ellen (20), John (23), Lucinda (18), Anabella (16), James (15), Charles (14), Peter (12), Michael (10) and William (8) (LAC 1861). The O’Hara family were Irish Catholic farmers that resided in the subject building at 5104 Ninth Line.

It appears as though Charles O’Hara died between 1861 and 1862, as his wife Mary and son William are listed as the occupants/owners of Lot 1, Concession 9 New Survey in 1862 (Warnock 1862:12). In 1875, Mary O’Hara sold the family farm to her youngest sons, William and Michael.

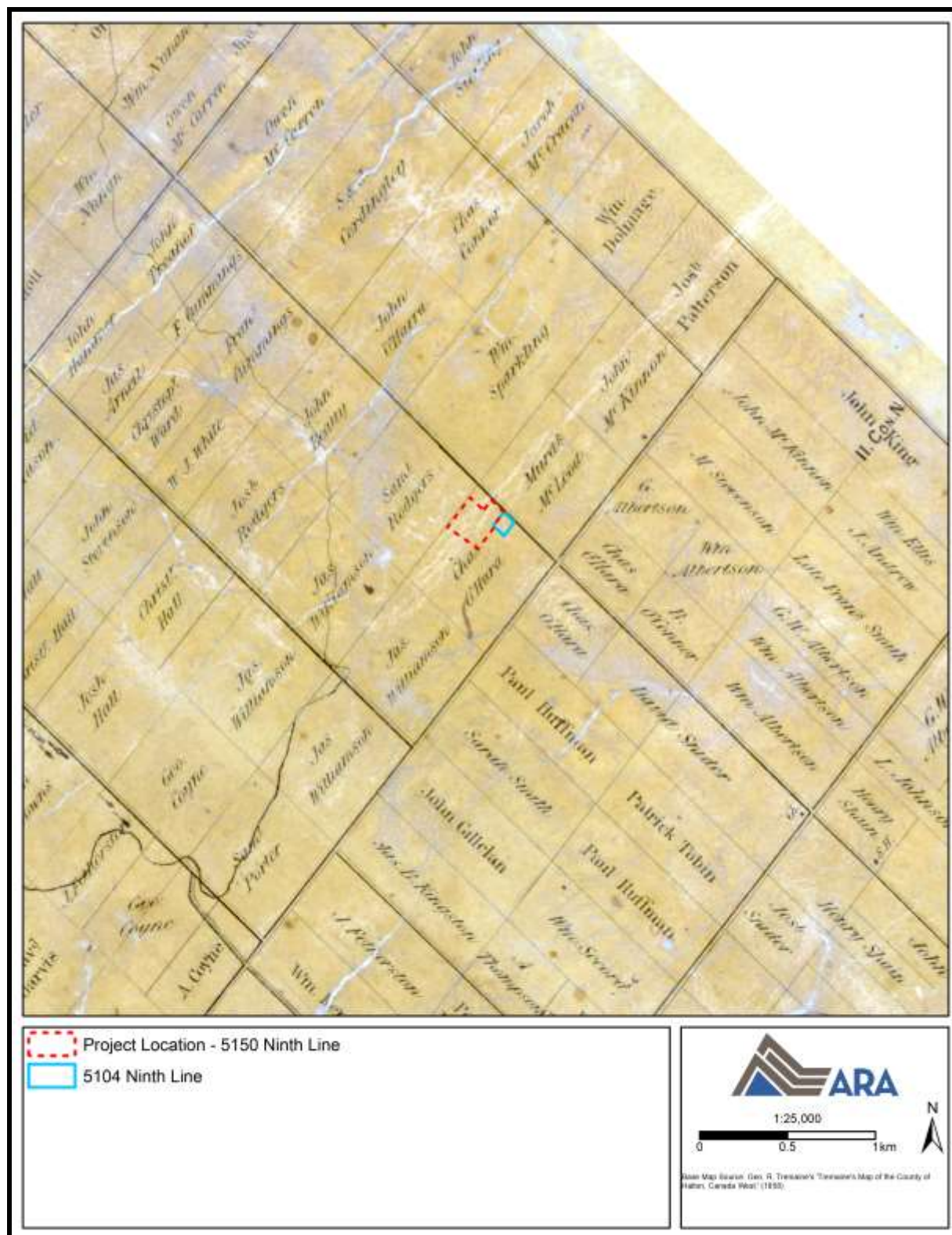
By 1875, the family farmstead had come under the ownership of Michael O’Hara. O’Hara and his family do not appear in the 1891 census for the Township of Trafalgar, however historic

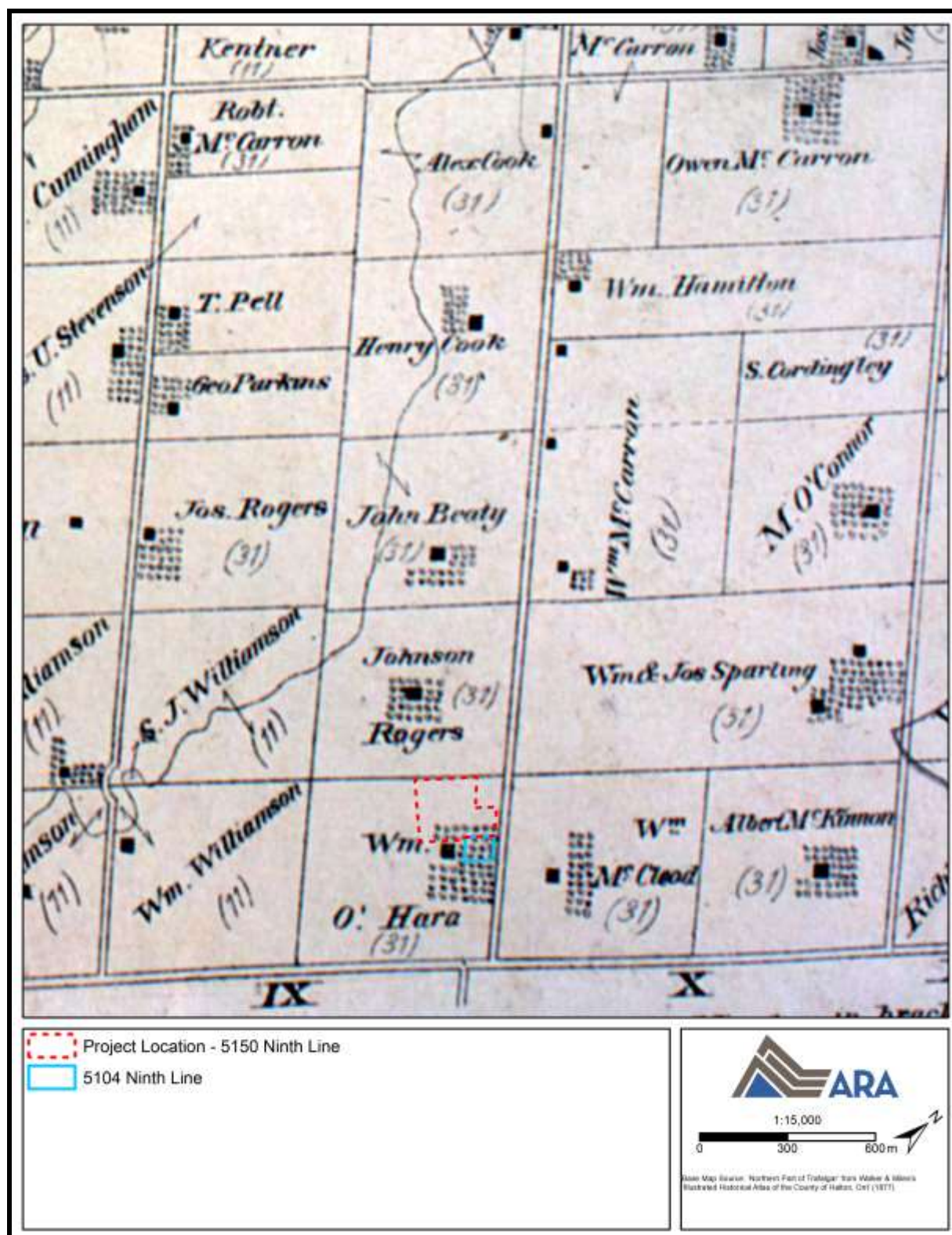
directories helped to corroborate that the Michael O'Hara family were residing at the property in 1893 (UPC 1893:35). According to the 1901 census, Michael O'Hara (54) and his wife Catherine (39) were residing in a nine-room brick house with their children William (18), Mary (15), Frederick (14), John (12), Wilfred (9), Charles (7), Catherine (5), Joseph (3) and Annie (7 months) (LAC 1901a). At that time, the Irish-Catholic O'Hara family had four barns on their farm property along with their residence (LAC 1901b).

The O'Hara family is enumerated in both the 1911 and 1921 census as residing in the nine-room brick house with their family. Between 1901 and 1911, Michael and Catherine had additional children, though by 1921 all had left home with the exception of Joseph (33) and Lillian (16) (LAC 1911, LAC 1921). Michael O'Hara died in 1928, though his wife Catherine continued to reside at the farmstead until her passing in 1948 (Find a Grave 2019). In 1946 (registered in 1948), the executors of Michael O'Hara sold the 100-acre farm property to Toyne Grice. Grice sold off parcels of the 100-acre lot for development, with the O'Hara farmstead being sold to the Director of the *Veteran's Land Act* (VLA) in 1953. The occupation history of the property from 1953-1973 is unclear, with the Director of the VLA selling the property to Allan and Catherine St. John in 1973. Weeks after the sale to the St. Johns, the property was again sold, though to Joseph and Nicole Burak. The property continued to change ownership two more times until 1994 when it came under the ownership of Robert Turnbull.

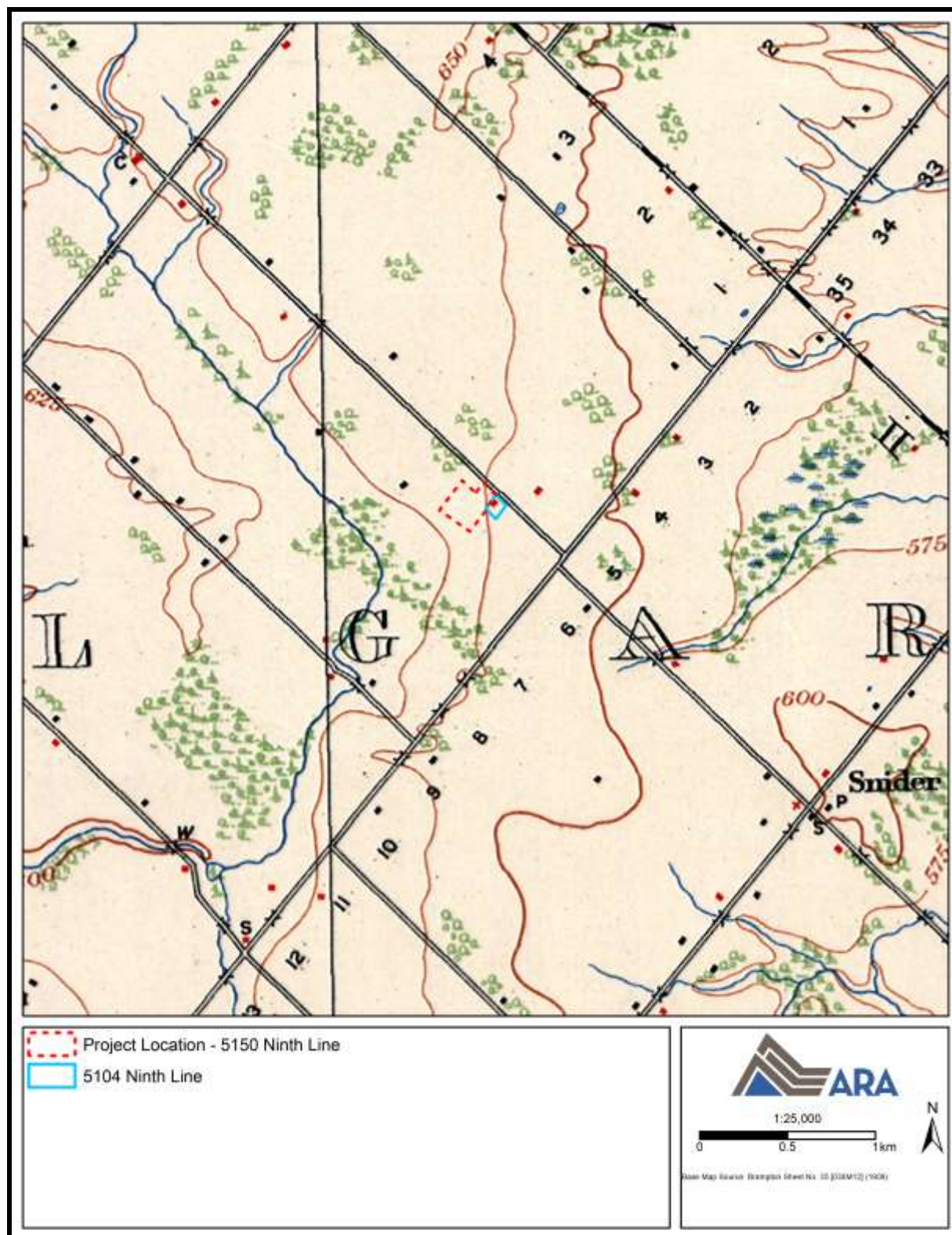
According to the City of Mississauga, the house was built in 1848 and is known as the "O'Hara House." Specifically, "This house is believed to have been built by Charles O'Hara Junior in 1848. His father was Charles O'Hara Senior who had originally petitioned for land from the Crown in 1830. This farm remained in the O'Hara family until 1948. This Georgian style house is one-and-a-half storeys with red brick and gable roof. An extension is built on the rear."

Research conducted including review of the Heritage Mississauga materials, business directories and census information did not suggest any notable contributions to the community made by the individuals who lived on this property.





Map 3: Project Location and 5104 Ninth Line on an 1877 Map
(Produced by ARA under licence using ArcGIS® software by Esri, © Esri; Walker & Miles 1877)



4.0 HERITAGE CONTEXT

In order to determine whether any previously-identified properties with CHVI are located within the study area, ARA consulted a number of heritage groups, the municipality and online heritage resources.

4.1 Consultation

The MHSTCI's current list of heritage conservation districts was consulted. No designated districts were identified in the study area (MHSTCI 2019). The list of properties designated by the MHSTCI under Section 34.5 of the *OHA* was consulted. No properties in the study area are listed. The OHT plaque database and the Federal Canadian Heritage Database were searched. None of the properties within the study area are commemorated with an OHT plaque.

ARA staff contacted the City of Mississauga via telephone and email on July 25, 2019. The City provided direction regarding completion of a scoped HIA in accordance with the *City of Mississauga Heritage Impact Assessment Terms of Reference* (2017) and indicated that Section 2.1 (a detailed site history to include a listing of owners from the land registry office) and Section 2.3 bullets 1 and 2 (internal photographs and measured drawings) were not required.

4.2 Site Visit

A site visit was conducted on July 29, 2019 to photograph and document the project location and adjacent property at 5104 Ninth Line, to record any local features that could enhance ARA's understanding of their setting in the landscape and to contribute to the cultural heritage evaluation process. ARA staff received permission to enter 5150 Ninth Line to conduct the site visit. Photographs of 5104 Ninth Line were taken from public lands as well as from 5150 Ninth Line. Photographs of the project location and 5104 Ninth Line can be found in Appendix A.

5.0 DESCRIPTION OF BUILDINGS AND STRUCTURES – PROJECT LOCATION AT 5150 NINTH LINE

The property at 5150 Ninth Line contains a residence with a detached garage located in the northwest corner of the lot, fronting Ninth Line. A workshop is located adjacent to the north extent of the property line setback from the road and behind the property at 5170 Ninth Line. An access road runs north-south across the property and leads to a cell phone tower located in the southwest corner of the lot.

5.1 Context

The land adjacent to 5150 Ninth Line, on the north side of the roadway, has been subdivided and contains contemporary suburban dwellings (see Image 35). The historic residential property at 5104 Ninth Line is located on the lot immediately to the east and a contemporary commercial building and residence are located to the west. Highway 407 is located to the rear of the property.

5.2 Arrangement of Buildings and Structures

The property is accessed via three driveways from Ninth Line. The dwelling is located on the west side of the lot with the façade oriented to the north, facing Ninth Line. Accessory structures

include a large aluminum clad garage located immediately west of the dwelling, a large workshop constructed of concrete block located on the west extent of the lot, and a cell phone tower and associated access road located in the southwest corner of the property. The 2017 surveys included in Figure 1 and Figure 2 illustrate the current location of these structures.

5.3 Landscape Features

The dwelling at 5150 Ninth Line is set back from Ninth Line slightly behind the garage, both of which are accessed via a driveway from the road. A shorter, abandoned driveway is located southeast of the dwelling. The house, garage and workshop are situated on the northern portion of the lot's relatively flat topography, which begins to roll slightly in the southern extent of the lot. The site's landscape surrounding the dwelling and outbuildings to the south include agricultural fields that had been recently tilled but were not under cultivation at the time of the site visit (see Image 2, Image 15-Image 18, Image 34). Mature trees surround the house and garage, screening the northern half of the lot (see Image 2). Formal rows of mature coniferous trees were observed flanking the east and west sides of the garage (see Image 8-Image 10). Notable and impressively large mature deciduous trees flank the property along Ninth Line (see Image 13-Image 14).

5.4 Dwelling Exterior

The dwelling at 5150 Ninth Line is a one-and-a-half storey red brick and vinyl clad structure with a side gable roof clad in asphalt shingles (see Image 3-Image 7). The foundation appears to be concrete. The north, three-bay façade features a projecting one-and-a-half storey attached one-car garage, the main entrance and a large window opening. The east and west elevations reflect an A-frame structure and include a porch below the projecting eaves. Two windows are included in the projecting dormer on the south elevation. A simple wood porch wraps around the dwelling (see Image 5).

5.5 Detached Garage Exterior

A detached garage is located immediately to the northwest of the dwelling. It is a five-bay structure clad in aluminum siding and constructed to a rectangular plan with a front gable roof (see Image 8-Image 10). The façade features a single garage door as does the east elevation in addition to two entrance doors.

5.6 Workshop Exterior

A large workshop is located in the north corner of the property adjacent to the west property line. It is constructed of concrete block to a rectangular plan and features aluminum siding in the gable ends (see Image 11-Image 12). Two sloped overhangs extending from the north and south elevations provide sheltered space. A single garage door is located in the west elevation.

5.7 Architectural Style/Design

The dwelling and outbuildings at 5150 Ninth Line are contemporary structures constructed of modern materials in the late 20th century and are not representative of formal architectural styles considered significant or rare provincially or in the City of Mississauga.



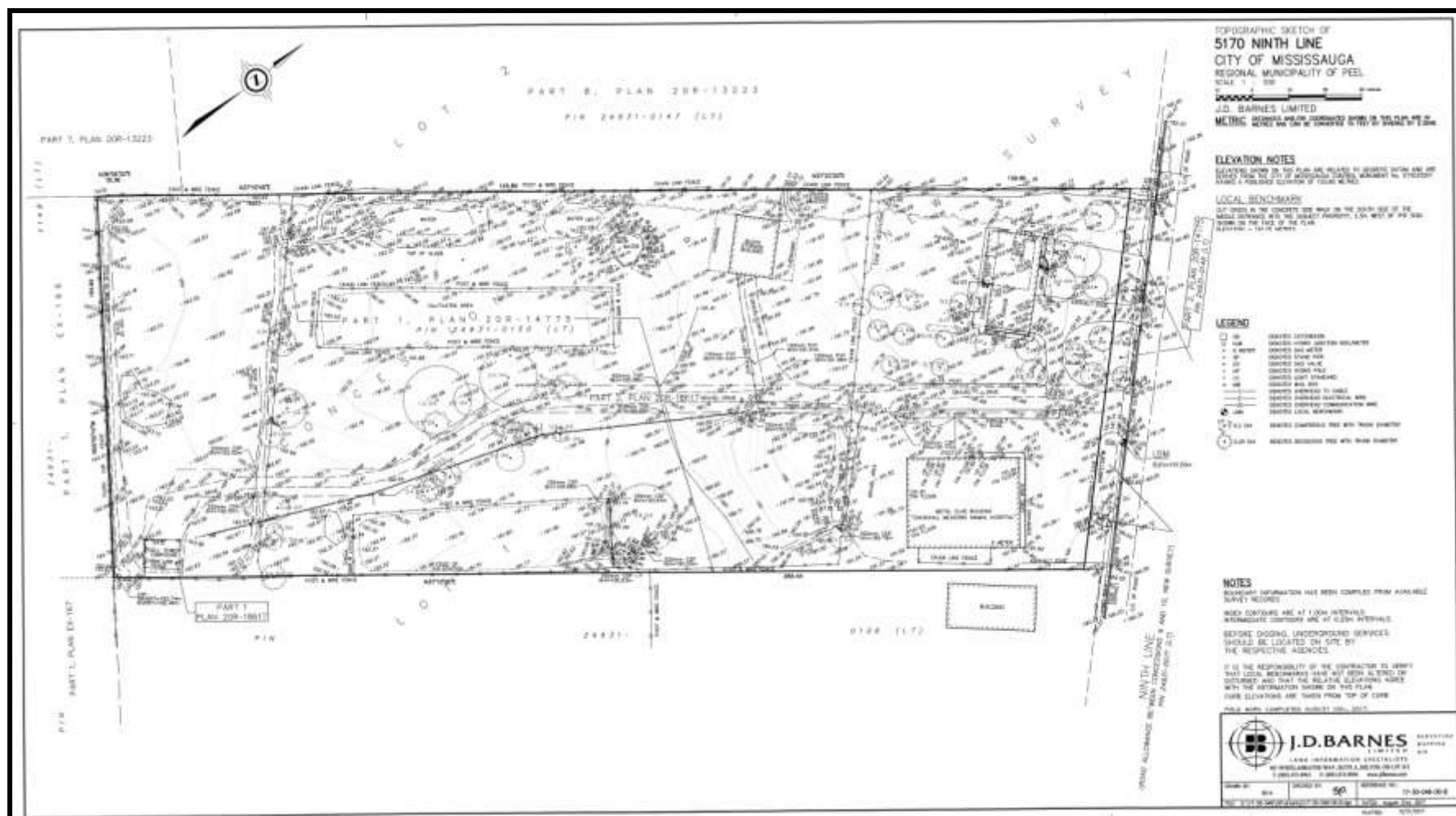


Figure 2: Topographic Sketch of 5170 Ninth Line
(J. D. Barnes Limited 2017b)

6.0 DESCRIPTION OF BUILDINGS AND STRUCTURES – SUBJECT PROPERTY AT 5104 NINTH LINE

The subject property at 5104 Ninth Line contains a house oriented towards Ninth Line located in the centre of the west side of the lot along with a large detached workshop located to the rear of the house at the southern property boundary.

6.1 Context

The land adjacent to 5104 Ninth Line, on the north side of the roadway, has been subdivided and contains contemporary suburban dwellings (see Image 35). The residential property at 5150 Ninth Line is located on the lot immediately to the west and a vacant lot is located to the east. Highway 407 is located to the south of the property.

6.2 Arrangement of Buildings and Structures

The property is accessed via a long driveway from Ninth Line. The dwelling is located on the west side of the lot with the façade oriented to the north, facing Ninth Line. Accessory structures include a large workshop, located to the south of the dwelling and a small shed located immediately to the west of the house.

6.3 Landscape Features

The dwelling at 5104 Ninth Line is set back from Ninth Line and is accessed via a long driveway that extends beyond the east side of the house to the workshop at the rear of the property. The dwelling is situated on a rise of land (see Image 22). The site's landscape includes remnant agricultural fields that were not under cultivation at the time of the site visit (see Image 21). Vegetation surrounding the dwelling was overgrown and formal plantings were not observed. A stand of mature trees flank the house to the east, west and south.

6.4 Dwelling Exterior

The dwelling is a one-and-a-half storey red brick structure with a side gable roof clad in asphalt shingles. The brick cladding is organized in a Common Bond pattern that is composed of stretchers with a header course every sixth course. The foundation construction material could not be determined due to overgrown vegetation. The north, three-bay façade features a central entryway with a single-pane transom and sidelights flanked by square window openings on either side (see Image 23). The brick voussoirs are obscured, or possibly removed on these openings. The roof line features overhanging eaves and a thick band of vinyl forming a frieze (see Image 24). The ghost of a former chimney is visible in the brick on the west elevation of the original portion of the house (see Image 28). The east and west elevations of the original structure contain two window openings per floor (see Image 25, Image 27). It appears that all original windows and doors are early 21st century vinyl replacements. The windows now appear to be casement. Some of the contemporary units are smaller than the original openings, resulting in the extra space above the windows having been covered with vinyl cladding (see Image 29). The addition of new windows has resulted in the covering or interruption of brick voussoirs over the window openings. This has altered the overall fenestration of the house.

Two, one-storey additions have been constructed extending from the south elevation, the first clad in painted wood board and batten and the rear addition clad in light red brick (see Image

26, Image 27). The additions both include a gable roof as well as square window and door openings.

The few remaining exterior details of the structure that are indicative of its early construction date include its red brick cladding, entryway location and some brick voussoirs (see Image 25, Image 29).

6.5 Garage Exterior

The large detached workshop located to the rear of the house is likely of late-20th or early 21st century construction (see Image 21-Image 24). It exhibits a utilitarian design built to a rectangular plan and clad in aluminum siding. The building features a side gable roof, two garage door access points in the north elevation as well as simple rectangular window and door openings.

6.6 Shed Exterior

A simple, utilitarian shed clad in aluminum siding with a sloped roof is situated on the west property line immediately adjacent to the dwelling (see Image 33).

6.7 Architectural Style/Design

The dwelling at 5104 Ninth Line is built in the Neoclassical architectural style (City of Mississauga 2012:4). The Neoclassical style typically describes “both residential and commercial buildings constructed on the traditional Georgian plan, but... [with] a new gaiety and light-heartedness. Detailing became more refined, delicate, and elegant” (Kyles 2017). When in vogue, Neoclassicism was thought to be an “advanced” style, as it was “popularly felt to be a fashionable refinement, and thus an improvement of the earlier Georgian look” (Fram 2003:24). Neoclassical architecture is similar to the Georgian style in symmetry but is often more delicate including detailing around door and window openings (UW 2009:5). The style was common between 1810-1850 (UW 2009:5; Kyles 2017).

The Neoclassical style was a direct result of the War of 1812 (Kyles 2017). Upper Canadians returning from the war were second or third generation Loyalists who had inherited land from their families and now had the opportunity to buy land and construct residences in Ontario. Pattern books from England illustrating Classical detailing were available and influenced many residential designs, notably introducing fine door and window ornaments (Kyles 2017).

The style is generally expressed in two or three storeys, with a three-bay façade, hip or end gable roofs with matching chimneys and a centred entrance decorated with pilasters and sidelights, transoms and/or fan lights around a single door (UW 2009:5). Many Neo-Classical buildings were built of brick and their detailing is always white (Kyles 2017). Exterior finishes and details can vary significantly across the province. Examples of Neoclassical architecture observed in the City of Mississauga represent modest, vernacular interpretations of the style likely designed by local builders.

The red brick cladding, entryway location and some brick voussoirs are the only remaining features reflective of a Neoclassical style dwelling. The dwelling has undergone extensive modifications including the replacement of windows and doors, as well as modification of their openings, the roof, chimney removal and rear additions. These modifications have impacted elements of the structure’s original design such that it is no longer a representative example of

the Neoclassical style. Constructed in 1848, the house is also not an early example of the style, which can date to 1810. Neoclassical style houses are found in the City of Mississauga, indicated by the style's inclusion in the City's architectural style guide, *Architectural Styles in Mississauga* (2012). Other more representative examples of the style are found in the City of Mississauga at 27 Mill Street and 1620 Orr Road (City of Mississauga 2012:4; see Image 36). These examples retain both their massing and fenestration as well as their detailing.

7.0 HERITAGE ASSESSMENT

Based on Section 3.0 and 5.0, it does not appear that 5150 Ninth Line possesses potential cultural heritage value or interest. An evaluation of the property at 5104 Ninth Line according to O. Reg. 9/06 can be found in Table 3 below.

Table 3: Evaluation of 5104 Ninth Line Using O. Reg. 9/06

Evaluation of Property			
Criteria	Description	✓	Value Statement(s)
Design or Physical Value	Is a rare, unique, representative or early example of a style, type, expression, material or construction method		The red brick cladding, entryway location and some brick voussoirs of 5104 Ninth Line are reflective of the Neoclassical architectural style. However, modifications have impacted the structure's architectural features such that it is no longer a representative example of the Neoclassical style. Other more representative examples of the style are found in the City of Mississauga at 27 Mill Street and 1620 Orr Road. These examples retain both their massing and fenestration as well as their detailing. Constructed in 1848, the house is also not an early example of the style, which can date to 1810.
	Displays a high degree of craftsmanship or artistic value		5104 Ninth Line does not display a high degree of craftsmanship or artistic value.
	Displays a high degree of technical or scientific achievement		5104 Ninth Line does not display a high degree of technical or scientific achievement.
Historical or Associative Value	Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community		5104 Ninth Line is not directly associated with a theme, event, belief, person, activity, organization or institution that is significant to a community. The O'Haras built the farmhouse, established the farmstead and lived on the property for 100 years before selling it. It was then severed and subdivided in the mid-1900s. Although the property is associated with the O'Haras, an early farming family, research conducted did not suggest any notable contributions to the community made by the individuals who lived on this property.
	Yields or has the potential to yield information that contributes to the understanding of a community or culture		5104 Ninth Line does not yield information that contributes to the understanding of a community or culture.
	Demonstrates or reflects the work or ideas of an architect, builder, artist, designer or theorist who is significant to a community		5104 Ninth Line does not demonstrate or reflect the work or ideas of an architect, builder, artist, designer or theorist who is significant to a community. The research conducted did not locate an individual or firm associated with the construction of the building.

Contextual Value	Is important in defining, maintaining or supporting the character of an area	5104 Ninth Line is not important in defining, maintaining or supporting the character of an area. The rural character of the area has been diminished over time through the introduction of contemporary suburban development, the construction of Highway 407 and the ongoing proposed development of the Ninth Line Lands.
	Is physically, functionally, visually or historically linked to its surroundings	5104 Ninth Line is not physically, functionally, visually or historically linked to its surroundings. The adjacent property at 5150 Ninth Line is contemporary and buildings to the east have been demolished. Contemporary suburban development is located to the north and Highway 407 is located to the south.
	Is a landmark	5104 Ninth Line is not a landmark.

5104 Ninth Line does not meet the criteria of O. Reg. 9/06. As such, it does not merit designation under Part IV of the *Ontario Heritage Act*.

It should be noted that this evaluation was completed without Permission to Enter the property and without an interior investigation. It is possible that a closer examination of the building may result in a revised evaluation.

8.0 PROPOSED DEVELOPMENT

8.1 Description of Proposed Development

The materials provided by Mattamy (5150 Ninth Line) Ltd. detailing the South Ninth Line development included a site survey plan (see Figure 1-Figure 2), a topographic map showing existing building locations (see Map 5), the site plan (see Figure 3), preliminary building elevations for each proposed product type (see Appendix C, Figure 8-Figure 39), and a plan and section indicating the distance between the development and the property at 5104 Ninth Line (see Figure 6-Figure 7). The developable area is approximately 3.72 ha (excluding the transit corridor, 14 m Ministry of Transportation setback, 10 m buffer and public road) and the site density is proposed at 47.8 units per hectare with 42 visitor parking spaces (Mattamy 2020b). The concept plan provided by Mattamy (5150 Ninth Line) Ltd. describes the proposed development to consist of 178 units (all attached product – dual frontage towns, front load towns, and back-to-back towns (village homes)). Private amenity space is proposed in the southeast quadrant of the site, totalling 668 m sq. All roads are planned to be condo with the exception of the 20 m L-shaped right-of-way (Street “A”) that connects Ninth Line and the development to the east (“Your Home Developments”). Most of the units will be condo with the exception of those fronting onto the public road (Street “A”). The 20 m public road is shown to align just south of the subject property on the concept plan, following the property line (see Figure 3).

The distance from the development (rear of the “front load towns”) to the property line of 5104 Ninth Line includes a proposed 7.3 m wide rear yard area (see Figure 6-Figure 7) (NAK 2020a). The total distance from the property line to the subject property is 4.3 m, representing a total separation of 11.6 m between the new development and 5104 Ninth Line (NAK 2020a). A privacy fence 1.8 m in height is proposed to delineate the property boundary between the two properties (see Figure 7).

8.2 Purpose and Rationale for Proposed Development

The materials that detail the proposed South Ninth Line development outline the construction of medium-density residential buildings with dual frontage town (17 units, see Appendix C, Figure 8-Figure 12), front load town (93 units, see Appendix C, Figure 13-Figure 26) and back-to-back town (village home) (68 units, see Appendix C, Figure 27-Figure 39) condominiums located along the south side of Ninth Line. Upon completion of the development, it will result in a “mix of housing to accommodate residents at all stages in life” (Mattamy 2018). Section 8.3 provides the land use planning context that offers a further understanding of the rationale for the proposed project.

8.3 Land Use Planning Context

The property located at 5104 Ninth Line was originally located within the boundaries of the Town of Milton. On January 1, 2010 lands situated east of Highway 407, including the subject property, were annexed to the City of Mississauga (City of Mississauga 2009).

The City of Mississauga’s Official Plan notes the responsibility of the City and Province to conserve and protect cultural heritage resources (City of Mississauga 2019a:7-7). However, Section 7.4.1.12, states that, “The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction” (City of Mississauga 2019a:7-8).

The Ninth Line Neighbourhood has been identified by the City as “mostly underdeveloped” (City of Mississauga 2017b:1). The City of Mississauga has a vision for the Ninth Line Neighbourhood to become “sustainable, transit-supportive, connected and distinct” (City of Mississauga 2017b:3). Extensive public and stakeholder engagement were involved in the creation of this vision and urban design guidelines for Ninth Line.

Under the City of Mississauga’s Official Plan, the area is designated as a Special Study Area (City of Mississauga 2019a). The City of Mississauga’s *Shaping Ninth Line Urban Design Guidelines* (2017b) calls for Residential Medium Density of three to six storeys for the study area (City of Mississauga 2017b:7-8; see Figure 4, Figure 5). The new streetscape, as per the *Guidelines*, would include a range of housing types. This may include apartments and condominiums, as well as townhouse forms (City of Mississauga 2017b:27). The proponent notes that their concept is in keeping with the *Guidelines*, and that this vision would protect the natural heritage system and stable neighbourhoods to the east, while directing compact mixed-use development where it will be transit supportive (Mattamy 2018).



Figure 3: Site Plan for 5150 Ninth Line
(Mattamy 2020a)

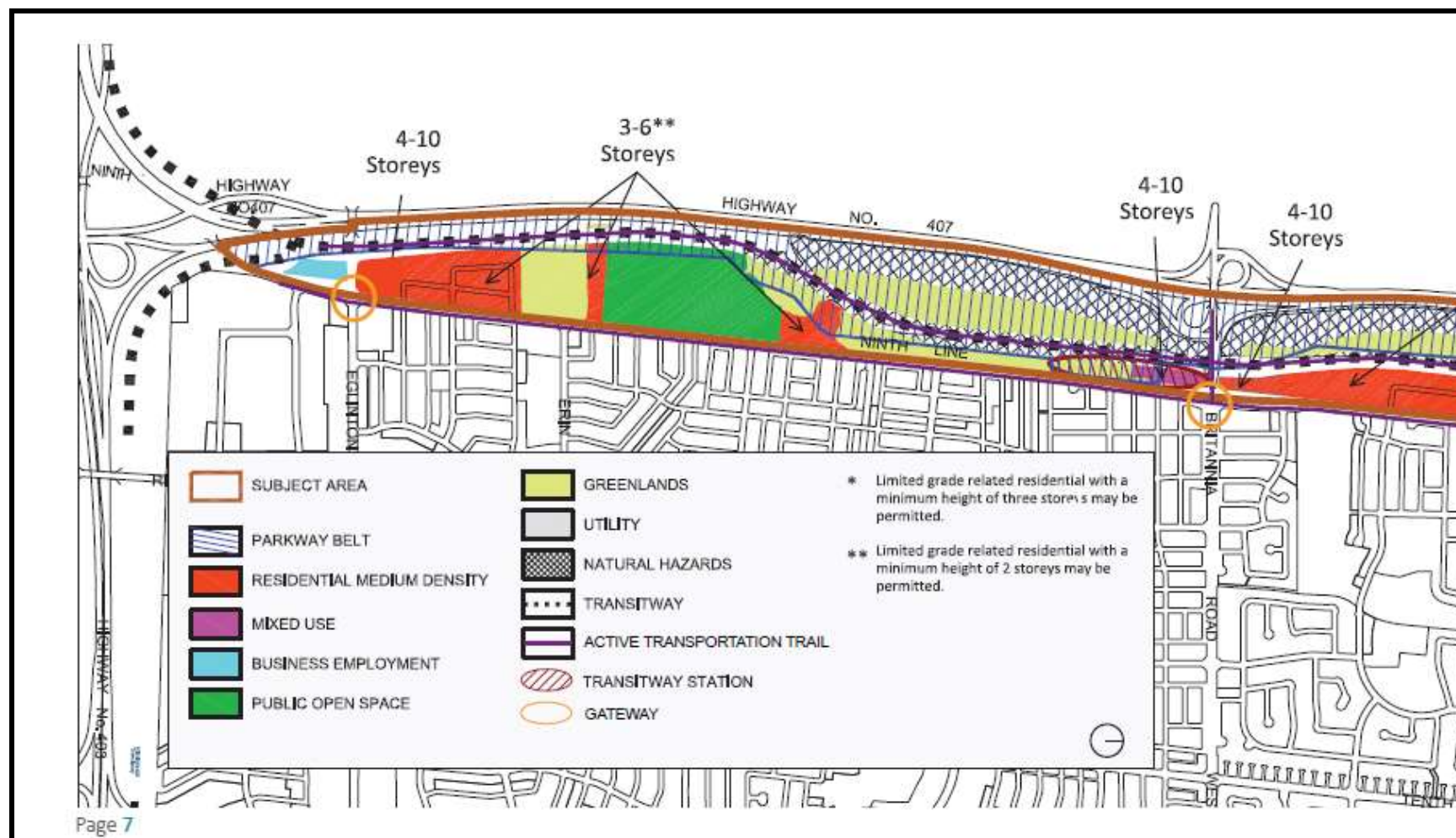


Figure 4: Concept from City of Mississauga's *Shaping Ninth Line Urban Design Guidelines*
(City of Mississauga 2017b:7)



Figure 5: Private Realm Design Guidelines from City of Mississauga's *Shaping Ninth Line Urban Design Guidelines* (City of Mississauga 2017b:2)

9.0 ANALYSIS OF POTENTIAL IMPACTS

The proposed development entails the demolition of the existing dwelling and outbuildings at 5150 Ninth Line and the construction of approximately 178 condominium units in medium-density residential buildings to be located along the south side of Ninth Line. The proposed buildings are all attached homes including dual frontage towns, front load towns, and back-to-back townhouses (village homes). **The adjacent subject property at 5104 Ninth Line will be retained *in situ*.** The following analysis of project impacts is based upon the drawings and development description provided by Mattamy (5150 Ninth Line) Ltd. as outlined in Section 8.0. On the following page, Map 5 overlays the concept plan for the proposed development on a current aerial image to provide context to the assessment of impacts resulting from the development on the landscape and extant structures in the study area. Figure 6 and Figure 7 further illustrate the proximity of the proposed development to the subject property.

Based on Section 3.0 and 5.0, it does not appear that 5150 Ninth Line possesses potential cultural heritage value or interest. Although the property at 5104 Ninth Line was not found to meet the criteria of O. Reg. 9/06, it is an historic farmhouse, as such development considerations sympathetic to the existing historic fabric can be suggested. The best mitigation option for historic properties is generally retention. Retention allows cultural heritage resources to be retained in their original location and encourages adaptive re-use and sympathetic or compatible development (i.e., mass, setback, setting and materials). **The adjacent property at 5104 Ninth Line will be retained *in situ*, as such no direct impacts are anticipated.**

With the implementation of the development's land use concept and plan, and the destruction of the extant building at 5150 Ninth Line, **the property would no longer be a remnant agricultural landscape and the adjacent subject property's context would be impacted.** However, due to the presence of Highway 407 to the rear (south) of the property and a suburban subdivision to the north, the lands adjacent to 5150 and 5104 Ninth Line are no longer within an agricultural context. As a result, both properties have lost any contextual value they may have possessed in the past.

The current design concept does not propose a significant buffer between the rear of the planned residential units ("front load towns") and the adjacent property at 5104 Ninth Line. The extant and proposed structure will be separated by 11.6 m, including a rear yard distance of 7.3 m and 4.3 m from the property line to the subject property. **Although a limited buffer is provided, no direct impacts are anticipated to affect 5104 Ninth Line.**

Indirect impacts such as shadows cast by the rear lane townhouses on 5104 Ninth Line were evaluated through a revised Shadow Impact Study prepared by NAK design strategies in May 2020. Several shadow models were illustrated for time increments (local time EDT) on June 21 (5:38 to 21:04), September 21 (7:05 to 19:17) and December 21 (7:48 to 16:46) and were used to assess incremental shadow impacts. The diagrams demonstrate that most shadows will remain within the project location while shifting throughout the day. As a result, the Shadow Impact Study found that the proposed front load towns will have minimal impacts on the subject property, with shadows cast on the dwelling by 19:21 on June 21 and by 17:47 on September 21 (NAK 2020b). No shadow impacts were modeled in December. It should be noted that there are currently mature trees along the lot line casting shadows on the house. It is possible that no new shadows or less shadows will be cast if the mature vegetation is removed.

Upon review of the preliminary elevation drawings circulated by Mattamy (5150 Ninth Line) Ltd. for the proposed townhouse units, visual indirect impacts to the adjacent property at 5104 Ninth

Line are anticipated (Q4A 2020). The architectural design, construction materials and colour palate of the townhouses are not reflective of, or sympathetic to, the design of the neighbouring dwelling at 5104 Ninth Line.



Map 5: Overlay of Site Plan for 5150 Ninth Line on Current Aerial Imagery
(Produced by ARA under licence using ArcGIS® software by Esri, © Esri)

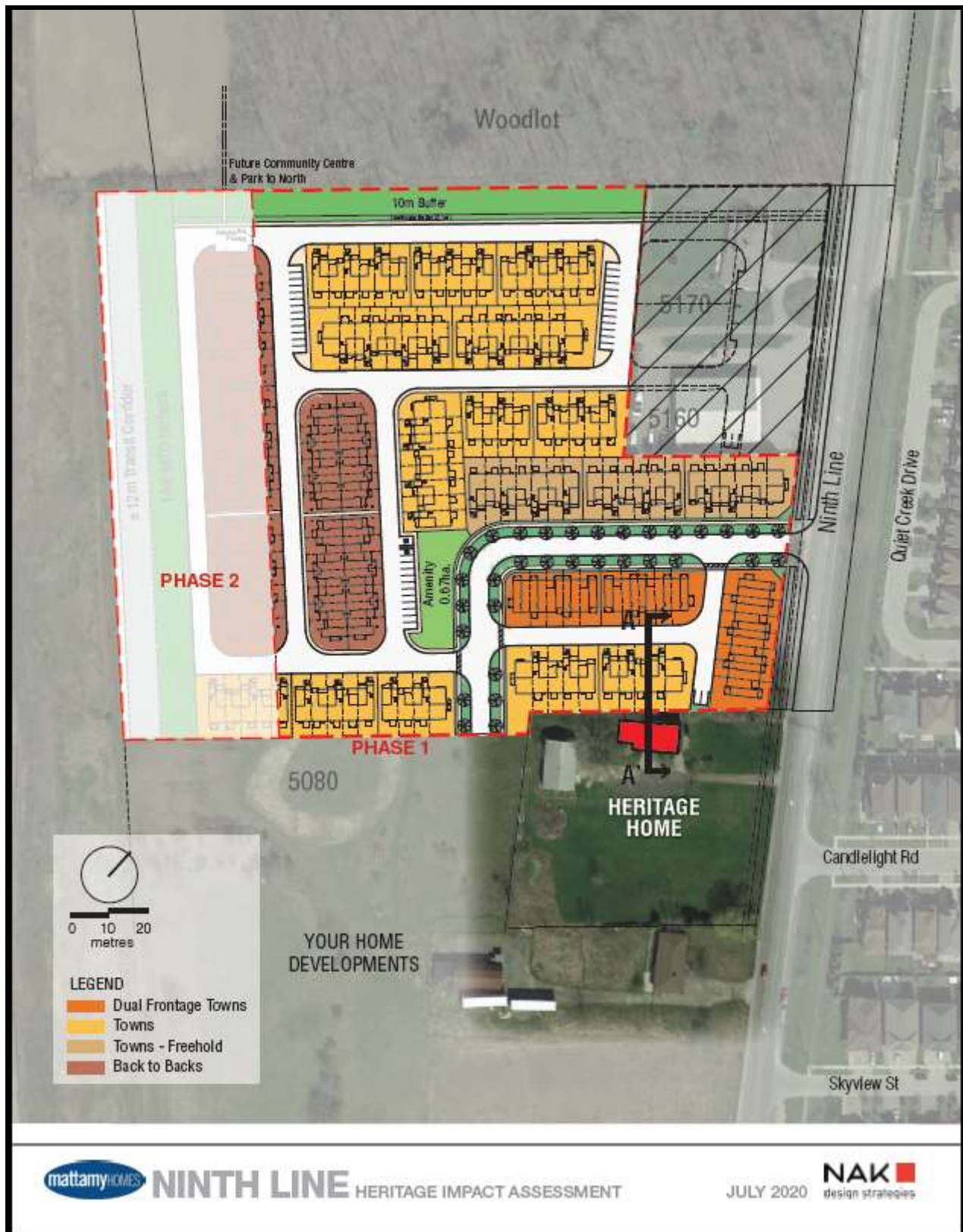


Figure 6: Proximity of Proposed Development to 5104 Ninth Line
(NAK 2020a)

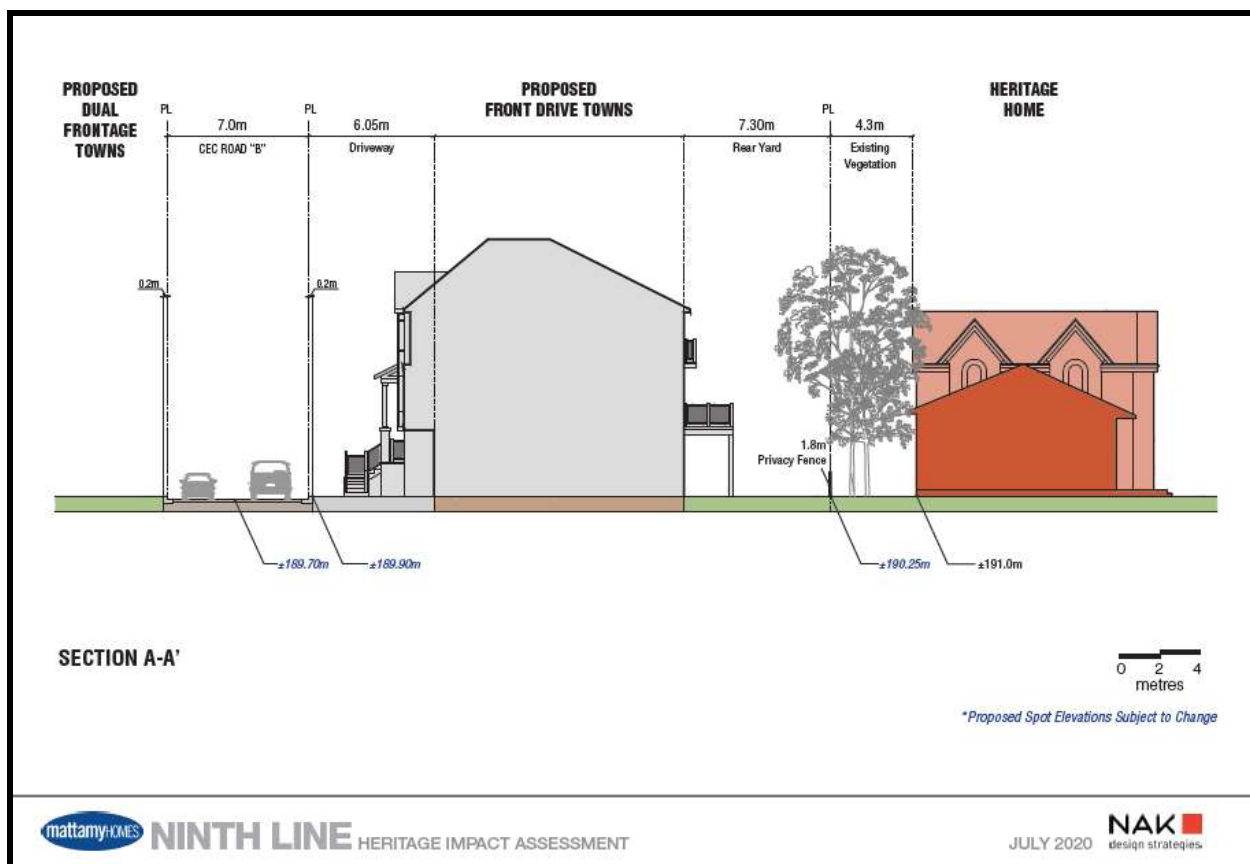


Figure 7: Section Showing Proximity of Proposed Development to 5104 Ninth Line (NAK 2020a)

10.0 ASSESEMENT OF MITIGATION MEASURES

Mattamy (5150 Ninth Line) Ltd. has examined different development alternatives and mitigation measures as outlined below. These alternatives and mitigation measures address the approaches outlined in *InfoSheet #5: Heritage Impact Assessments and Conservation Plans* (MLC 2006b) and the City of Mississauga *Heritage Impact Assessment Terms of Reference* (2017a). The feasibility of each of the mitigation suggestions is described based on materials provided by the client and considers the character of the subject property as outlined in Section 7.0. **Due to the adjacency of the subject property to the project location, mitigation measures focus on ensuring that the concept plan and townhouse designs for the proposed development are visually sympathetic with the property at 5104 Ninth Line.**

10.1 Previous Design Alternative

The previous concept that ARA evaluated (dated May 10, 2019 and submitted to the City of Mississauga) included a more significant buffer between the project location and the property at 5104 Ninth Line. The distance from the front of the proposed “rear lane towns” to the subject property was approximately 18 m, which included a 2.8 m front yard, an 11 m wide private road with a 2.6 m wide lay-by parking area, a 2 m sidewalk and a 4.2 m landscape buffer (W. Coutinho, Personal Communication Oct. 28, 2019). Although this earlier concept provided greater building separation, it proposed a local traffic route within 4.2 m of the property line (W. Coutinho, Personal Communication Oct. 28, 2019).

Due to additional property constraints, the concept plan was revised, removing the open space between the project location and the subject property and siting a row of front load towns directly adjacent to the dwelling at 5104 Ninth Line. Mattamy (5150 Ninth Line) Ltd. described that their rationale for revising the concept plan was primarily driven by two factors:

“First, due to intersection spacing requirements, Mattamy was advised to move the intersection of Street “A” and Ninth Line north so not to conflict with other Ninth Line intersections. By moving the intersection north Mattamy needed to site product that would back onto the heritage property.

The second issue was that the City of Mississauga has required that Street “A” be a public right-of-way. This requirement reflects the City’s desire for better traffic and possible transit through Mattamy’s lands and the parcels to the south. Due to right-of-way widths, spacing and product layout, the current design resulted in an enclave of approximately 23 townhomes to be located adjacent to the heritage property at 5104 Ninth Line” (C. Scarlett, Personal Communication October 22, 2019).

The second concept plan that ARA evaluated in the scoped HIA (dated October 23, 2019) included a narrower buffer between the project location and the property at 5104 Ninth Line. On the new July 2020 site plan, the distance from the development (rear of the “front load towns”) to the property line of 5104 Ninth Line has enlarged the rear yard area slightly from 5.8 m to 7.3 m (see Figure 6-Figure 7) (NAK 2020a). The total distance from the property line to the subject property remains at 4.3 m, representing a total separation of 11.6 m and an increase of 1.5 m between the new development and 5104 Ninth Line (NAK 2020a). A privacy fence 1.8 m in height is still proposed to delineate the property boundary between the two properties (see Figure 7). This increase in distance further mitigated the already minor evening shadows modeled to be cast on the subject property as a result of the new development (see Section 9.0).

In addition to the increased setback adjacent to the subject property, the revised July 2020 site plan increased the unit count from 119 to 171 and; changed visitor parking spaces and amenity area calculations; increased visitor parking spaces from 31 to 42; added units and driveways on the lots as well as block numbers and road labels; slightly revised the design of product types; and realigned the public street slightly further south to avoid the subject property and align with the proposed roadway in the development (“Your Home Developments”) planned to the east (Mattamy 2020b).

10.2 Buffers

The creation of buffer zones is noted as a method of minimizing or avoiding negative impacts to a property and should be incorporated into the concept plan for the South Ninth Line development (MLC 2006b:4). Shifting the placement of amenity space from the centre of the development to the east side of the lot buffering the property at 5104 Ninth Line was suggested by ARA. Mattamy (5150 Ninth Line) Ltd. indicated that this was not feasible, noting that, “Due to the size and shape of the site, there are many constraints...The amenity area is placed centrally so it’s easily accessible by all residents” (D. Hegarty, Personal Communication July 29, 2019). As a result, the current July 2020 site plan has incorporated amenity space in the southeast quadrant of the proposed development. As the provision of a buffer of open space between properties was not considered a viable design alternative, the following considerations are suggested to ensure that the development is designed in a manner that is visually sympathetic with the subject property at 5104 Ninth Line.

To ensure the adequate screening of the property at 5104 Ninth Line based on the current concept plan, it is recommended that the proposed options presented by Mattamy (5150 Ninth Line) Ltd., such as the erection of a wood privacy fence, the conservation of extant mature trees, and the planting of rows of vegetation be implemented along the east side of the development, adjacent to the subject property. The introduction of additional screening may enhance the views from the historic property to the new development, thus mitigating a portion of potential visual impacts. A number of mature trees are currently growing along the north and east property line of the project location and their retention and incorporation into the development's concept plan is recommended to buffer the development from 5104 Ninth Line as well as to maintain a vestige of the formerly rural context of the study area. To ensure the adequate screening of the property at 5104 Ninth Line, a landscape architect or arborist should be retained to investigate the feasibility of conserving mature trees along the east side of the development, directly adjacent to the subject property.

10.3 Architectural Design Elements

The current preliminary elevations for the South Ninth Line development indicate that front load towns (see Appendix C, Figure 13-Figure 26) will be constructed immediately adjacent to the subject property, with their rear elevations oriented to the east facing the extant dwelling (Q4A 2020). Slight modifications to the designs of all of the proposed product types, specifically the rear elevation of the front load towns (see Figure 14, Figure 17, Figure 20, Figure 23), is recommended to draw from and reflect the proportions, materials and design elements reflective of 5104 Ninth Line, notably the brick cladding, brick voussoirs, medium pitched side gable roof, square window and door openings, and centrally placed front entrance with sidelights and/or transom.

10.4 Colour Palate

To reflect the adjacent structure at 5104 Ninth Line, red brick and white design elements should be considered for incorporation into the townhouse designs.

10.5 Construction Fencing

During construction, it is recommended that construction fencing between the property at 5104 Ninth Line and the project location be erected at a sufficient distance to ensure that there will be no direct or indirect impacts to the building. The fencing should be maintained throughout the duration of the construction period.

11.0 SUMMARY STATEMENT AND CONSERVATION RECOMMENDATIONS

Research, consultation and field work completed for this report found that 5150 Ninth Line did not possess potential cultural heritage value or interest. 5104 Ninth Line is a listed property on the *City of Mississauga's Municipal Register of Property of Cultural Heritage Value or Interest*. Following consultation, historical research, field survey and evaluation against the criteria set out in O. Reg. 9/06 of the *OHA*, the property at 5104 Ninth Line was not found to possess CHVI. Although the property was not found to meet the criteria of O. Reg. 9/06, it is an historic farmhouse, as such development considerations sympathetic to the existing historic fabric have been suggested.

As outlined in Section 8.0, the proposed development will have indirect impacts on the adjacent heritage property. The development entails the demolition and removal of the existing dwelling and outbuildings at 5150 Ninth Line and the construction of approximately 178 condominium units in medium-density residential buildings to be located along the south side of Ninth Line. The proposed buildings are all attached homes including dual frontage towns, front load towns, and back-to-back townhouses (village homes).

Mitigation measures have been suggested for the development at 5150 Ninth Line that consider the existing historic fabric of the adjacent property at 5104 Ninth Line:

- That slight modifications to the designs of all of the proposed product types be considered, specifically the rear elevation of the front load towns (proposed directly adjacent to the subject property), to draw from and reflect the proportions, materials and design elements reflective of 5104 Ninth Line, notably the red brick cladding, brick voussoirs, medium pitched side gable roof, square window and door openings, and centrally placed front entrance with sidelights and/or transom.
- That to better reflect the adjacent property at 5104 Ninth Line, red brick and white design elements should be considered in the colour palate of each proposed product type.
- That during construction, it is recommended that construction fencing be erected between the property at 5104 Ninth Line and the project location at a sufficient distance to ensure that there will be no direct or indirect impacts to the building. The fencing should be maintained throughout the duration of the construction period.
- That to ensure the adequate screening of the property at 5104 Ninth Line, it is recommended that the proposed options presented by Mattamy (5150 Ninth Line) Ltd., including the erection of a wood privacy fence, the conservation of extant mature trees on the subject property and the planting of rows of vegetation be implemented along the east side of the development, directly adjacent to 5104 Ninth Line.
- That a landscape architect or arborist should be retained to investigate the feasibility of conserving mature trees along the east side of the development, directly adjacent to the subject property to further ensure adequate screening.

The 2020 *Provincial Policy Statement* notes that CHVI is bestowed upon cultural heritage resources by communities (MMAH 2020). Accordingly, the system by which heritage is governed in this province places emphasis on the decision-making of local municipalities in determining CHVI. It is hoped that the information presented in this report will be useful in those deliberations.

12.0 MANDATORY STATEMENT

Based on the results of consultation, historical research, field survey and assessment of the study area, 5104 Ninth Line was evaluated against the criteria in O. Reg. 9/06 and was not found to meet any of the criteria.

12.1 Design or Physical Value

The dwelling at 5104 Ninth Line was built in the Neoclassical style (City of Mississauga 2012:4). Constructed in 1848, the structure is not an early example of the style, which was common in the period between 1810-1850. Neoclassical architecture typically describes “both residential and commercial buildings constructed on the traditional Georgian plan, but ...[with] a new gaiety and light-heartedness. Detailing became more refined, delicate, and elegant” (Kyles 2017).

When in vogue, Neoclassicism was thought to be an “advanced” style, as it was “popularly felt to be a fashionable refinement, and thus an improvement of the earlier Georgian look” (Fram 2003:24). Neoclassical architecture is similar to the Georgian style in symmetry but is often more delicate including detailing around door and window openings (UW 2009:5). The red brick cladding, entryway location and some brick voussoirs of 5104 Ninth Line are reflective of the Neoclassical architectural style. The dwelling has undergone extensive alterations including the replacement of windows (including the modification of their openings), doors, the roof, chimney removal and rear additions. These modifications have impacted elements of the structure’s original design such that it is no longer a representative example of the Neoclassical style.

12.2 Historical or Associative Value

In 1830, the Crown Patent for the east half of Lot 9, Concession 1 originally in the Township of Trafalgar, Halton County went to Charles O’Hara, who then sold the property to his son, Charles O’Hara Jr. The O’Hara family were Irish Catholic farmers that formed part of an Irish Catholic settlement in the area. According to the *Census of 1861*, Charles O’Hara Jr. and his wife Mary lived in a one-and-a-half storey brick house, the subject building, at 5104 Ninth Line with their nine children. After O’Hara Jr.’s death, his wife Mary and son William owned and occupied the property until 1875 when Mary sold the land to her youngest sons, William and Michael. According to the 1901 census, Michael O’Hara and his wife Catherine lived on the property in a nine-room brick house until his death in 1928. Catherine continued to live on the property until her passing in 1948 after which the property changed hands many times until it was sold to Robert Turnbull in 1994. Although the subject property is associated with the O’Haras, a long-time farming family in the area, currently available resources do not suggest any notable contributions to the community made by members of the family.

12.3 Contextual Value

The house, accessed via a driveway, is set back from Ninth Line on a rise of land. While the dwelling was originally part of a farming complex, the associated barn and other farm structures have been removed. The rural character of the area has been diminished overtime with the introduction of contemporary suburban development, Highway 407 and the ongoing proposed development of the Ninth Line Lands.

12.4 Conclusion

In summary, alterations made to the dwelling at 5104 Ninth Line have impacted its architectural elements such that it is no longer a representative example of the Neoclassical style. In addition, it is not an early example of the style. The property is associated primarily with the O’Haras, a long-time farming family in the area. Research conducted did not suggest any notable contributions to the community made by the individuals who lived on the property. The contextual value of the property has been diminished through adjacent and proposed contemporary suburban development. As such, the property does not meet any of the criteria included in O. Reg. 9/06 and does not merit designation under Part IV of the *Ontario Heritage Act*.

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Appendix A: 5150 and 5104 Ninth Line Images



Map 6: Photo Location Map of 5150 and 5104 Ninth Line
(Produced by ARA under licence using ArcGIS® software by Esri, © Esri)



Image 1: Contextual View to 5150 Ninth Line from Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 2: Contextual View to 5150 Ninth Line from the South
(Photo taken on July 29, 2019; Facing North)



Image 3: Façade of 5150 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 4: Northeast corner of 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southwest)



Image 5: Southeast corner of 5150 Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 6: South Elevation of 5150 Ninth Line
(Photo taken on July 29, 2019; Facing North)



Image 7: Southwest corner of 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Northeast)



Image 8: Façade of Garage at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 9: Northeast Corner of Garage at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing East)



Image 10: Northwest Corner of Garage at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southeast)



Image 11: Northeast Corner of Workshop at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 12: East Elevation of Workshop at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 13: Mature Trees on North End of Property at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Northwest)



Image 14: Mature Trees on North End of Property at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southeast)



Image 15: Contextual View of Agricultural Portion of Property Adjacent to Residence at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southeast)



Image 16: Contextual View of Agricultural Portion of Property Adjacent to Residence at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southwest)



Image 17: Contextual View of South Portion of Property at 5150 Ninth Line Bound by Highway 407
(Photo taken on July 29, 2019; Facing Southeast)



Image 18: Contextual View of South Portion of Property at 5150 Ninth Line Looking Towards Cell Phone Tower
(Photo taken on July 29, 2019; Facing Southwest)



Image 19: Access Road to Cell Phone Tower Running North-South on West Side of Property at 5150 Ninth Line
(Photo taken on July 29, 2019; Facing Southwest)



Image 20: Contextual View to 5104 Ninth Line from Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 21: Contextual View to 5104 Ninth Line from 5150 Ninth Line
(Photo taken on July 29, 2019; Facing East)



Image 22: Contextual View of 5104 Ninth Line from Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 23: Façade of 5104 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 24: Detailed View of Cladding, Window and Roofline, 5104 Ninth Line
(Photo taken on July 29, 2019; Facing Southeast)



Image 25: Northeast Corner of 5104 Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 26: South Elevation of 5104 Ninth Line
(Photo taken on July 29, 2019; Facing North)



Image 27: West Elevation of 5104 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 28: Detail of Chimney Ghost, 5104 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 29: Detail of first storey window opening
(Photo taken on July 29, 2019; Facing South)



Image 30: Northwest Corner of 5104 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 31: Northeast Corner of Workshop at 5104 Ninth Line
(Photo taken on July 29, 2019; Facing West)



Image 32: West Elevation of Workshop at 5104 Ninth Line
(Photo taken on July 29, 2019; Facing East)



Image 33: Northwest Corner of Shed at 5104 Ninth Line
(Photo taken on July 29, 2019; Facing South)



Image 34: Contextual View of Study Area Along Ninth Line
(Photo taken on July 29, 2019; Facing Northwest)



**Image 35: Contextual View of Suburban Residential Development North of Study Area
Across Ninth Line**
(Photo taken on July 29, 2019; Facing North)

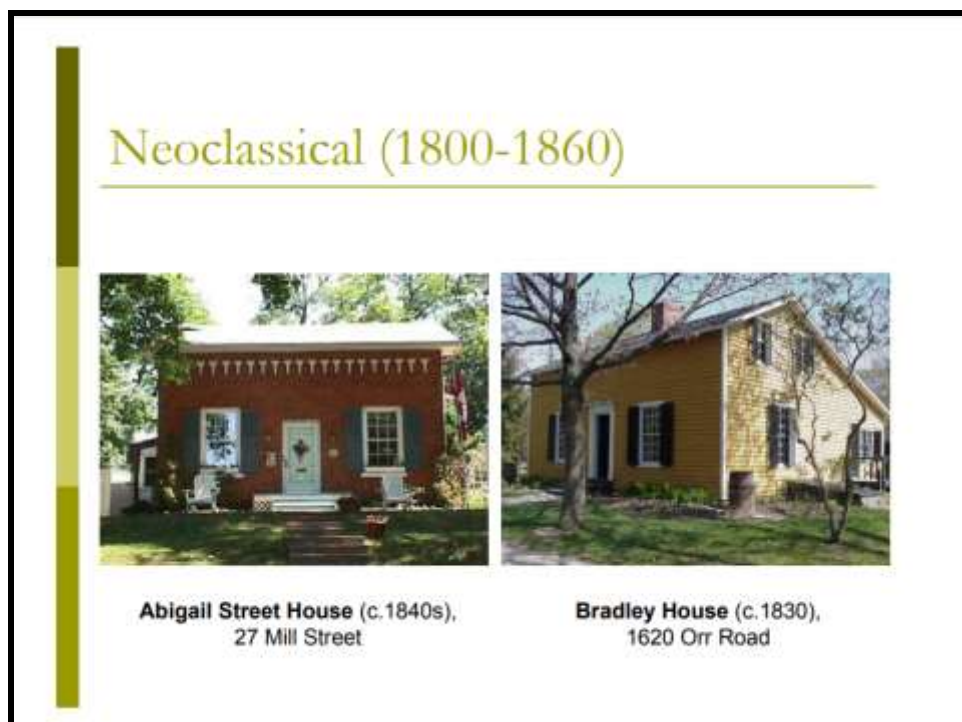


Image 36: Examples of Neoclassical Architectural Style in City of Mississauga
(City of Mississauga 2012:4)

Appendix B: Historical Photos, Drawings and Other Archival Material

**Table 4: Ownership History of 5104 Ninth Line
(LRO 20)**

Instrument Number	Instrument	Date	Grantor	Grantee	Comments
	Patent	30 Apr 1830	Crown	Charles O'Hara	E ½, 100 acres
662	B&S	13 Jul 1830	Charles O'Hara Sr.	Charles O'Hara Jr.	E ½, 100 acres
813	B&S	24 Dec 1861	Heirs of Charles O'Hara	Mary O'Hara	E ½, 100 acres
3356	B&S	15 May 1875	Mary O'Hara	Michael and William O'Hara	E ½, 100 acres
3941	Quit Claim	15 Nov 1880	William O'Hara	Michael O'Hara	E ½, 100 acres
4024	B&S	12 Mar 1884	Michael O'Hara	William O'Hara	E ½, 100 acres
4415	B&S	26 Aug 1885	William O'Hara	Michael O'Hara	E ½, 100 acres
3343	Probate of Will	2 Aug 1928	Michael O'Hara		E ½, 100 acres
22500	Grant	9 Jul 1946	Executors of Michael O'Hara	[Toyne] Grice	E ½, 100 acres
28611	Grant	4 Aug 1953	Toyne Grice	Director of the Veteran's Land Act	
133411	Amalgamation	18 Oct 1962	Re: Oakville – Trafalgar		
370555	Grant	1 Aug 1973	Director of the Veteran's Land Act	Allan and Catherine St. John	
370556	Grant	13 Aug 1973	Allan and Catherine St. John	Joseph and Nicole Burak	Pt. E ½
541656	Deed	15 Jun 1981	Joseph and Nicole Burak	Michael and Renata Pustina, Ivan and Zlata Rozic; joint tenants	Pt. E ½
639747	Grant	29 Apr 1986	Michael and Renata Pustina, Ivan and Zlata Rozic	Robert and Deborah Turnbull	
828362	Transfer	9 Sep 1994	Deborah and Robert Turnbull	Robert Turnbull	Pt. E ½

**Table 5: Ownership History of 5150 Ninth Line
(LRO 20)**

Instrument Number	Instrument	Date	Grantor	Grantee	Comments
	Patent	30 Apr 1820	Crown	Charles O'Hara	E ½, 100 acres
662	B&S	13 Jul 1830	Charles O'Hara Sr.	Charles O'Hara Jr.	E ½, 100 acres
813	B&S	24 Dec 1861	Heirs of Charles O'Hara	Mary O'Hara	E ½, 100 acres
3356	B&S	15 May 1875	Mary O'Hara	Michael and William O'Hara	E ½, 100 acres
3941	Quit Claim	15 Nov 1880	William O'Hara	Michael O'Hara	E ½, 100 acres
4024	B&S	12 Mar 1884	Michael O'Hara	William O'Hara	E ½, 100 acres
4415	B&S	26 Aug 1885	William O'Hara	Michael O'Hara	E ½, 100 acres
3343	Probate of Will	2 Aug 1928	Michael O'Hara	E ½, 100 acres	
22500	Grant	9 Jul 1946	Executors of Michael O'Hara	[Toyne] Grice	E ½, 100 acres
28227	Grant	8 Jun 1953	Toyne Grice	Gabor Szilagyi	Part E ½
133411	Amalgamation	18 Oct 1962	Re: Oakville – Trafalgar		
367648	Grant to Uses	10 Jul 1973	Gabor Szilagyi	Gary Rynsoever	Grant to uses

Appendix C: Preliminary Building Elevations for Each Product Type



Figure 8: Dual Front Towns (Modern), 4 Unit Block – Front Elevation
(Q4A Architects 2020a)



Figure 9: Dual Front Towns (Modern), 4 Unit Block – Rear Elevation
(Q4A Architects 2020a)



Figure 10: Dual Front Towns (Modern), 4 Unit Block – Side Elevations
(Q4A Architects 2020a)

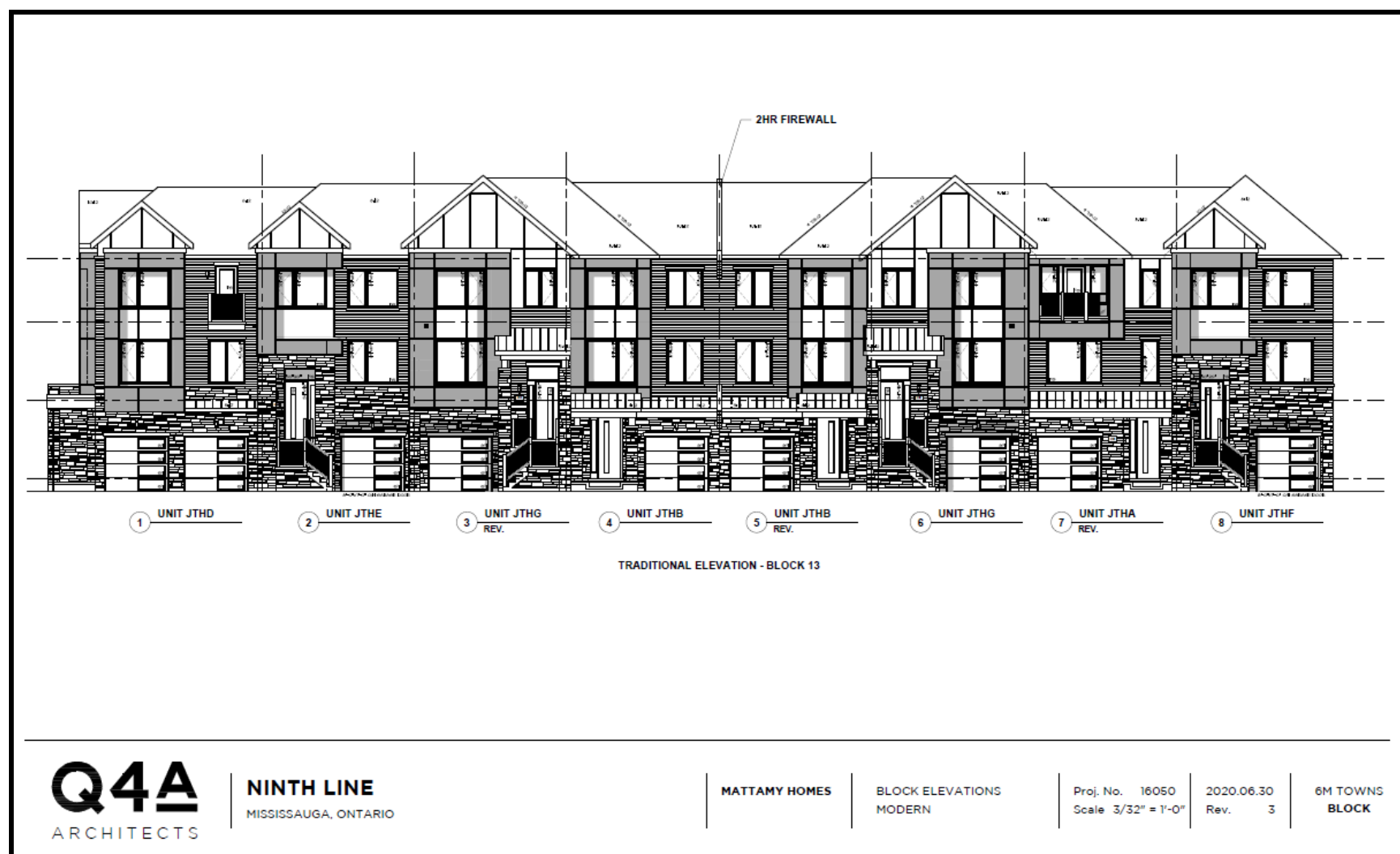


Figure 11: Dual Front Towns (Modern), Block 13 – Front Elevation
(Q4A Architects 2020b)

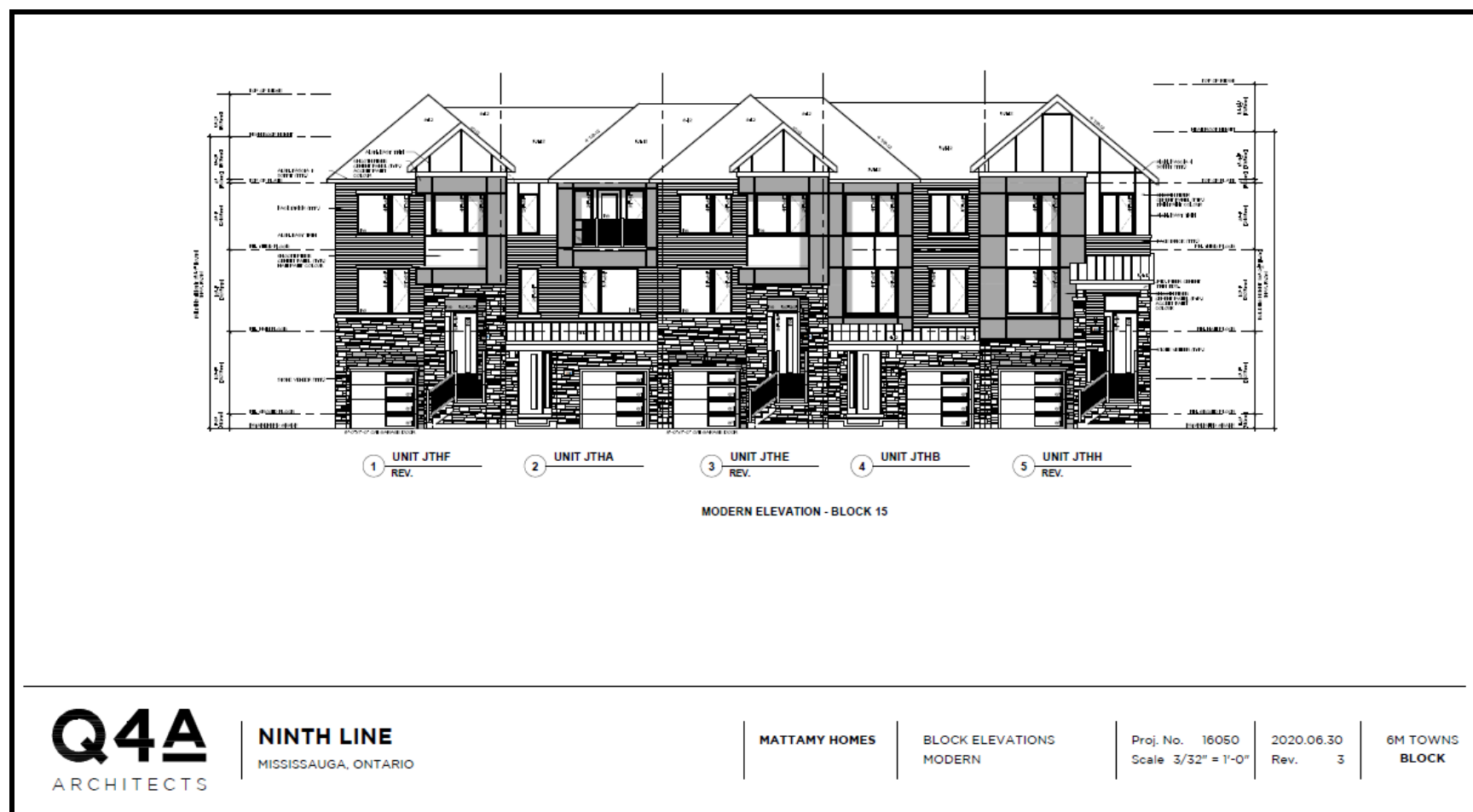


Figure 12: Dual Front Towns (Modern), Block 15 – Front Elevation
(Q4A Architects 2020b)



Figure 13: Front Load Towns (French Manor), Block 1 – Front Elevation
(Q4A Architects 2020a)



Figure 14: Front Load Towns (French Manor), Block 1 – Rear Elevation
(Q4A Architects 2020a)



Figure 15: Front Load Towns (French Manor), Block 1 – Side Elevations
(Q4A Architects 2020a)



Figure 16: Front Load Towns (French Manor), Block 2 – Front Elevation
(Q4A Architects 2020a)



Figure 17: Front Load Towns (French Manor), Block 2 – Rear Elevation
(Q4A Architects 2020a)



Figure 18: Front Load Towns (French Manor), Block 2 – Side Elevations
(Q4A Architects 2020a)



Figure 19: Front Load Towns (Traditional), Block 1 – Front Elevation
(Q4A Architects 2020a)



Figure 20: Front Load Towns (Traditional), Block 1 – Rear Elevation
(Q4A Architects 2020a)



Figure 21: Front Load Towns (Traditional), Block 1 – Side Elevations
(Q4A Architects 2020a)



Figure 22: Front Load Towns (Traditional), Block 2 – Front Elevation
(Q4A Architects 2020a)



Figure 23: Front Load Towns (Traditional), Block 2 – Rear Elevation
(Q4A Architects 2020a)



Figure 24: Front Load Towns (Traditional), Block 2 – Side Elevations
(Q4A Architects 2020a)



Figure 25: Front Load Towns (Traditional), Block 12 – Front Elevations
(Q4A Architects 2020b)

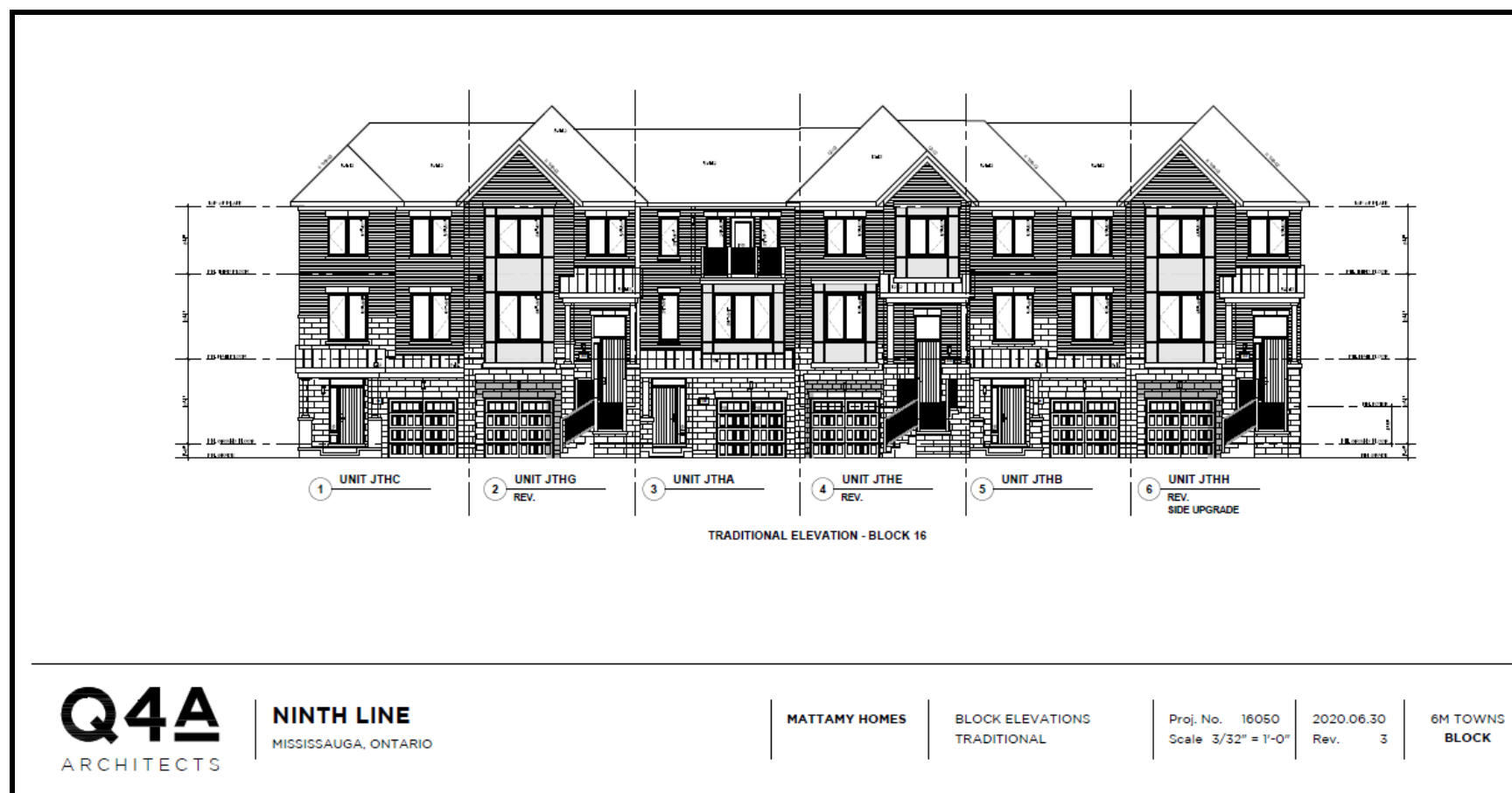


Figure 26: Front Load Towns (Traditional), Block 16 – Front Elevations
(Q4A Architects 2020b)



Figure 27: Village Homes (French Manor), Block 1 – Front Elevation
(Q4A Architects 2020a)



Figure 28: Village Homes (French Manor), Block 1 – Rear Elevation
(Q4A Architects 2020a)



Figure 29: Village Homes (French Manor), Block 1 – Side Elevations
(Q4A Architects 2020a)



Figure 30: Village Homes (French Manor), Block 2 – Front Elevation
(Q4A Architects 2020a)



Figure 31: Village Homes (French Manor), Block 2 – Rear Elevation
(Q4A Architects 2020a)



Figure 32: Village Homes (French Manor), Block 2 – Side Elevations
(Q4A Architects 2020a)



Figure 33: Village Homes (French Manor), Block 14 – Front Elevation
(Q4A Architects 2020b)



Figure 34: Village Homes (Traditional), Block 1 – Front Elevation
(Q4A Architects 2020a)



Figure 35: Village Homes (Traditional), Block 1 – Rear Elevation
(Q4A Architects 2020a)



Figure 36: Village Homes (Traditional), Block 1 – Side Elevations
(Q4A Architects 2020a)



Figure 37: Village Homes (Traditional), Block 2 – Front Elevation
(Q4A Architects 2020a)



Figure 38: Village Homes (Traditional), Block 2 – Rear Elevation
(Q4A Architects 2020a)



Figure 39: Village Homes (Traditional), Block 2 – Side Elevations
(Q4A Architects 2020a)

Appendix D: Key Team Member Two-Page Curriculum Vitae

Paul J. Racher, MA, CAHP
Principal - Management and Senior Review (MSR) Team
ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.
219-900 Guelph Street, Kitchener, ON N2H 5Z6
Phone: (519) 804-2291 x100 Mobile: (519) 835-4427
Fax: (519) 286-0493
Email: pracher@arch-research.com
Web: www.arch-research.com

Biography

Paul Racher is a Principal of ARA. He has a BA in Prehistoric Archaeology from WLU and an MA in anthropology from McMaster University. He began his career as a heritage professional in 1986. Over the three decades since, he has overseen the completion of several hundred archaeological and cultural heritage contracts. Paul has years of experience related to linear transportation and rail projects, notably through the ongoing work to complete a Cultural Heritage Inventory for the Region of Waterloo's Stage 2 LRT from Kitchener to Cambridge, Ontario. He holds professional license #P007 with the MHSTCI. Paul is a former lecturer in Cultural Resource Management at WLU. He is a professional member of the Canadian Association of Heritage Professionals (CAHP) and the President of the Ontario Archaeological Association (OAS).

Education

1992-1997	PhD Programme, Department of Anthropology, University of Toronto. Supervisors: E.B. Banning and B. Schroeder. Withdrawn.
1989-1992	M.A., Department of Anthropology, McMaster University, Hamilton, Ontario. Thesis titled: "The Archaeologist's 'Indian': Narrativity and Representation in Archaeological Discourse."
1985-1989	Honours B.A., Wilfrid Laurier University, Waterloo, Ontario. Major: Prehistoric Archaeology.

Professional Memberships and Accreditations

Current	Ministry of Tourism Culture and Sport Professional Licence (#P007). Professional Member of the Canadian Association of Heritage Professionals (CAHP), Volunteer on the ethics committee. Member of the Ontario Archaeological Society (OAS), Volunteer on the Professional Committee. Associate of the Heritage Resources Centre, University of Waterloo. RAQS registered with MTO.
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Work Experience

Current	Vice-President, Operations, Archaeological Research Associates Ltd. Responsible for winning contracts, client liaison, project excellence, and setting the policies and priorities for a multi-million dollar heritage consulting firm.
2000-2011	Project Manager/Principal Investigator, Archaeological Research Associates Ltd. Managed projects for a heritage consulting firm. In 10 field seasons, managed hundreds of projects of varying size.

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- 2008-2011 **Part-Time Faculty, Wilfrid Laurier University**
Lecturer for Cultural Resource Management course (AR 336). In charge of all teaching, coursework, and student evaluations.
- 1995 **Field Archaeologist, University of Toronto**
Served as a supervisor on a multinational archaeological project in northern Jordan.
- 1992-1995 **Teaching Assistant, University of Toronto**
Responsible for teaching and organizing weekly tutorials for a number of courses.
- 1991-1994 **Part-Time Faculty, Wilfrid Laurier University**
Lectured for several courses in anthropology. Held complete responsibility for all teaching, coursework, and student evaluations.
- 1992-1996 **Partner in Consulting Company, Cultural Management Associates Incorporated**
Supervised several archaeological contracts in Southern Ontario. Participated in a major (now published) archaeological potential modeling project for MTO.
- 1989-1991 **Partner in Consulting Company, Cultural Resource Consultants**
Managed the financial affairs of a consulting firm whilst supervising the completion of several contracts performed for heritage parks in central Ontario.
- 1988-1991 **Principal Investigator/Project Director, Archaeological Research Associates Ltd.**
Oversaw the completion of large contracts, wrote reports, and was responsible for ensuring that contracts were completed within budget.
- 1988 **Assistant Director of Excavations, St. Marie among the Hurons, Midland, ON**
Duties included crew supervision, mapping, report writing and photography.
- 1986-1987 **Archaeological Crew Person, Archaeological Research Associates Ltd., Waterloo, ON**
Participated in background research, survey, and excavation on a number of Archaeological sites across Ontario.

Kayla Jonas Galvin, MA, MCIP, RPP, CAHP
 Heritage Operations Manager
ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.
 1480 Sandhill Drive, Unit 3, Ancaster, ON L9G 4V5
 Phone: (519) 804-2291 x120 Fax: (519) 286-0493
 Email: kjgalvin@arch-research.com Web: www.arch-research.com

Biography

Kayla Jonas Galvin, Archaeological Research Associates Ltd.'s Heritage Operations Manager, has extensive experience evaluating cultural heritage resources and landscapes for private and public-sector clients to fulfil the requirements of provincial and municipal legislation such as the *Environmental Assessment Act*, the *Standards & Guidelines for the Conservation of Provincial Heritage Properties* and municipal Official Plans. She served as Team Lead on the Ministry of Tourism, Culture and Sport Historic Places Initiative, which drafted over 850 Statements of Significance and for *Heritage Districts Work!*, a study of 64 heritage conservation districts in Ontario. Kayla was an editor of *Arch, Truss and Beam: The Grand River Watershed Heritage Bridge Inventory* and has worked on Municipal Heritage Registers in several municipalities. Kayla has drafted over 150 designation reports and by-laws for the City of Kingston, the City of Burlington, the Town of Newmarket, Municipality of Chatham-Kent, City of Brampton and the Township of Whitchurch-Stouffville. Kayla is the Heritage Team Lead for ARA's roster assignments for Infrastructure Ontario and oversees evaluation of properties according to *Standards & Guidelines for the Conservation of Provincial Heritage Properties*. Kayla is a Registered Professional Planner (RPP), Member of the Canadian Institute of Planners (MCIP), professional member of the Canadian Association of Heritage Professionals (CAHP) and sits on the board of the Ontario Association of Heritage Professionals.

Education

2016	MA in Planning, University of Waterloo. Thesis Topic: <i>Goderich – A Case Study of Conserving Cultural Heritage Resources in a Disaster</i>
2003-2008	Honours BES University of Waterloo, Waterloo, Ontario Joint Major: Environment and Resource Studies and Anthropology

Professional Memberships and Accreditations

Current	Professional Member, Canadian Association of Heritage Professionals (CAHP) Member of the Canadian Institute of Planners (MCIP) Professional Member, Ontario Professional Planners Institute (OPPI) Board Member, Ontario Association of Heritage Professionals.
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Work Experience

Current	Heritage Operations Manager, Archaeological Research Associates Ltd. Oversees business development for the Heritage Department, coordinates completion of designation by-laws, Heritage Impact Assessments, Built Heritage and Cultural Heritage Landscape Assessments, and Cultural Heritage Resource Evaluations.
2009-2013	Heritage Planner, Heritage Resources Centre, University of Waterloo Coordinated the completion of various contracts associated with built heritage including responding to grants, RFPs and initiating service proposals.
2008-2009, 2012	Project Coordinator–Heritage Conservation District Study, ACO Coordinated the field research and authored reports for the study of 32 Heritage Conservation Districts in Ontario. Managed the efforts of over 84 volunteers, four staff and municipal planners from 23 communities.

2007-2008 **Team Lead, Historic Place Initiative, Ministry of Culture**

Liaised with Ministry of Culture Staff, Centre's Director and municipal heritage staff to draft over 850 Statements of Significance for properties to be nominated to the Canadian Register of Historic Places. Managed a team of four people.

Selected Professional Development

- 2019 OPPI and WeirFoulds Client Seminar: Bill 108 – More Homes, More Choice, 2019
- 2019 Annual attendance at Ontario Heritage Conference, Goderich, ON (Two-days)
- 2019 Information Session: Proposed Amendments to the OHA, by Ministry of Tourism, Culture and Sport
- 2018 Indigenous Canada Course, University of Alberta
- 2018 Volunteer Dig, Mohawk Institute
- 2018 Indigenizing Planning, three webinar series, Canadian Institute of Planners
- 2018 Cultural Heritage, Archaeology and Planning Symposium
- 2018 Transforming Public Apathy to Revitalize Engagement, Webinar, MetorQuest
- 2018 How to Plan for Communities: Listen to the Them, Webinar, Canadian Institute of Planners
- 2017 Empowering Indigenous Voices in Impact Assessments, Webinar, International Association for Impact Assessments
- 2017 Capitalizing on Heritage, National Trust Conference, Ottawa, ON.
- 2016 Cultural Heritage, Archaeology and Planning Symposium
- 2016 Heritage Rising, National Trust Conference, Hamilton
- 2016 Ontario Heritage Conference St. Marys and Stratford, ON.
- 2016 Heritage Inventories Workshop, City of Hamilton & ERA Architects
- 2015 Cultural Heritage, Archaeology and Planning Symposium
- 2015 City of Hamilton: Review of Existing Heritage Permit and Heritage Designation Process Workshop.
- 2015 Leadership Training for Managers Course, Dale Carnegie Training

Selected Publications

- 2018 "Conserving Cultural Heritage Landscapes in Waterloo: An Innovative Approach." *Ontario Association of Heritage Professionals Newsletter*, Winter 2018.
- 2018 "Restoring Pioneer Cemeteries" *Ontario Association of Heritage Professionals Newsletter*. Spring 2018. *In print*.
- 2015 "Written in Stone: Cemeteries as Heritage Resources." *Municipal World*, Sept. 2015.
- 2015 "Bringing History to Life." *Municipal World*, February 2015, pages 11-12.
- 2014 "Inventorying our History." *Ontario Planning Journal*, January/February 2015.
- 2014 "Mad about Modernism." *Municipal World*, September 2014.

Lindsay Benjamin, MAES, MCIP, RPP, CAHP
 Project Manager - Heritage
ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.
 219-900 Guelph Street, Kitchener, ON, N2H 5Z6
 Phone: (519) 804-2291 x120 Fax: (519) 286-0493
 Email: lindsay.benjamin@arch-research.com
 Web: www.arch-research.com

Biography

Lindsay Benjamin is practiced at providing professional planning recommendations and expertise on complex studies, research projects, cultural heritage impact and archaeological assessments. Through her work as a Cultural Heritage Planner, Lindsay researched, drafted and implemented policies for the Regional Official Plan and other planning documents regarding the recognition, review and conservation of cultural heritage resources, including archaeological resources, heritage bridges, cultural heritage landscapes and scenic roads. She was the Primary Author of *Arch, Truss and Beam: The Grand River Watershed Heritage Bridge Inventory*, served as a Team Lead on the former MTCS' Historic Places Initiative that drafted over 850 Statements of Significance, and was Series Editor for Phase 2 of *Heritage Districts Work!* a study of 32 heritage districts in Ontario. Lindsay has developed cultural heritage landscape inventories, heritage property tax relief programs, worked on Municipal Heritage Registers and drafted designation by-laws in several municipalities. She holds a Master of Applied Environmental Studies degree from the University of Waterloo School of Planning, is a Registered Professional Planner (RPP), Member of the Canadian Institute of Planners (MCIP) and is a professional member of the Canadian Association of Heritage Professionals (CAHP).

Education

2013	MAES, University of Waterloo, Waterloo, ON, Focus: Planning.
2009	Post-Graduate Diploma, Centennial College, Toronto, ON Publishing & Professional Writing
2007	Honours BES, University of Waterloo, Waterloo, ON Major: Urban Planning, Co-op. Distinction: Dean's Honours List

Professional Memberships and Accreditations

Current	Canadian Association of Heritage Professionals (CAHP), Professional Membership
Current	Ontario Professional Planners Institute (OPPI), Professional Member
Current	Member of the Canadian Institute of Planners (MCIP)

Work Experience

2017-Present	Project Manager - Heritage, Archaeological Research Associates Ltd. Coordinate the completion of heritage projects, including the evaluation of the cultural heritage value or interest of a variety of cultural heritage resources.
2013-2017	Cultural Heritage Planner, Region of Waterloo Planned and implemented Arts, Culture and Heritage initiatives that support creativity and quality of life in the Region of Waterloo. Researched, developed and implemented Regional cultural heritage policies and programs. Fulfilled Regional and Provincial cultural heritage and archaeological review responsibilities under the Planning Act and Ontario Heritage Act.
2009-2013	Heritage Planner, Heritage Resources Centre, University of Waterloo Facilitate the completion of various cultural heritage contracts by undertaking archival research, site visits, report writing, liaising with municipal staff and stakeholders and coordinating project scheduling and budgetary responsibilities.

- 2006-2007 **Project Manager, Heritage Resources Centre, University of Waterloo**
Established the process of nominating heritage properties to the National Register of Historic Places. Primary liaison between all stakeholder groups, responsible for motivating each group to participate and provide funding. Drafted over 130 Statements of Significance for properties to be nominated to the National Register. Managed a team of five employees.
- 2005-2006 **Heritage Conservation Easement Planning Assistant, Ontario Heritage Trust**
Supported easement acquisitions through researching the historical and architectural value of potential acquisitions and extensive photo documentation. Screened and processed activity requests from property owners and stakeholders relating to the easement program. Conducted site visits to monitor conservation easement sites and prepared condition assessment reports.

Selected Professional Development

- 2019 OPPI and WeirFoulds Client Seminar: Bill 108 – More Homes, More Choice
- 2019 Annual attendance at Ontario Heritage Conference, Goderich, ON
- 2015-2019 Cultural Heritage, Archaeology and Planning Symposium
- 2018 How to Plan for Communities: Listen to the Them, Webinar, CIP
- 2013-2017 Ontario Heritage Planners Network Workshops
- 2017, 2016 National Trust for Canada Conference
- 2012 Heritage Impact Assessments Workshop, Region of Waterloo
- 2012 National Trust for Historic Preservation Conference and Conducting Historic Building Assessments Workshop, Spokane, WA
- 2012 Canadian Institute of Planners National Conference, Banff, ON

Awards

- 2014 Heritage River Award, Watershed Awards & Canadian Heritage River Celebration, Grand River Conservation Authority
- 2009 A. K. (Alice King) Sculthorpe Award for Advocacy - ACO

Publications

- 2019 “Journey Through German Mills.” *Waterloo Historical Society Annual Volume*. Volume 106 – 2018.
- 2018 “Conserving Cultural Heritage Landscapes in Waterloo: An Innovative Approach.” *Ontario Association of Heritage Professionals Newsletter*, Winter 2018.
- 2017 Historic Interpretive Plaque - Village of German Mills
- 2016 Historic Interpretive Plaques - West Montrose Covered Bridge; Huron Road Bridge

Volunteer Experience

- 2017-2019 Lieutenant Governor’s Ontario Heritage Awards Jury Member

Penny M. Young, MA, CAHP (#P092)
 Project Manager - Heritage
ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.
 1480 Sandhill Drive, Unit 3, Ancaster, ON L9G 4V5
 Phone: (519) 804-2291 x121 Email: penny.young@arch-research.com
 Web: www.arch-research.com

Biography

Penny Young has 27 years of cultural heritage management experience, 21 years working in government, as a Heritage Planner, Heritage Coordinator, Regional Archaeologist and Archaeological Database Coordinator where she managed and coordinated the impacts to cultural heritage resources including built heritage, archaeological sites and cultural heritage landscapes for compliance with municipal, provincial and federal legislation and policy. She has conducted results-driven and collaborative management of complex cultural heritage resource projects within the public sector involving developing project terms of reference, defining scope of work, preparation of budgets and conducting sites visits to monitor and provide heritage/archaeological and environmental advice and direction. At the Ministry of Transportation Penny revised, updated and developed policy, as part of a team, for the *Ontario Heritage Bridge Guidelines for Provincially Owned Bridge Guidelines for Provincially Owned Bridges*. She received the MTO Central Region Employee Recognition Award in 2001 and 2002. While at MTO she provided technical advice and input into the development of the *MTO Environmental Reference for Highway Design - Section 3.7 Built Heritage and Cultural Heritage Landscapes* and the *MTO Environmental Guide for Built Heritage and Cultural Heritage Landscapes*. She is a professional member of the Canadian Association of Heritage Planners (CAHP) and holds Professional License #P092 from MHSTCI. She also holds memberships in the Ontario Professional Planners Institute (OPPI) and the Ontario Archaeological Society (OAS).

Education

1990-1993 Master of Arts, Department of Anthropology McMaster University, Hamilton Ontario. Specializing in Mesoamerican and Ontario archaeology.
 1983-1987 Honours Bachelor of Arts (English and Anthropology), McMaster University, Hamilton, Ontario.

Professional Memberships and Accreditations

Current Professional Member, Canadian Association of Heritage Professionals (CAHP)
 Member of Ontario Archaeological Society
 Pre-Candidate Member, Ontario Professional Planners Institute (OPPI)
 Ministry of Tourism Culture & Sport Professional Licence (#P092)

Work Experience

Current **Project Manager - Heritage, Archaeological Research Associates Ltd.**
 Coordinates ARA project teams and conducts heritage assessment projects including Heritage Impact Assessments, Built Heritage and Cultural Heritage Landscape Assessments, and Cultural Heritage Resource Evaluations. Additional responsibilities include the completion of designation by-laws and heritage inventories. Liaises with municipal staff, provincial ministries and Indigenous communities to solicit relevant project information and to build relationships.

- 2008-2016 **Heritage Planner, Culture Services Unit, Former Ministry of Tourism, Culture & Sport (MTCS)**
Responsible for advising and providing technical review for management of cultural heritage resources in environmental assessment undertakings and planning projects affecting provincial ministries, municipalities, private sector proponents and Indigenous communities. Advised on municipalities' Official Plan (OP) policies cultural heritage conservation policies. Provided guidance on compliance with the Public Work Class EA, other Class EA legislation and 2010 *Standards and Guidelines for Provincial Heritage Properties*.
- 2014 **Senior Heritage Planner, Planning and Building Department, City of Burlington** (temporary assignment)
Project manager of the study for a potential Heritage Conservation District. Provided guidance to a multiple company consultant team and reported to municipal staff and the public. Liaised with Municipal Heritage Committee and municipal heritage property owners approved heritage permits and provided direction on Indigenous engagement, archaeological site assessments and proposed development projects.
- 2011 **Heritage Coordinator, Building, Planning and Design Department, City of Brampton** (temporary assignment)
Project lead for new Heritage Conservation District Study. The assignment included directing consultants, managing budgets, organizing a Public Information Session, and reporting to Senior Management and Council. Reviewed development/planning documents for impacts to heritage including OP policies, OP Amendments, Plans of subdivision and Committee of Adjustment applications and Municipal Class EA undertakings.
- 2010-2011 **Senior Heritage Coordinator, Culture Division, City of Mississauga** (temporary assignment)
Provided advice to Senior Management and Municipal Council on heritage conservation of built heritage, archaeological sites and cultural heritage landscapes. Liaised with multiple municipal staff including the Clerks' office, Parks and development planners and the public. Supervised and directed project work for junior heritage planner.
- 1999-2008 **Regional Archaeologist, Planning and Environmental Section, Ministry of Transportation (MTO)**
Responsibilities included: project management and coordination of MTO archaeology and heritage program, managed multiple consultants, conducted and coordinated field assessments, surveys and excavations, liaised with First Nations' communities and Band Councils, estimated budgets including \$200,000 retainer contracts.

Sarah Clarke, BA
Research Manager
ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.
219-900 Guelph Street, Kitchener, ON N2H 5Z6
Phone: (519) 755-9983 Email: sclarke@arch-research.com
Web: www.arch-research.com

Biography

Sarah Clarke is Archaeological Research Associates Ltd.'s Heritage Research Manager. Sarah has over 12 years of experience in Ontario archaeology and 10 years of experience with background research. Her experience includes conducting archival research (both local and remote), artifact cataloguing and processing, and fieldwork at various stages in both the consulting and research-based realms. As Team Lead of Research, Sarah is responsible for conducting archival research in advance of ARA's archaeological and heritage assessments. In this capacity, she performs Stage 1 archaeological assessment site visits, conducts preliminary built heritage and cultural heritage landscape investigations and liaises with heritage resource offices and local community resources in order to obtain and process data. Sarah has in-depth experience in conducting historic research following the *Ontario Heritage Toolkit* series, and the *Standards and Guidelines for Provincial Heritage Properties*. Sarah holds an Honours B.A. in North American Archaeology, with a Historical/Industrial Option from Wilfrid Laurier University and is currently enrolled in Western University's Intensive Applied Archaeology MA program. She is a member of the Ontario Archaeological Society (OAS), the Society for Industrial Archaeology, the Ontario Genealogical Society (OGS), the Canadian Archaeological Association, and is a Council-appointed citizen volunteer on the Brantford Municipal Heritage Committee. Sarah holds an R-level archaeological license with the MHSTCI (#R446).

Education

Current	MA Intensive Applied Archaeology, Western University, London, ON. Proposed thesis topic: Archaeological Management at the Mohawk Village.
1999–2010	Honours BA, Wilfrid Laurier University, Waterloo, Ontario Major: North American Archaeology, Historical/Industrial Option

Professional Memberships and Accreditations

Current	Member of the Ontario Archaeological Society
Current	Member of the Society for Industrial Archaeology
Current	Member of the Brant Historical Society
Current	Member of the Ontario Genealogical Society
Current	Member of the Canadian Archaeological Association
Current	Member of the Archives Association of Ontario

Work Experience

Current	Team Lead – Research; Team Lead – Archaeology, Archaeological Research Associates Ltd. Manage and plan the research needs for archaeological and heritage projects. Research at offsite locations including land registry offices, local libraries and local and provincial archives. Historic analysis for archaeological and heritage projects. Field Director conducting Stage 1 assessments.
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- 2013-2015 **Heritage Research Manager; Archaeological Monitoring Coordinator, Archaeological Research Associates Ltd.**
Stage 1 archaeological field assessments, research at local and distant archives at both the municipal and provincial levels, coordination of construction monitors for archaeological project locations.
- 2010-2013 **Historic Researcher, Timmins Martelle Heritage Consultants Inc.**
Report preparation, local and offsite research (libraries, archives); correspondence with the Ministry of Tourism, Culture, and Sport; report submission to the former MTCS and clients; and administrative duties (PIF and Borden form completion and submission, data requests).
- 2008-2009 **Field Technician, Archaeological Assessments Ltd.**
Participated in field excavation and artifact processing.
- 2008-2009 **Teaching Assistant, Wilfrid Laurier University.**
Responsible for teaching and evaluating first year student lab work.
- 2007-2008 **Field and Lab Technician, Historic Horizons.**
Participated in excavations at Dundurn Castle and Auchmar in Hamilton, Ontario. Catalogued artifacts from excavations at Auchmar.
- 2006-2010 **Archaeological Field Technician/Supervisor, Wilfrid Laurier University.**
Field school student in 2006, returned as a field school teaching assistant in 2008 and 2010.

Professional Development

- 2019 Annual attendance at Ontario Heritage Conference, Goderich, ON
- 2018 Cultural Heritage, Archaeology and Planning Symposium
- 2018 Grand River Watershed 21st Annual Heritage Day Workshop & Celebration
- 2018 Mississaugas of the New Credit First Nation Historical Gathering and Conference
- 2017 Ontario Genealogical Society Conference
- 2016 Ontario Archaeological Society Symposium
- 2015 Introduction to Blacksmithing Workshop, Milton Historical Society
- 2015 Applied Research License Workshop, Former MTCS
- 2014 Applied Research License Workshop, Former MTCS
- 2014 Heritage Preservation and Structural Recording in Historical and Industrial Archaeology. Four-month course taken at Wilfrid Laurier University, Waterloo, ON. Professor: Meagan Brooks.

Presentations

- 2018 *The Early Black History of Brantford.* Brant Historical Society, City of Brantford.
- 2017 *Mush Hole Archaeology.* Ontario Archaeological Society Symposium, Brantford.
- 2017 *Urban Historical Archaeology: Exploring the Black Community in St. Catharines, Ontario.* Canadian Archaeological Association Conference, Gatineau, QC.

Volunteer Experience

- Current Council-appointed citizen volunteer for the Brantford Municipal Heritage Committee.

Jacqueline McDermid, BA

Technical Writer

ARCHAEOLOGICAL RESEARCH ASSOCIATES LTD.

1480 Sandhill Drive, Unit 3, Ancaster, ON L9G 4V5

Phone: (905) 304-6893 x221 Fax: (519) 286-0493

Email : jmcdermid@arch-research.com Web: www.arch-research.com

Biography

In the spring of 2018, Jacqueline completed a six-month contract with the MTO as a Heritage Specialist for Central Region, returning to her permanent position at ARA in the Fall 2018 where she had been the acting Heritage Team Lead for the year previous. As the lead, she directed the preparation and oversaw the submission of deliverables to clients. Currently, she is the Heritage Team Technical Writer and Researcher, where she continues to research and evaluate the significance of cultural heritage resources using *Ontario Regulation 9/06* and *10/06*, most recently completing designation reports for the City of Burlington, City of Kingston and Town of Newmarket and the Town of Whitchurch-Stouffville. Further, Jacqueline has overseen the completion of many Built Heritage and Cultural Heritage Landscape Studies as well as Heritage Impact Assessments including reports for a proposed aggregate pit, road widening, the LRT in the Region of Waterloo and a National Historic Site in St. Catharines. As well as being a proficient technical writer, Jacqueline is skilled at writing in approachable language demonstrated by my crafting of 30 properties stories and 35 thematic stories for Heritage Burlington's website. She holds an Honours Bachelor of Arts in Near Eastern Archaeology from Wilfrid Laurier University. In addition to heritage experience, Jacqueline also has archaeological experience working as field crew, as an Assistant Lab Technician and archaeological technical writer.

Education

2000-2007 Honours B.A., Wilfrid Laurier University, Waterloo, Ontario
Major: Near Eastern Archaeology

Work Experience

2015-Present **Technical Writer and Researcher – Heritage, Archaeological Research Associates Ltd., Kitchener, ON**

Research and draft designation by-laws, heritage inventories, Heritage Impact Assessments, Built Heritage and Cultural Heritage Landscape Assessments, and Cultural Heritage Resource Evaluations using Ontario Regulation 9/06, 10/06 and the Ontario Heritage Bridge Guidelines.

2018 **Environmental Planner – Heritage Ministry of Transportation, Central Region – Six-month contract.**

Responsibilities included: project management and coordination of MTO heritage program, managed multiple consultants, conducted and coordinated field assessments and surveys, estimated budgets including \$750,000 retainer contracts. Provided advice on heritage-related MTO policy to Environmental Policy Office (EPO) and the bridge office.

2017-2018 **Acting Heritage Team Lead – Heritage Archaeological Research Associates Ltd., Kitchener, ON**

Managed a team of Heritage Specialists, oversaw the procurement of projects, retainers; managed all Heritage projects, ensured quality of all outgoing products.

2014-2015	Technical Writer – Archaeology, Archaeological Research Associates Ltd., Kitchener, ON Report preparation; correspondence with the Ministry of Tourism, Culture, and Sport; report submission to the Ministry and clients; and administrative duties (PIF and Borden form completion).
2012-2013	Lab Assistant, Archaeological Research Associates Ltd., Kitchener, ON Receive, process and register artifacts.
2011-2012	Field Technician, Archaeological Research Associates Ltd., Kitchener, ON Participated in field excavation and artifact processing.
2005-2009	Teaching Assistant, Wilfrid Laurier University, Waterloo, ON Responsible for teaching and evaluating first, second, third- and fourth-year student lab work, papers and exams.
2005-2007	Lab Assistant, Wilfrid Laurier University – Near Eastern Lab, Waterloo, ON Clean, Process, Draw and Research artifacts from various sites in Jordan.

Selected Professional Development

2019	OPPI and WeirFoulds Client Seminar: Bill 108 – More Homes, More Choice
2019	Annual attendance at Ontario Heritage Conference, Goderich, ON (Two-days)
2019	Information Session: Proposed Amendments to the OHA, Former MTCS
2018	Indigenizing Planning, three webinar series, Canadian Institute of Planners
2018	Cultural Heritage, Archaeology and Planning Symposium
2018	Transforming Public Apathy to Revitalize Engagement, Webinar, MetorQuest
2018	How to Plan for Communities: Listen to the Them, Webinar, CIP
2017	Empowering Indigenous Voices in Impact Assessments, Webinar, International Association for Impact Assessments
2015	Introduction to Blacksmithing (One day)
2015	Leadership Training for Managers Course, Dale Carnegie Training