

Heritage Impact Assessment Concerning the Redevelopment of 1644 Glenburnie Road, Mississauga

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for the Property Owner and the City of Mississauga

June 16, 2020



1644 Glenburnie Road and adjacent 1654 Glenburnie Road

Heritage Impact Assessment Concerning the Redevelopment of 1644 Glenburnie Road, Mississauga

Purpose of the Heritage Impact Assessment

A policy in the Mississauga Official Plan stipulates preparation of a heritage impact assessment when development is proposed adjacent to a property which has been listed in the municipal heritage register under Section 27 (1.2) of the *Ontario Heritage Act*. The City has identified the property immediately north of 1644 Glenburnie Road and located beside the Queen Elizabeth Way right-of-way – 1654 Glenburnie Road – as the Henry Belford House. The City believes the property at 1654 Glenburnie Road has cultural heritage interest for its historical significance, and has listed it in the municipal heritage register.

For the purpose of the heritage impact assessment concerning the redevelopment of 1644 Glenburnie Road, the Henry Belford House is presumed to have historical significance; and an evaluation of its cultural heritage value which would determine its merit for *Ontario Heritage Act* designation will not be conducted. The heritage impact assessment is also limited in its scope for determining the cultural heritage value of 1644 Glenburnie Road. For the heritage impact assessment, City heritage staff have eliminated a detailed site history and a statement on the significance and heritage attributes of the property. An evaluation of the cultural heritage value of the property at 1644 Glenburnie Road will not undertaken. It is therefore presumed that the existing house at 1644 Glenburnie Road has not acquired historical significance.

The heritage impact assessment will focus on any effects the proposed new house at 1644 Glenburnie Road may have on the Henry Belford House. The heritage impact assessment will also consider the redevelopment proposal in the context of the existing landscape and architectural character of the area near 1644 Glenburnie Road.

Study Methodology

An initial site visit was made on April 22, 2019. Photographic recording of the property at 1644 Glenburnie Road and area occurred on May 5, 2019, and as-found photographs presented in the report date from the May 5 site visit.

Paula Wubbenhorst, the City's heritage planner, and Matthew Wilkinson of Heritage

Mississauga provided historic photographs of the Henry Belford House. Topographical maps, a fire insurance plan from 1952, the registered plan of subdivision and the early entries in the land title were copied and analyzed. Andrew Murray's archaeological assessment, dated May 6, 2019, was read; and his copies of aerial photographs from the City of Toronto Archives were noted. Histories of the Queen Elizabeth Way were consulted. A search through the scrapbook of newspaper articles written by Mildred Belleghem and of her articles published in the *Streetsville Review* did not uncover the historic photograph of the Henry Belford House which is attributed to her or the story that would have accompanied the photograph. As well, the source of the photograph credited to the Peel Region Archives could not be found.

Zoning By-law 0225-2007 was examined for regulations applicable to 1644 Glenburnie Road.

In addition to correspondence with the property owner and his house designer, in-person meetings and telephone conferences were held to discuss ways of mitigating the impact of the new house on the Henry Belford House and area.

The report presented here follows a year-long design process that was delayed by deliberations over the handling of water flow through the property's backyard swale.

Property's Location

The property at 1644 Glenburnie Road is located just to the west of the Port Credit highway interchange at Hurontario Street (Figure 1 in Appendix A). The property is the second lot south of the Queen Elizabeth Way right-of-way on the west side of Glenburnie Road. Glenburnie Road, a north-south street running between Mineola Road West and the Queen Elizabeth Way, is part of the Mineola neighbourhood, a low-density residential area (Fig. 2). Pinetree Way, the east-west street closest to 1644 Glenburnie Road, connects to Hurontario Street.

Land Use and Zoning

The R2-4 zoning at 1644 Glenburnie Road and in the area north of Pinetree Way permits detached dwellings, a built form which matches the existing land use (Fig. 3).

In addition to zoning regulations, the property is found within a large site plan control area. All of the properties located between the Queen Elizabeth Way and the Canadian

National Railway line through Port Credit are under site plan control.

History of the Locale

In 1909 when May Gordon had her surveyor, Clayton Bush, lay out twelve lots along a new north-south road intersecting with the Middle Road in Toronto Township, the area was a wooded and swampy landscape (Fig. 4 and 5). The Middle Road, so called because it ran between two other east-west roads – the Lake Shore Road and Dundas Street – was a narrow country road (Fig. 6). Lot 1 in Gordon's plan of subdivision was a block of land with a frontage of about 216 feet along the Middle Road and 660 feet along the west side of the new road aptly named Forest Road (today's Glenburnie Road). Lots 2 to 11 on the east side of Forest Road were smaller. Lot 12 at the foot of Forest Road was large like Lot 1.

Henry Robison Belford, a farmer, purchased Lot 1 from May Gordon in October 1910. As told in the City's municipal heritage listing, Henry Belford built a house here about 1910. It was meant to be a house for his wife to be and the family they would have; but his marriage did not happen, and he lived in the house as a bachelor until his death in 1964. A photograph, believed to have been taken in the 1920s, recorded Belford's frame house (Fig. 7). One-and-a-half-storeys tall, the "L"-shaped house surmounted by cross gables had a decided vertical orientation. Three bays spanned the front of the house, which faced the Middle Road. The middle bay was a tower topped by a pyramidal roof. The front entrance was tucked into the tower's side and sheltered by a side porch. Tongue-and-groove wood siding clad the house (see also Fig. 8). The siding was painted a lighter colour than the corner boards and window surrounds. The house's architectural style could be mistaken for a late nineteenth century origin, but the fenestration would place the house in the first decade of the twentieth century. In rural Ontario, vernacular construction such as Henry Belford's house often lagged behind architectural trends in urban areas.

Henry Belford's quiet enjoyment of his rural property persisted without disturbance until 1934 when the Provincial Department of Highways took 0.108 acres of frontage off his lot for the first intercity divided highway in Canada (Fig. 9).

According to Robert Stamp's history of the Queen Elizabeth Way, motor vehicle traffic congestion between Toronto and Hamilton had been a problem since 1914. To alleviate traffic jams, the provincial government commissioned a survey of alternative routes for

the Toronto-Hamilton corridor – the Lake Shore Road, Dundas Street and the Middle Road. Lake Shore Road was chosen for widening, and in 1917 the Toronto-Hamilton Highway (Highway 2) opened. It was the first concrete highway in Canada. Soon, however, the widened Lake Shore Road could not handle the increase in traffic. As the Lake Shore Road could not be widened further, the Province decided to turn the Middle Road into the Middle Road Highway – four lanes of paved surface, on a grade no more than three per cent, with curves less than two degrees, and with elimination of all railway level crossings. Construction on the Port Credit cloverleaf interchange at Highway 10 (Hurontario Street) began in 1937. It was the first cloverleaf built in Canada. When the superhighway was officially opened in 1939 by King George VI and Queen Elizabeth (mother of Queen Elizabeth II), the Province rededicated the Middle Road Highway as the Queen Elizabeth Way. The four-lane highway with its grassy median and landscaped boulevards introduced a presence along Belford's property frontage far different from the pastoral Middle Road (Fig. 10 to 12).

In 1952 when the Underwriters' Survey Bureau drew its fire insurance plan for Port Credit and part of Toronto Township, Belford's frame house with an address on the 110-foot-wide Queen Elizabeth Way remained relatively surrounded by open space (Fig. 13). A number of houses had been erected along Forest Road which had at-grade access to the highway. New streets had been laid out to the south, and houses built along them.

By 1960, the area south of Belford's house was so built up that it was shown as an urban area on the topographical map (Fig. 14). The relative isolation of Belford's house was completely lost in 1963, the year before Belford's death, when the bungalow south of his property (1644 Glenburnie Road) was built. On the north side, the property faced six lanes of the newly widened Queen Elizabeth Way. Access to the highway was blocked. To the west, a rebuilt interchange replaced the original Port Credit cloverleaf at Highway 10. An aerial photograph taken in 1963 recorded the altered context for Belford's house (Fig. 15).

The mid-twentieth century Mineola neighbourhood in which Belford's early twentieth century house stood was described in a 2005 study commissioned by the City of Mississauga to identify cultural landscapes across the municipality:

"Mineola was developed before it became standard practice to regrade top soil into large piles in the early twentieth century, level every nuance of natural topography and engineer the complete stormwater drainage system artificially. In Mineola a road system was gently imposed on the natural rolling topography of the Iroquois Plain; homes were

nestled into slightly larger lots and natural drainage areas were retained. This provided greater opportunity to save existing trees and because the soils and drainage system were minimally impacted, provided fertile ground for the planting of new vegetation, the natural regeneration of native trees and landscaping of the residential landscapes. What has evolved today is a wonderful neighbourhood with a variety of quality housing stock and a rich stimulating landscape that blends the houses with their natural and manicured surroundings. There are no curbs on the roads which softens the transition between street and front yards. The roads wind, rise and fall with the natural topography and houses sit often at odd angles to take advantage of slopes and the location of large trees. A gradual infilling has increased the density over the years and care must be taken to ensure that this does not, in the end, ruin the very quality and character that makes this neighbourhood so appealing and attractive. Of the many neighbourhoods in Mississauga, the Mineola neighbourhood stands out as one of the most visually interesting and memorable. As is often the case, when new development is balanced with the protection of the natural environment, a truly livable and sustainable community evolves. Mineola is an excellent example of this type of community."

Since 2005, many lots in Mineola have been redeveloped with much larger single-detached houses than the houses built in the mid-twentieth century. Redevelopment has missed Glenburnie Road north of Pinetree Way, likely because of the block's proximity to the Queen Elizabeth Way. The proposed redevelopment at 1644 Glenburnie Road is the first redevelopment project in the block.

As-found Appearance of 1644 Glenburnie Road

The one-storey bungalow at 1644 Glenburnie Road is set on a very deep lot – about 216 feet (66 metres) – the same as 1654 Glenburnie Road (Fig. 16). It nearly spans the lot's frontage of 76 feet (23 metres), leaving narrow yards on both the north and south sides. Its setback from the street at 31 feet (9.5 metres, exclusive of the front porch) is more than twice the setback for No. 1654.

The dwelling's low profile, long length, horizontal emphasis, shallow-pitched hipped roofs, integral garage attached to one side of the front facade and picture window are typical features of the many modestly sized Ranch-style Bungalows built in the 1960s across North America (Fig. 17 to 19). The popular Ranch-style Bungalow was a vernacular interpretation of the high-style houses designed by the American architect, Frank Lloyd Wright, in the mid-twentieth century. The dwelling's walls are clad in a dark yellow clay brick. Two trees in the boulevard – a spruce and a maple – likely date from the same decade when the house was built.

The dwelling's rear (west) elevation shows a tall basement lighted with a number of big windows (Fig. 20 and 21). Sliding glass doors opening onto a wooden deck and into the backyard are typical of the Ranch-style Bungalow style.

The backyard contains two slopes. It slopes from the house down to a swale that starts at 1654 Glenburnie Road and flows southward across the lot, and then it slopes up from the swale to the rear lot line (Fig. 22). The swale – probably, the remains of the swamp that used to occupy the land – carries enough water from Queen Elizabeth Way run-off and backyards for a small bridge to be required over it (Fig. 23 and 24). Two weeping willow trees that arch over the swale at the north lot line have benefited from the passage of water (Fig. 25). The backyard and rear elevation can be seen as far as Magenta Court, the cul-de-sac west of Glenburnie Road (Fig. 26).

The bungalow's front door, which is not centred in the front facade, opens into a small entry hall (Fig. 27). This placement and the door's Modern details (asymmetrical cut-outs of different shapes and a single sidelight) are further indications of the bungalow's construction in 1963. South of the entry hall is a combination living room and dining room (Fig. 28 and 29). Through the east picture window and large west window, the "L"-shaped room enjoys both east and west sunlight. The open-concept room contains features common to its era of construction – an oak hardwood floor and a plaster ceiling and cornice (Fig. 30 to 32). The kitchen is located at the west end of the entry hall (Fig. 33). The north-south bedroom hall, perpendicular to the east-west entry hall, leads to three bedrooms and a bathroom (Fig. 34 to 38). The basement staircase gives access to the backyard deck through sliding glass doors and to a basement apartment (Fig. 39 to 42).

As-found Appearance of 1654 Glenburnie Road

Henry Belford's house at 1654 Glenburnie Road differs in appearance from the next-door bungalow at 1644 Glenburnie Road. A wide south yard at 1654 Glenburnie Road separates the two (Fig. 43). Since the widening of the Queen Elizabeth Way and the erection of the sound barrier along its right-of-way, the side and rear elevations of Belford's house are seen from Glenburnie Road; and its front facade which used to face the Middle Road is hidden by the highway sound barrier (Fig. 44). The one-and-a-half-storey frame house rests on a concrete foundation, and aluminum siding covers the original wood cladding (Fig. 45). The rear elevation with its vertical emphasis and

sharply pitched dormer window give Belford's house a late nineteenth century Picturesque Gothic appearance, but the large windows on the east and north elevations are typical of the first decade of the twentieth century (Fig. 46 to 48). The side porch which once sheltered the front entrance in the front facade's tower has been enclosed. Although Belford's house from the early twentieth century and the bungalow from the mid-twentieth century are far different in appearance, both 1654 Glenburnie Road and 1644 Glenburnie Road have very deep west yards (Fig. 49 to 50).

As-found Appearance of Surrounding Properties

Henry Belford's house is unusual on the street. The other three houses on the west side of Glenburnie Road, north of Pinetree Way, have a horizontal emphasis and face the street (Fig. 51). All appear to have been built in the mid-twentieth century. The house at the northwest corner of Glenburnie Road and Pinetree Way stands two storeys tall, but the dwellings at 1640 and 1644 Glenburnie Road are bungalows (Fig. 52 and 53).

The houses on the east side of Glenburnie Road, north of Pinetree Way, are small – three of them bungalows and one that is one-and-a-half storeys (Fig. 54 to 57). All appear to date from the mid-twentieth century.

The swale that starts in the backyard of 1654 Glenburnie Road travels southward through the backyards at 1644, 1640 and 1628 Glenburnie Road and underneath Pinetree Way in a large poured concrete culvert (Fig. 58). Bordering the culvert on its west side is the largest house in the block between Glenburnie Road and Magenta Court, north of Pinetree Way (Fig. 59). At sometime, the house has been enlarged and altered.

The east side of Magenta Court is occupied by three bungalows (Fig. 60 to 62).

Redevelopment has occurred south of Pinetree Way along Glenburnie Road, but a couple of bungalows from the mid-twentieth century survive (Fig. 63 to 72). Most of the new houses recall and interpret houses in a variety of traditional, pre-Modern ways; however, 1569 Glenburnie Road with its overhanging flat roofs, expansive glazing and lack of ornamentation follows current trends in contemporary architecture.

Beyond the immediate vicinity of 1644 Glenburnie Road, there are neighbourhood examples of mid-twentieth century Modern houses and new houses in contemporary

styles. A pocket of these, for instance, is grouped around Kenollie Public School at 1376 Glenwood Drive (Fig. 73 to 78).

Description of the Proposed Redevelopment

The house to replace the existing bungalow at 1644 Glenburnie Road was conceived in 2018 and presented for this assessment in April 2019 as a 7,500 square-foot single-detached house in a Deconstructivist style (Fig. 79).

Named in 1988 as a definable trend in contemporary architecture, Deconstructivism embraces an aesthetic of deconstruction, giving the impression that the building is fragmenting. In reaction to the functional expression of construction and materials that characterized Modernism and to the hybridism of Post-Modernism, Deconstructivism aims to disturb the building's form through irregular geometry.

The initial design for the proposed house shows the influence of Deconstructivism through its dynamic play of volumes and its "architectural extrusions" that project over the building's fragmented mass.

Assessment of Impacts from the Proposed Redevelopment

The heritage-listed house at 1654 Glenburnie Road is an anomaly in the block north of Pinetree Way, expressing itself as a Late Victorian dwelling with a picturesque roof line. While Henry Belford's house is oriented vertically, the mid-twentieth century bungalows that prevail on the street are horizontally oriented and positioned low to the ground. Along with two other houses from the mid-twentieth century that do not conform to the prevailing pattern, there is considerable variation in building elevations on the street.

Although architectural contrast already exists in the block, the proposed house in a Deconstructivist style would assert a sudden difference in character. As proposed initially, it would stand out as much of an anomaly on the street as Henry Belford's house but more so.

Measures to Mitigate Impacts

Through the design process, a number of changes were made to reduce the visual impact of the proposed house on Henry Belford's house and on the block. Figures 80A

through 80L – the revised set of renderings, site plan, floor plans, roof plan and elevations – show the improved result.

1. The architectural extrusions projecting over the front elevation have been removed. As revised, the dwelling's front elevation facing Glenburnie Road reads as a front facade as is the custom in the block and neighbourhood.
2. The height of the front elevation has been reduced in absolute numbers. The front elevation's staggered height helps in reducing the visual impression of height. The highest point on the front elevation is an architectural frame that is part of a canopy design.
3. The proposed dwelling's size has been reduced from about 7,500 square feet to about 6,300 square feet. The proposed house is still large in comparison to the modest houses in the block, but the existing open space between the proposed house and Henry Belford's house helps in reducing the proposed house's architectural presence. In addition, the proposed house's side yards – slightly wider than in the initial iteration – are wider than the existing bungalow's side yards.
4. The distance between the proposed house and Henry Belford's house has been enhanced by increasing the front yard setback from the street.
5. In Deconstructivist style, the proposed house is clad in a wide range of cladding materials placed to separate fragmented volumes from one another. On the front elevation, there is a mix of smooth-cut Coronado limestone, matte black concrete panels, buff-coloured distressed concrete panels and glass. The ground-floor porch has a walnut-coloured Cape Elm wood screen over concrete, and two windows have privacy screens made of the same Cape Elm wood.

Figures 81A to D present the proposed house in relation to the existing bungalow at 1640 Glenburnie Road and Henry Belford's house at No. 1654. Among these streetscape elevations, an alternative cladding material is shown for the ground-floor guest room at the dwelling's northeast corner. Like the ground-floor porch at the southeast corner, a screen of Cape Elm wood over concrete covers the guest room.

In either case, the wood screens suggest a contemporary interpretation of the tongue-and-groove wood siding that originally clad Henry Belford's house.

Summary

A mid-twentieth century Ranch-style Bungalow at 1644 Glenburnie Road, which is presumed to lack historical significance, lies adjacent to the property at 1654 Glenburnie Road, which the City has listed in its municipal heritage register for its historical association with Henry Belford. The owner of the property at No. 1644 proposes to demolish the bungalow and replace it with another single-detached house.

As reported in the foregoing paragraphs, the impact of the proposed redevelopment at No. 1644 on Henry Belford's house was studied in terms of the historical and architectural context of these and surrounding properties. Henry Belford's house expresses itself as a Late Victorian dwelling, vertically oriented, and with a picturesque roof line. The Ranch-style Bungalow at No. 1644 and the other mid-twentieth century bungalows that prevail on Glenburnie Road north of Pinetree Way are conversely oriented horizontally and positioned low to the ground. Along with two other houses in the block that do not conform to the prevailing pattern, there is considerable variation in building elevations on the street. Although architectural contrast already exists in the block, the new house as it was initially conceived would assert a sudden difference in character.

Through a number of iterations of the new house's design, changes were made to reduce the visual impact of the proposed house on Henry Belford's house and on the block. The design as submitted for City review shows the improved result.

Appendix A: Illustrations

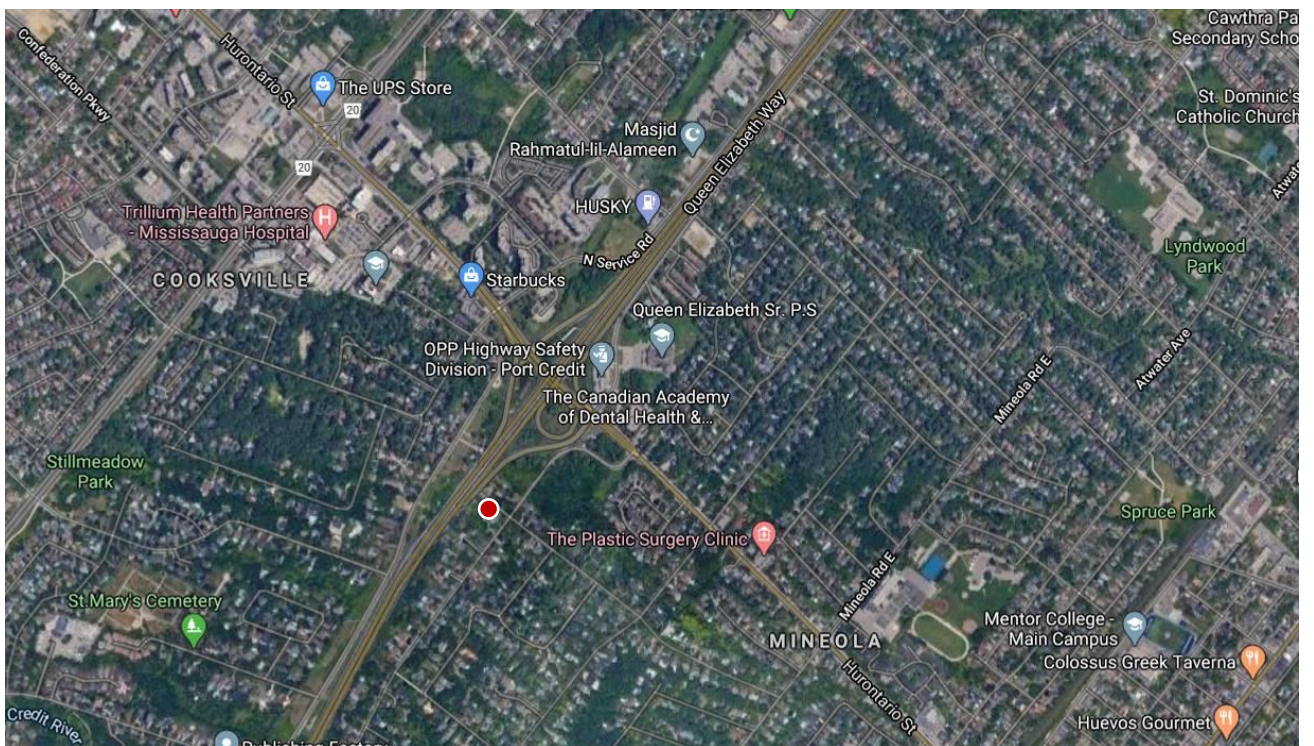
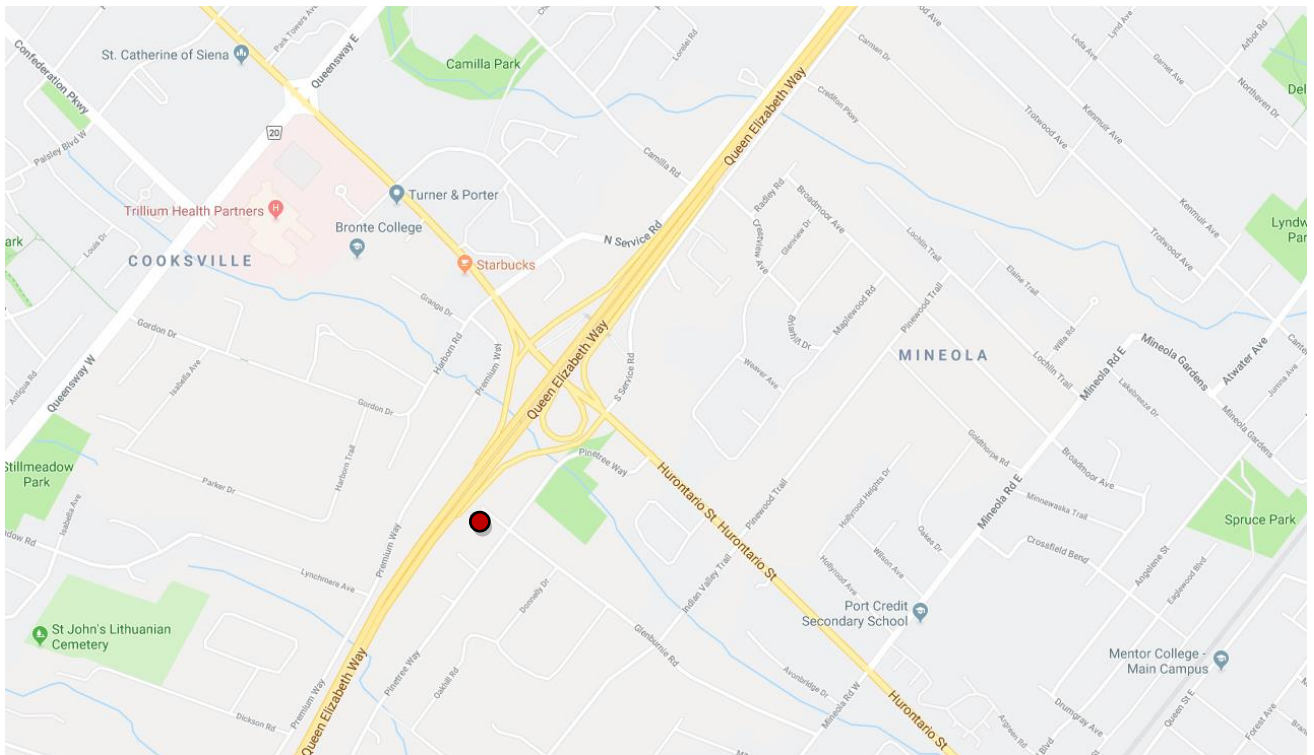


Fig. 1 The red dot on the Google street map and the red dot on the satellite image indicate the approximate location of 1644 Glenburnie Road.



Fig. 2 View of Glenburnie Road, looking north from Pinetree Way. In the distance, the grey and green concrete sound barrier along the Queen Elizabeth Way can be glimpsed. The barrier terminates Glenburnie Road.



Fig. 3 Detail from Zoning Map 08, revised 31 Jul. 2018. The red dot marks the approximate location of 1644 Glenburnie Road.

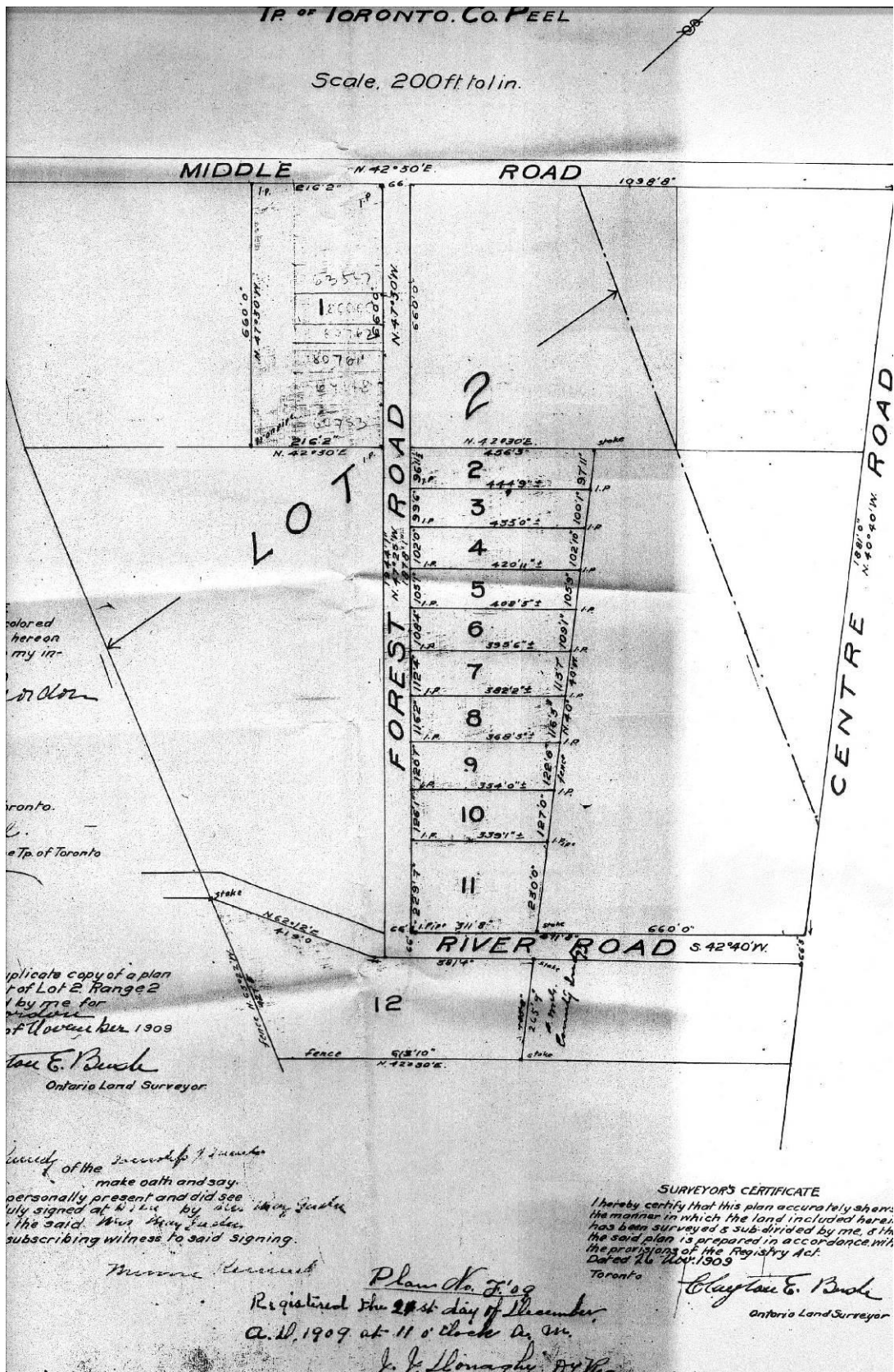


Fig. 4 Detail from Clayton E. Bush, "Plan of Subdivision of Part of Lot 2, Range 2, Credit Indian Reserve, Tp. of Toronto. Co. Peel: Registered Plan F-09," 21 Dec. 1909, Peel Land Registry Office.

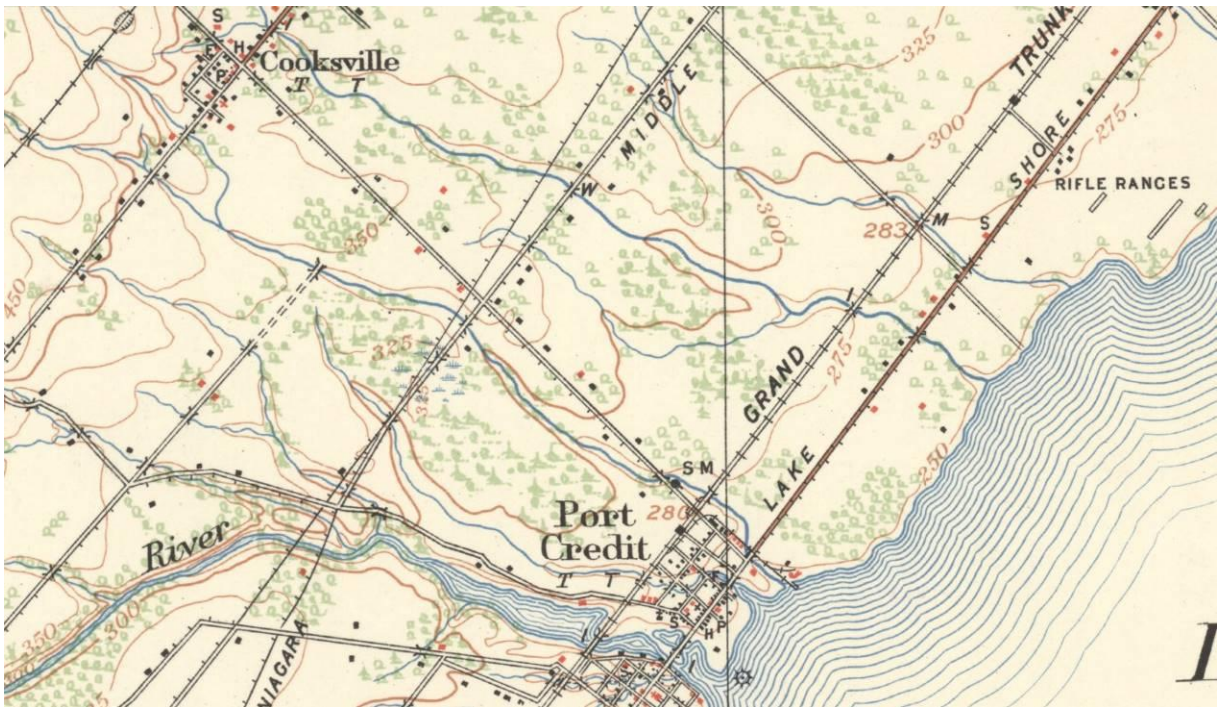


Fig. 5 Detail from Canada, Department of Militia and Defence, "Topographic Map, Ontario, Brampton Sheet, No. 35 [T./71.1909, 30M/12], surveyed 1907, printed 1909, https://ocul.on.ca/topomaps/map-images/HTDP63360K030M12_1909TIFF.jpg.



Fig. 6 Undated photograph of the Middle Road before its expansion as a superhighway, reproduced by Robert M. Stamp in *QEW: Canada's First Superhighway* (Erin, Ont.: Boston Mills Press, 1987), p. 14, from a photograph at the Archives of Ontario.



Fig. 7 Photograph of Henry Belford's house in the 1920s, reproduced by Heritage Mississauga.



Fig. 8 Photograph of Henry Belford as a middle-aged man – a digital image from Heritage Mississauga and sourced from the Peel Region Archives. The background to his photographic portrait could be his own house. Tongue-and-groove wood siding and a darkly painted corner board are similar to the cladding in Figure 7.

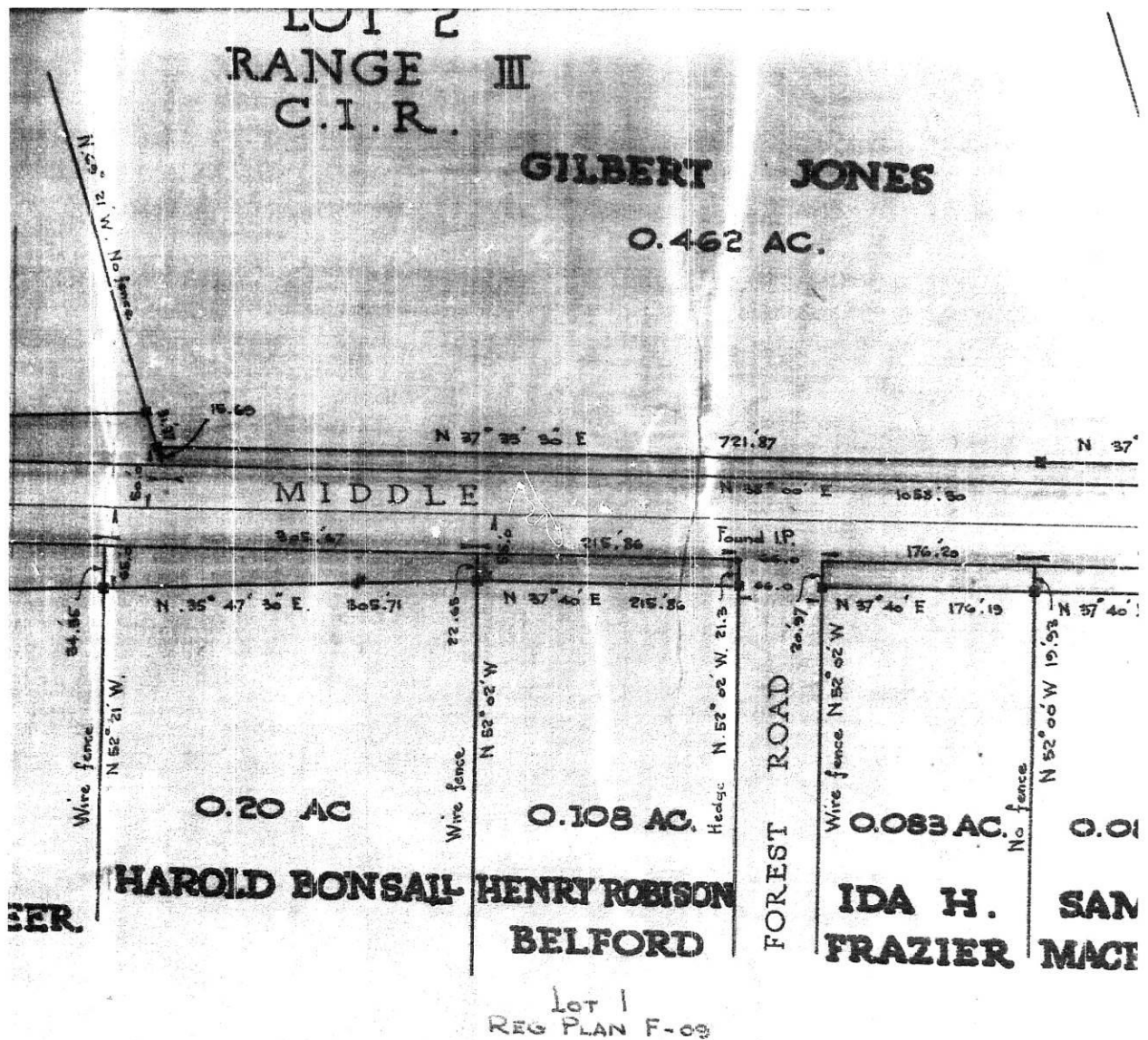


Fig. 9 Detail from John E. Jackson, "Plan of the King's Highway, Middle Road, in the Township of Toronto, County of Peel, Assumed by Deposited Plan No. 546 and Land Plan Shewing Diversion and Widening: Drawing No. P-1857-8," Deposit No. 557, 26 Apr. 1934, Peel Land Registry Office.

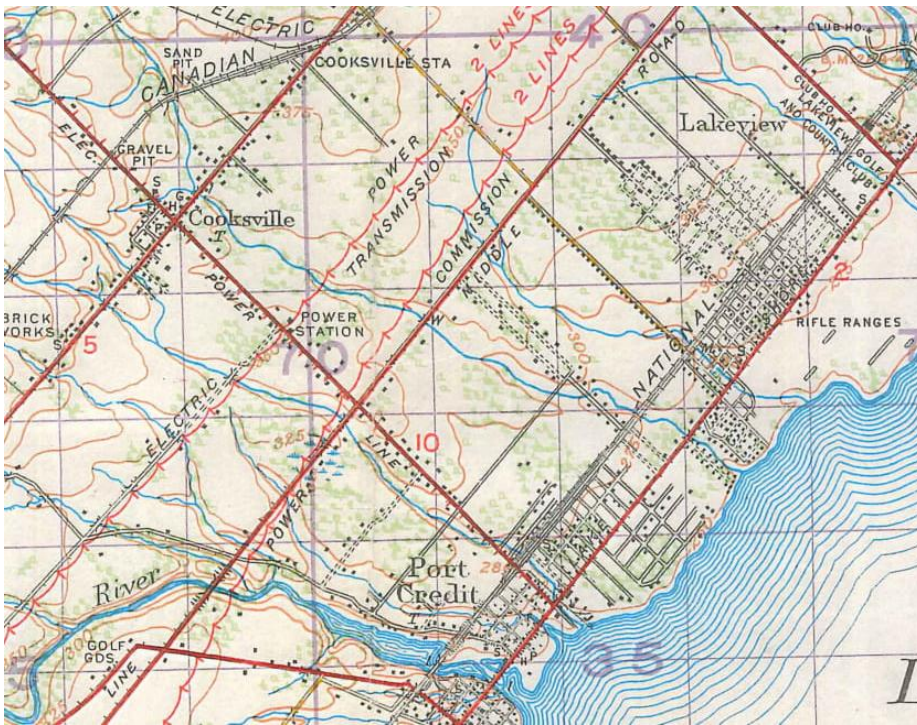


Fig. 10 Detail from Canada, Department of National Defence, "Topographic Map, Ontario, Brampton Sheet, 30M/12," 1938, Toronto Reference Library. The map shows the area before the Queen Elizabeth Way.

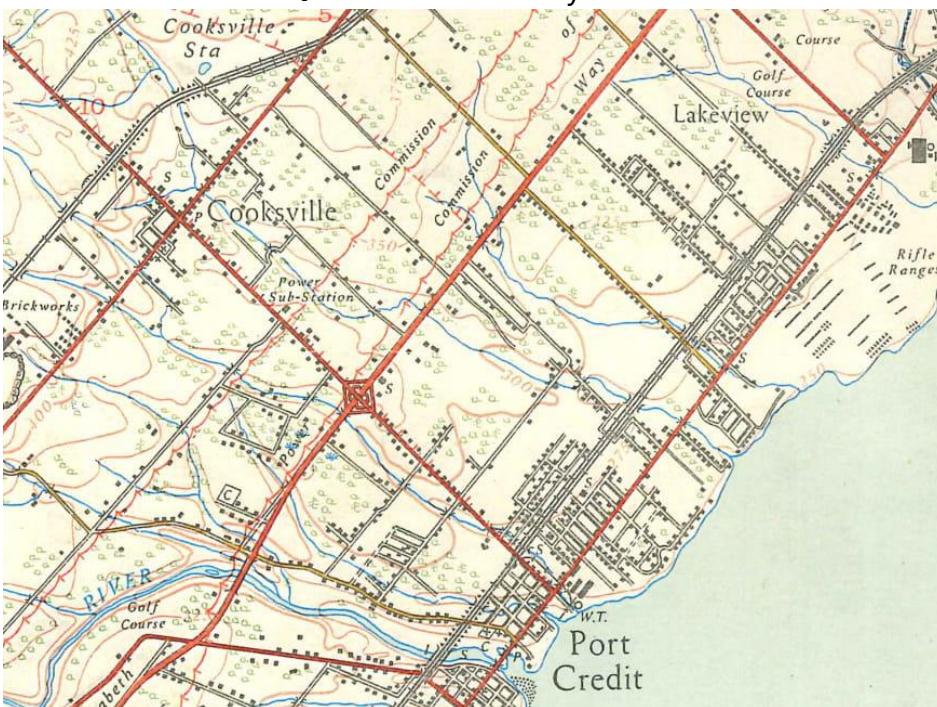


Fig. 11 Detail from Canada, Department of National Defence, "Brampton, Ontario, 30M/12," 1942, Toronto Reference Library. Forest Road, the original name for Glenburnie Road, is shown west of the Port Credit cloverleaf. Belford's house, on the west side of Forest Road, is shown along the Queen Elizabeth Way right-of-way. A swamp still lies south of Belford's house. The area remains wooded.

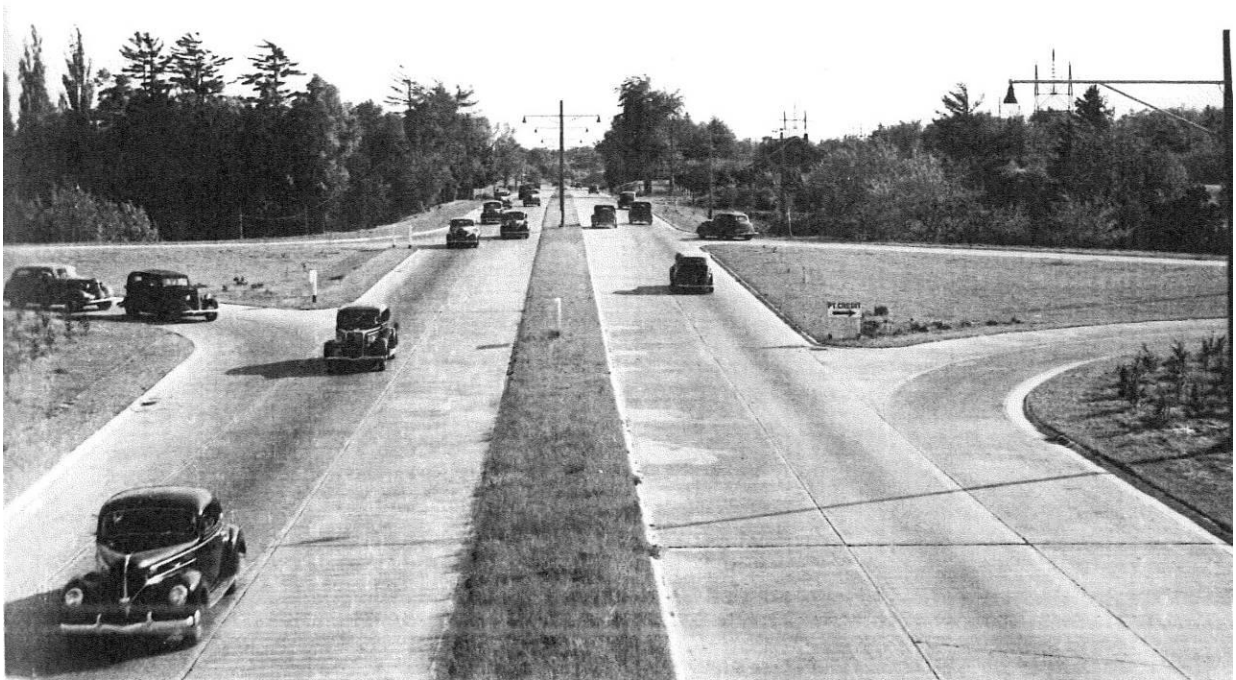


Fig. 12 Photograph of the Queen Elizabeth Way, looking west at Highway 10, Jun. 1939, reproduced from an Archives of Ontario photograph in Robert M. Stamp, p. 21. On the left, note the dense tree cover in the vicinity of Belford's property.

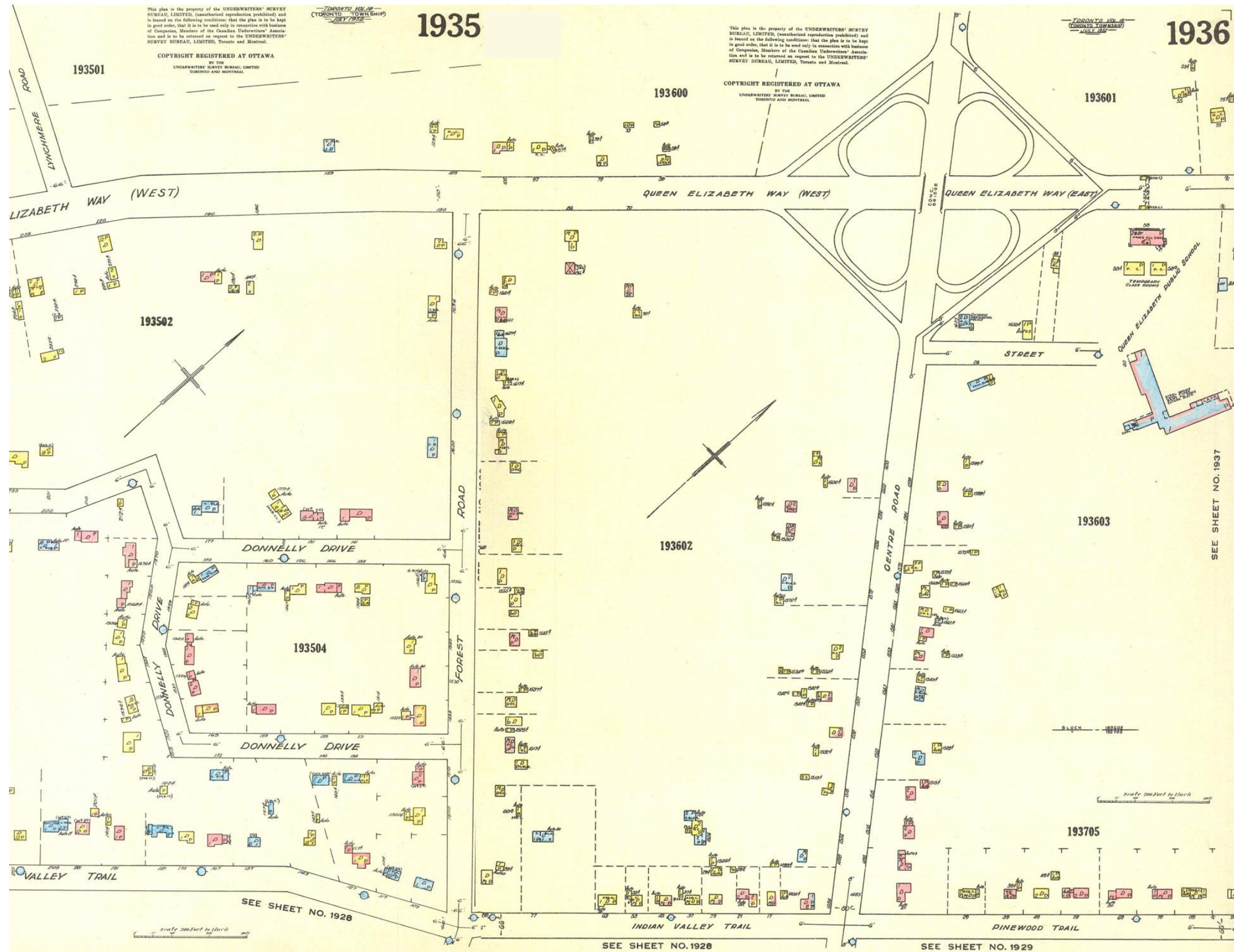


Fig. 13 Details from Underwriters' Survey Bureau, "Insurance Plan of the City of Toronto, Embracing the Village of Port Credit and Part of the Township of Toronto," V. 19 (Toronto & Montreal: Underwriters' Survey Bureau, Jul. 1952), pl. 1935 & 1936 [stitched together], Toronto Reference Library.

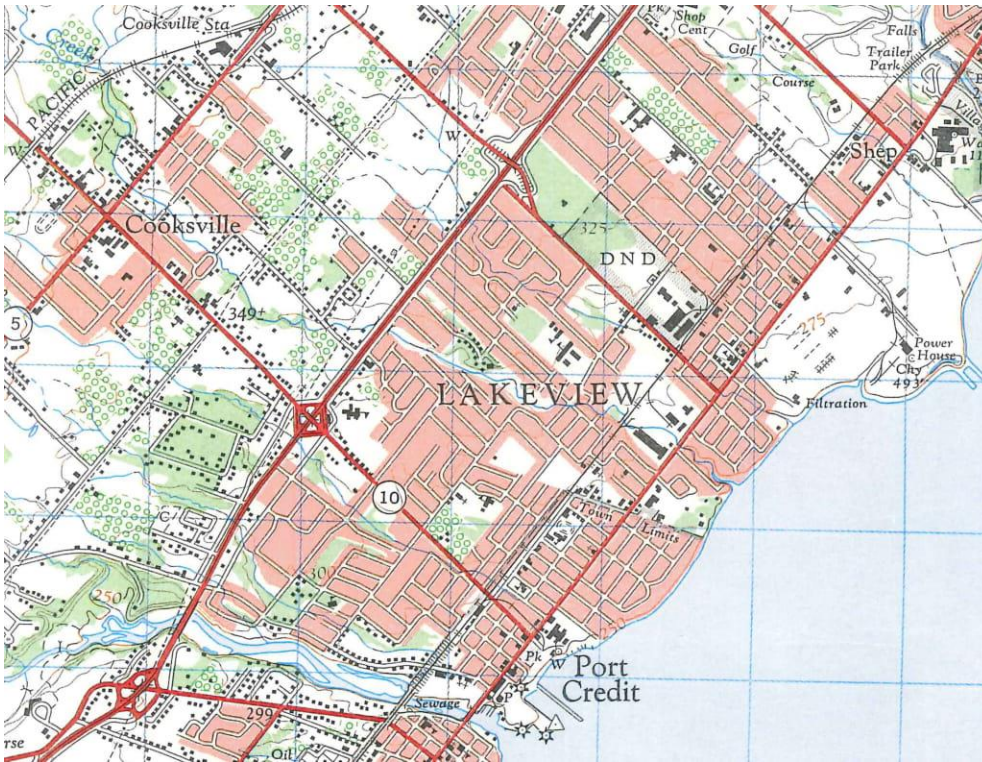


Fig. 14 Detail from Canada, Army Survey Establishment, "Brampton, Ontario, 30M/12," current as of 1960, printed 1964, Toronto Reference Library. The areas coloured pink represent built-up areas.



Fig. 15 Detail from aerial photograph, 1963, City of Toronto Archives, <https://www.toronto.ca/city-government/accountability-operations-customer-service/access-city-information-or-records/city-of-toronto-archives/whats-online/maps/aerial-photographs/aerial-photographs-1963>, pl. 21.

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Fig. 17 Front facade (east elevation) of 1644 Glenburnie Road, 5 May 2019



Fig. 18 Corner view, showing south elevation and front facade



Fig. 19 North elevation and narrow north side yard, adjacent to the lot line with 1654 Glenburnie Road, as seen from the backyard of 1644 Glenburnie Road. In the distance, on the other side of the street, stands another Ranch-style Bungalow.



Fig. 20 Rear (west) elevation



Fig. 21 Corner view, showing rear and south elevations



Fig. 22 View of backyard at 1644 Glenburnie Road, looking east from the rear lot line. On the left is Henry Belford's house at 1654 Glenburnie Road, and on the right is another bungalow at 1640 Glenburnie Road.



Fig. 23 View of backyard swale, looking south, and bridge over it



Fig. 24 Close-up view of swale



Fig. 25 Weeping willows at north lot line



Fig. 26 View of backyard and rear elevation from Magenta Court, through the wide north side yard of 1623 Magenta Court



Fig. 27 Front entry hall. Note the original door with its octagonal and rectangular cut-outs and single sidelight.



Fig. 28 Combination living and dining room, looking east



Fig. 29 Combination living and dining room, looking west



Fig. 30 Oak floor in combination living and dining room



Fig. 31 Plaster ceiling and cornice (crown moulding) in combination living and dining room



Fig. 32 Another detail of plaster ceiling and cornice, the cornice rounding the corner of the "L"-shaped room



Fig. 33 Kitchen



Fig. 34 Bedroom hall



Fig. 35 Bedroom closest to front entry hall



Fig. 36 Bathroom. Note pink bathtub from the mid-twentieth century.

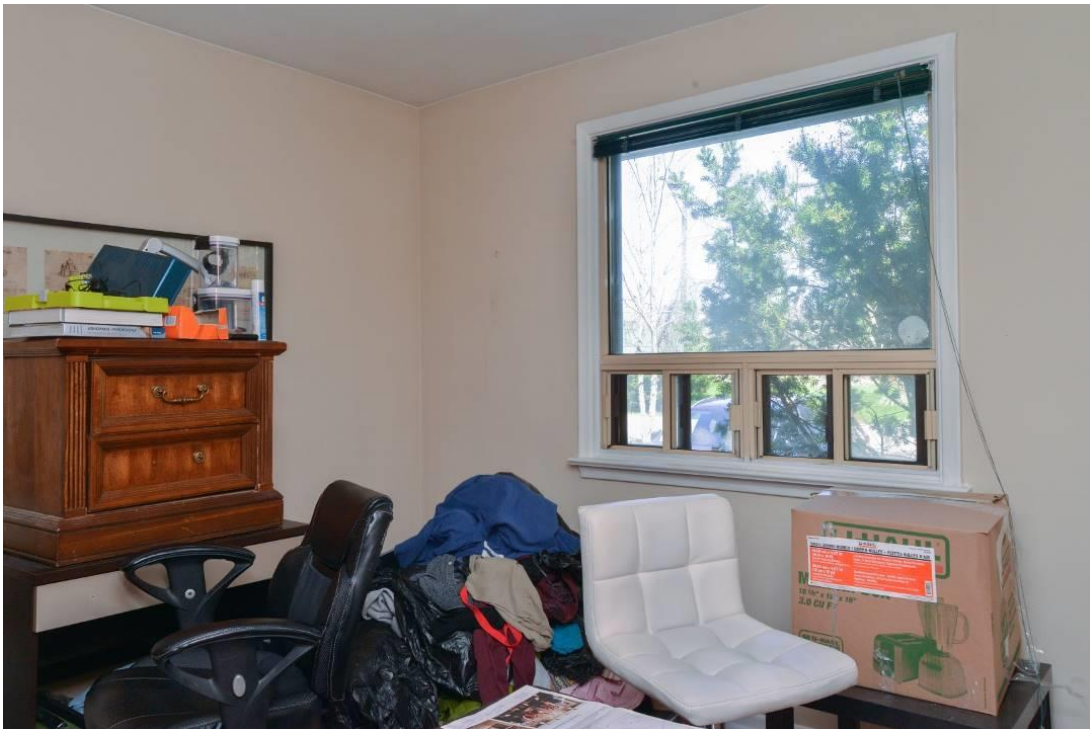


Fig. 37 Bedroom in the bungalow's northeast corner



Fig. 38 Bedroom in the northwest corner



Fig. 39 Basement staircase, looking down to sliding glass doors at the landing and to the backyard deck



Fig. 40 Basement apartment living room, originally the family's recreation room. Note the fireplaces and wood ceiling.



Fig. 41 Basement hall, looking north



Fig. 42 Basement kitchen



Fig. 43 1654 Glenburnie Road, looking northwest across its lot. Note the northeast corner of 1644 Glenburnie Road on the left.



Fig. 44 Corner view of 1654 Glenburnie Road, showing rear (south) and east elevations. Note how close the highway sound barrier is in relation to the house.



Fig. 45 Detail of concrete foundation



Fig. 46 Rear (south) elevation



Fig. 47 East elevation



Fig. 48 Corner view, showing east and north (front) elevations



Fig. 49 Corner view, showing west and rear elevations, and backyard



Fig. 50 View of backyard from north lot line at 1644 Glenburnie Road



Fig. 51 Photo montage of the west side of Glenburnie Road, north of Pinetree Way



Fig. 52 1628 Glenburnie Road at the northwest corner of Glenburnie Road and Pinetree Way



Fig. 53 1640 Glenburnie Road and the southeast corner of 1644 Glenburnie Road



Fig. 54 1627 Glenburnie Road, in a roughcast plaster cladding



Fig. 55 1637 Glenburnie Road



Fig. 56 1643 Glenburnie Road, opposite the bungalow at 1644 Glenburnie Road



Fig. 57 1653 Glenburnie Road, opposite Henry Belford's house



Fig. 58 View of swale and concrete culvert at Pinetree Way, looking north



Fig. 59 137 Pinetree Way, west of swale



Fig. 60 1613 Magenta Court at the northeast corner of Magenta Court and Pinetree Way



Fig. 61 1623 Magenta Court



Fig. 62 1633 Magenta Court



Fig. 63 1609 Glenburnie Road, at the southeast corner of Glenburnie Road and Pinetree Way



Fig. 64 1599 Glenburnie Road



Fig. 65 1593 Glenburnie Road



Fig. 66 1587 Glenburnie Road



Fig. 67 1577 Glenburnie Road



Fig. 68 1569 Glenburnie Road

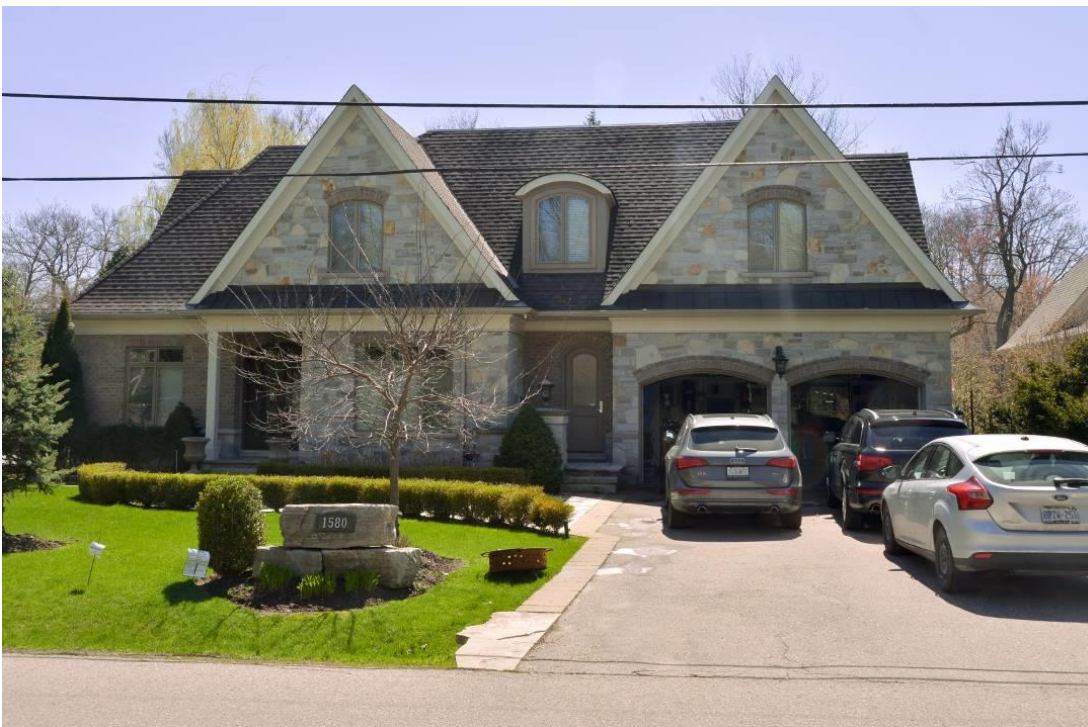


Fig. 69 1580 Glenburnie Road



Fig. 70 1590 Glenburnie Road



Fig. 71 1600 Glenburnie Road



Fig. 72 1610 Glenburnie Road



Fig. 73 Kenollie Public School, 1376 Glenwood Drive



Fig. 74 Another view of Kenollie Public School



Fig. 75 1435 Glenwood Drive, a mid-twentieth century Modern house (what the architectural historian, John Blumenson, calls '50s Contempo style) and a naturalistic landscape setting to complement it



Fig. 76 1445 Glenwood Drive, another mid-twentieth century Modern house



Fig. 77 1455 Glenwood Drive, perhaps a mid-twentieth century Modern house that has been revamped



Fig. 78 250 Mineola Road West, south of Kenollie Public School – an early twenty-first century house



UNIT 808 7191 YONGE ST, THORNHILL, ON

8.1



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1644 GLENBURNIE RD

LOT AREA: 1521.24 [m2] ZONNING: R2-4

BUILDING

ITEM	BY-LAW O225-2007 ALLOWANCE	PROPOSED
COVERAGE	30%,456.37 m2	37.71%, 134.66 m2
FSI	190 + 0.20 x LOT AREA, 494.25 m2	651.7m2 house 49.5m2 garage
DEPTH	20.0 m	23.20 m
HEIGHT	7.5 m	9.21 roof level 1 10.15 roof level 2 10.70 roof level 3
FRONTAGE	22.5m	23.10 m

LANDSCAPING

FRONT, TOTAL	40% (FRONT YARD 224.67 m2) 89.87 m2	124.89 m2
--------------	--	-----------

SETBACKS

FRONT	9.0 m	9.00 m
to awning		6.76 m
to architectural feature		5.20 m
REAR	7.5 m	9.1 m
SIDE	2.41 m	2.41 m
COMBINED SIDE	27% OF LOT WIDTH, 6.25 m	6.58 m

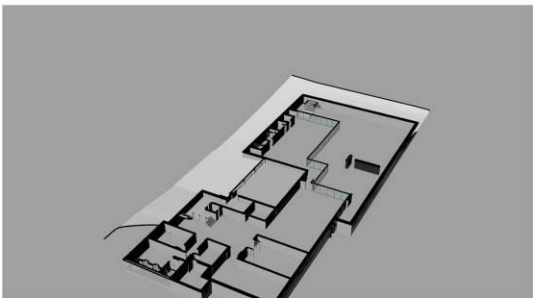
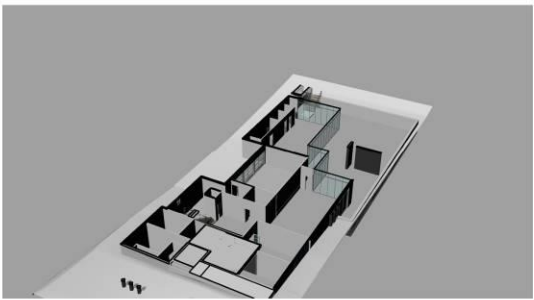
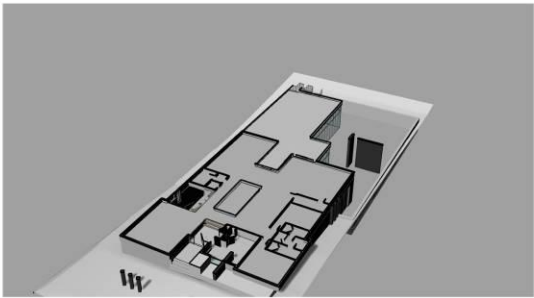
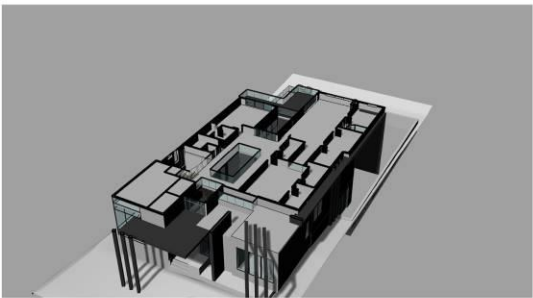
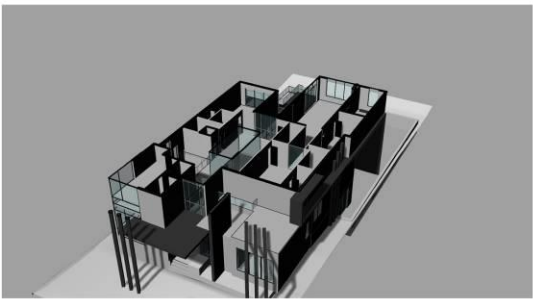
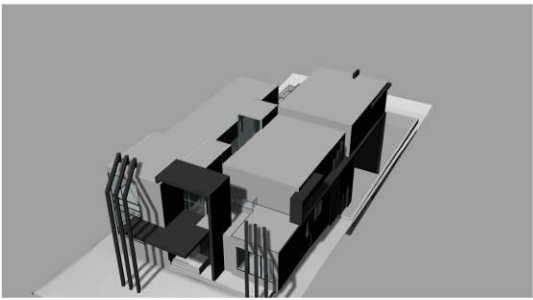


Fig. 79 6ixDesign, "1644 Glenburnie Rd: Top Sheet," 1 Sept. 2018.



: UNIT 808-7191 YONGE ST,
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: INFO@6IXDESIGN.CA

PROJECT ADDRESS: 1644 Glenburnie Rd, Mississauga, ON

PROJECT DESCRIPTION: NEW 2 STOREY DETACHED DWELLING

CONSULTANTS:

PRIME CONSULTANT(ARCHITECTURAL, STRUCTURAL, MEP):
6IXDESIGN ENGINEERING INC

PLANNER:
THORSTONE CONSULTING

HERITAGE:
PAUL DILSE

ARBORIST:
AL MILEY & ASSOCIATES

CIVIL ENGINEER:
N&N ENGINEERING LTD

LANDSCAPE ARCHITECT:
DIANA NOTENBOOM

ARCHEOLOGY:
A.M.ARCHAEOLOGICAL ASSOCIATES

INTERIOR AND GREEN ROOF LANDSCAPE ARCHITECT:
VERTICAL LANDSCAPE ARCHTIECT

CONTENTS:

A000	TITLE PAGE
A001	SITE PLAN
A100	BASEMENT PLAN
A101	1ST FLOOR PLAN
A102	2ND FLOOR PLAN
A103	ROOF PLAN
A200	FRONT ELEVATION
A201	REAR ELEVATION
A202	SE ELEVATION NW
A203	ELEVATION
A300	ZONING CALC

1644 GLENBURNIE RD

LOT AREA:		LOT FRONTAGE:	ZONNING:
1523.21 m2		23.16m	R2-4
BUILDING			
ITEM	BY-LAW O225-2007 ALLOWANCE	PROPOSED	
COVERAGE	30%, 456.96 m2	317.26 m2 house 48.97 m2 canopy/balcony 110.18 m2 deck/pool 31.27%, 476.41 m2 TOTAL	
GFA	190 + 0.20 x LOT AREA, 494.25 m2	551.27 m2 house 32.84 m2 garage 584.11 m2 TOTAL	
DEPTH	20.0 m	23.87 m	
HEIGHT	7.5 m	8.57 9.49 10.00	ROOF L1 ROOF L2 ROOF L3

LANDSCAPING

FRONT, TOTAL	40% x FRONT YARD	147.01 m2
FRONT YARD=266.88 m2	106.57 m2	

SETBACKS

FRONT to HOUSE to Architectural Feature	9.0 m	10.83 m 9.00 m	HOUSE PORCH
REAR	7.5 m	34.14 m 30.50	1ST FLOOR 2ND FLOOR
SIDE	2.41 m	2.78m 2.17m 4.57m	NW FOUNDATION NW CANTELIVER SE WALL
COMBINED SIDE	27% OF LOT WIDTH, 6.25 m	7.35 m	



A000-1
SCALE: NTS
FRONT NORTH RENDER



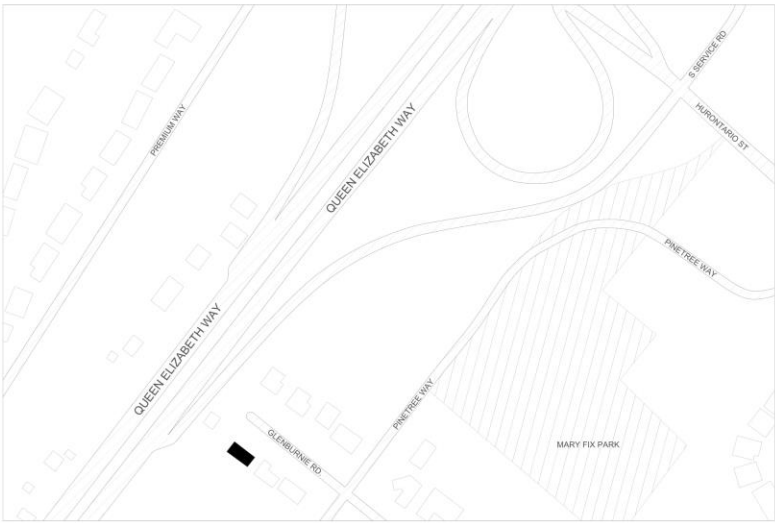
A000-2
SCALE: NTS
FRONT SOUTH RENDER



A000-3
SCALE: NTS
FRONT LANDSCAPE RENDER

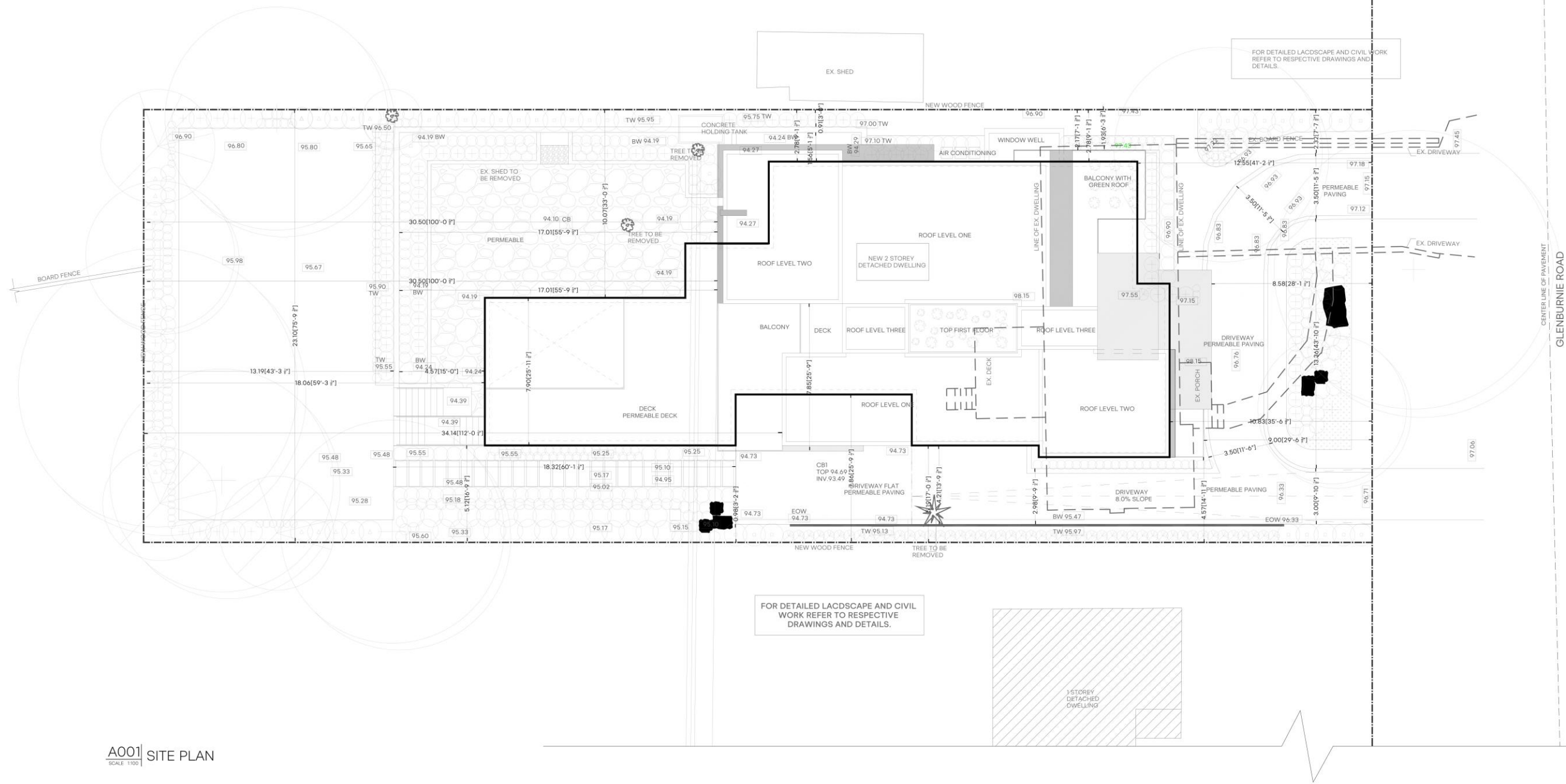


A000-4
SCALE: NTS
REAR LANDSCAPE RENDER



A000-5
SCALE: NTS
KEY PLAN

Fig. 80-A to 80-K 6ixDesign, "1644 Glenburnie Rd: Top Sheet, Site Plan, Floor Plans, Roof Plan and Elevations," 7 May 2020.



A001 SITE PLAN
SCALE: 1:100



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ISSUED FOR REVIEW	08.09.19
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ISSUED FOR SPA	07.05.20

A01

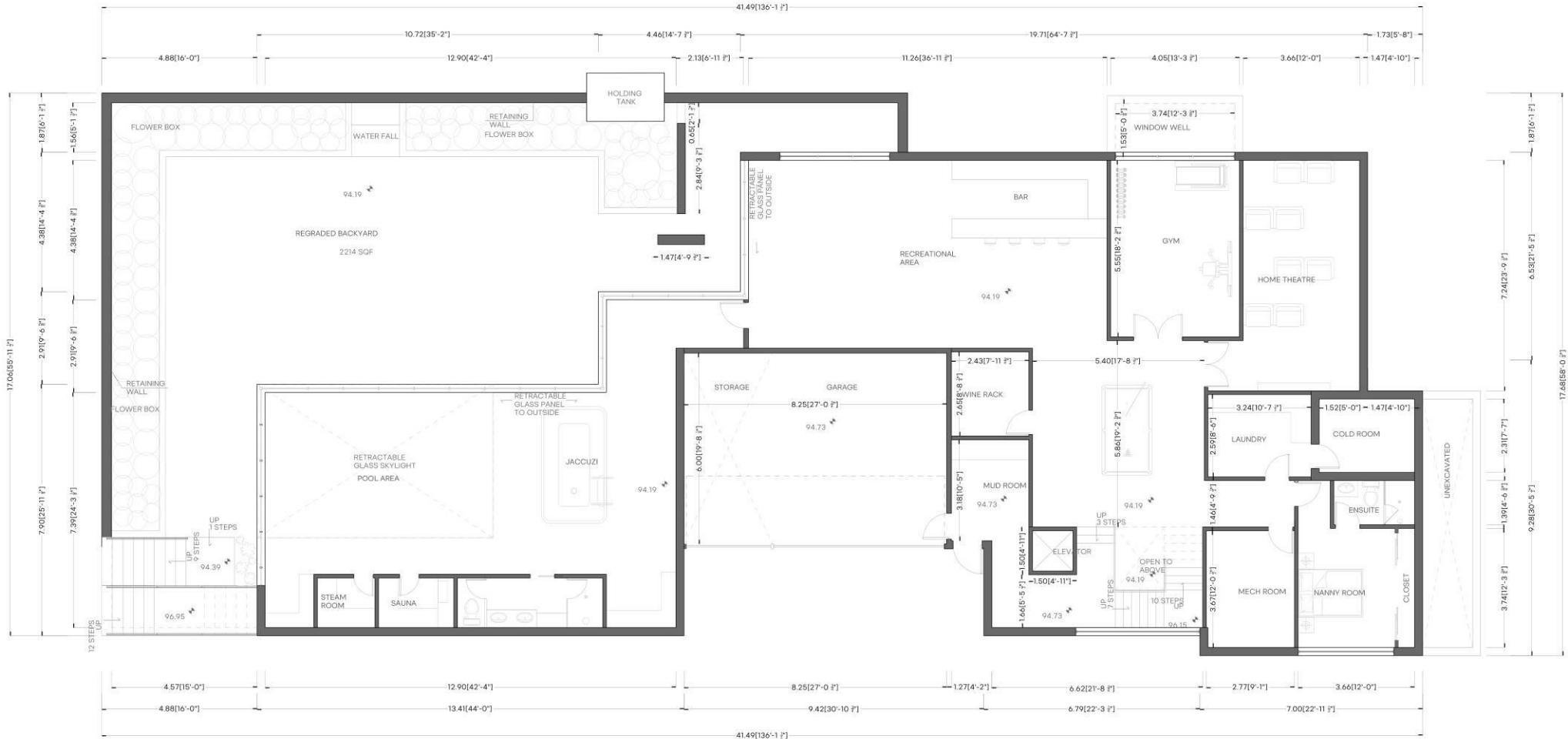
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SITE PLAN

SCALE 1:100



Fig. 80-B



A100 BASEMENT FLOOR



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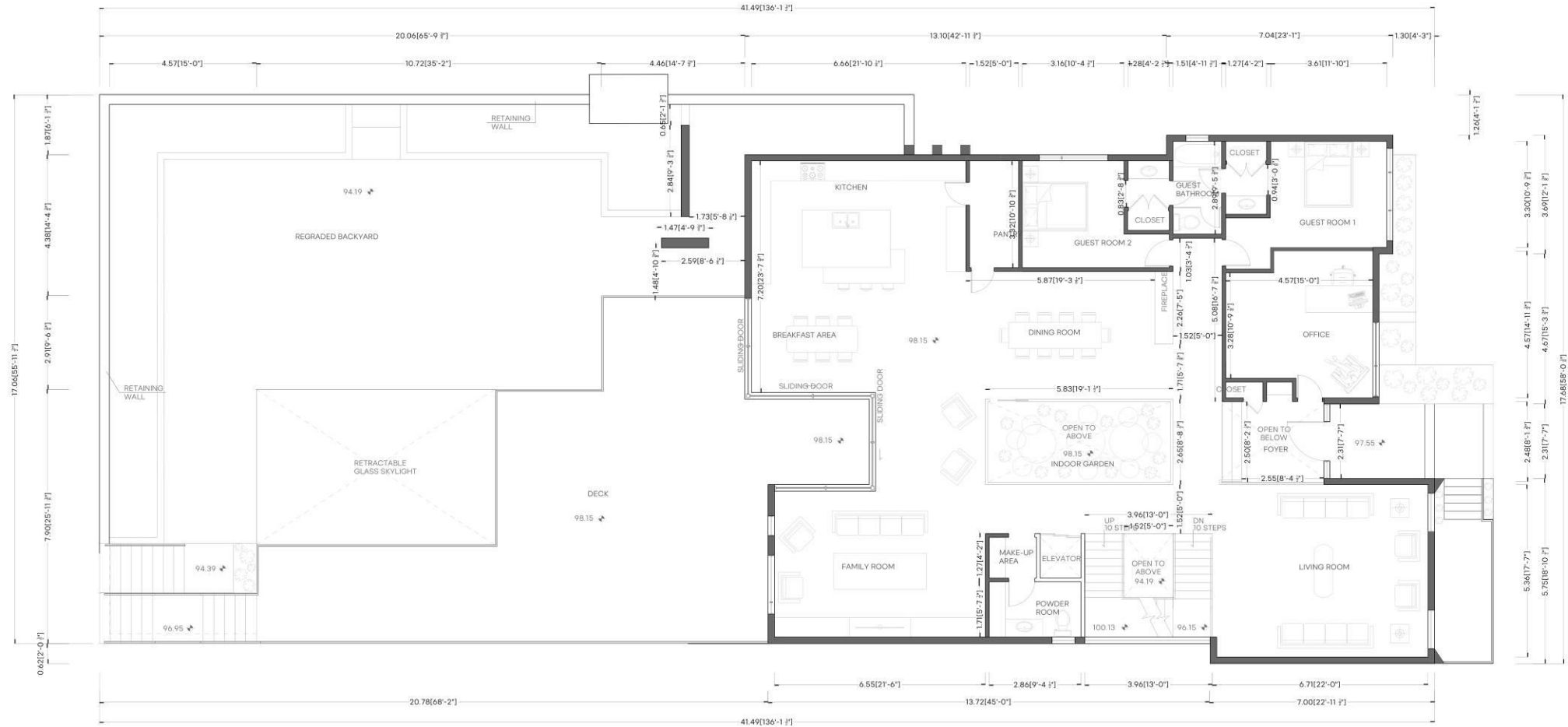
A100 FLOOR PLAN

PROJECT NUMBER 1809-007
DRAWN BY Y.A.
CHECKED BY F.T.

SCALE 1:75



Fig. 80-C



A101 | FIRST FLOOR
SCALE: 1/75



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ISSUED FOR SPA	07.04.20

A101 FLOOR PLAN

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SCALE 1:75



Fig. 80-D



A102 | SECOND FLOOR
SCALE: 1/8"



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A102 FLOOR PLAN

PROJECT NUMBER 1809-007
DRAWN BY Y.A.
CHECKED BY F.T.

SCALE 1:75

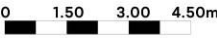
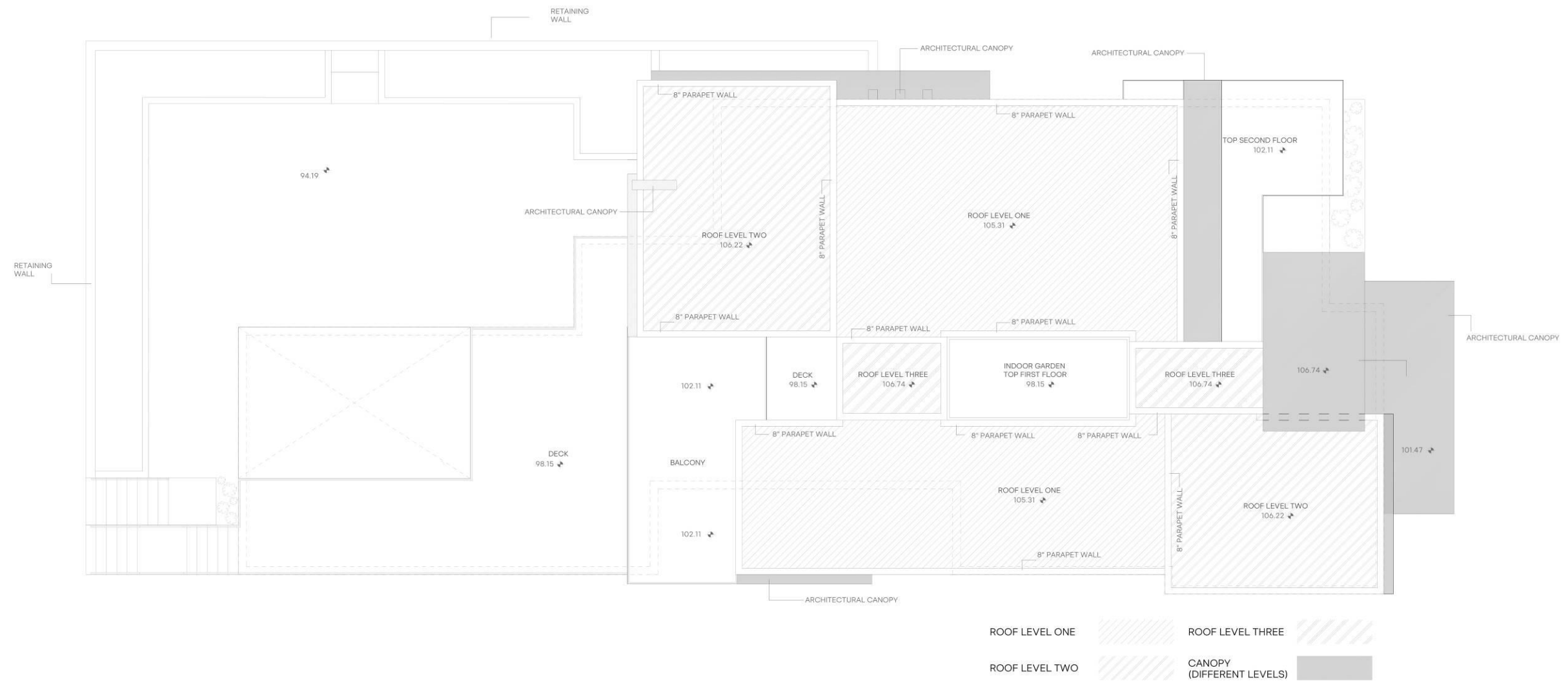


Fig. 80-E



A103 | ROOF PLAN
SCALE 1:75



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ISSUED FOR REVIEW	23.09.19
ISSUED FOR SPA	07.04.20

A103

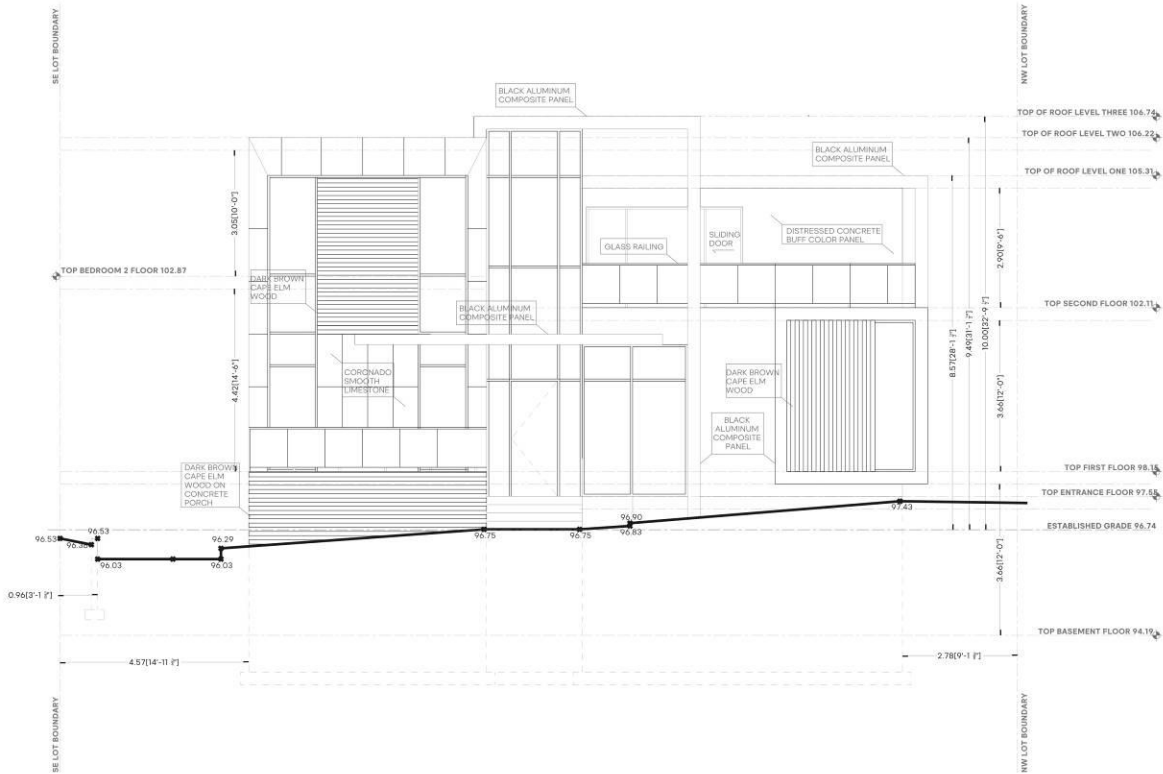
PROJECT NUMBER	1809-007
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CHECKED BY	F.T.

ROOF PLAN

SCALE 1:75



Fig. 80-F



A200
SCALE 1:75

NE ELEVATION(FRONT)



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A200

PROJECT NUMBER 1809-007
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CHECKED BY F.T.

FRONT
ELEVATION
SCALE 1:75

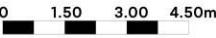
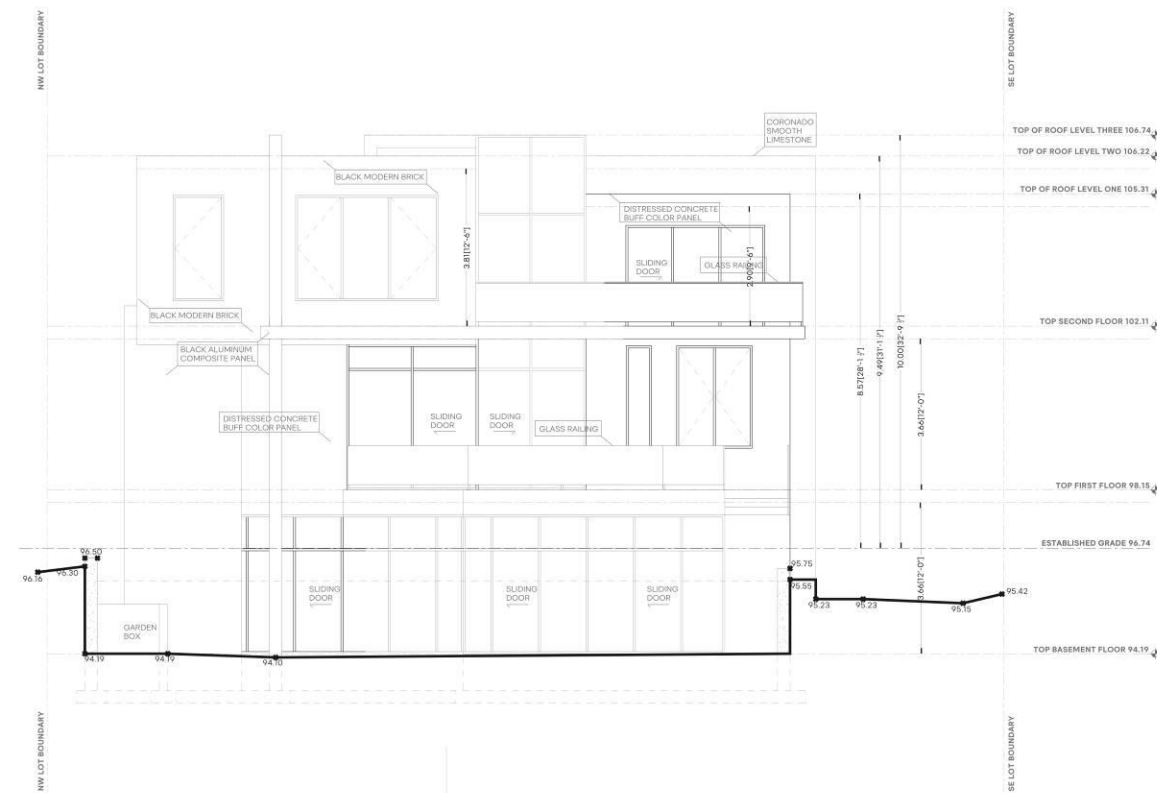


Fig. 80-G



A201 | SW ELEVATION(REAR)
SCALE 1/75



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ISSUED FOR REVIEW	23.09.19
ISSUED FOR SPA	07.04.20

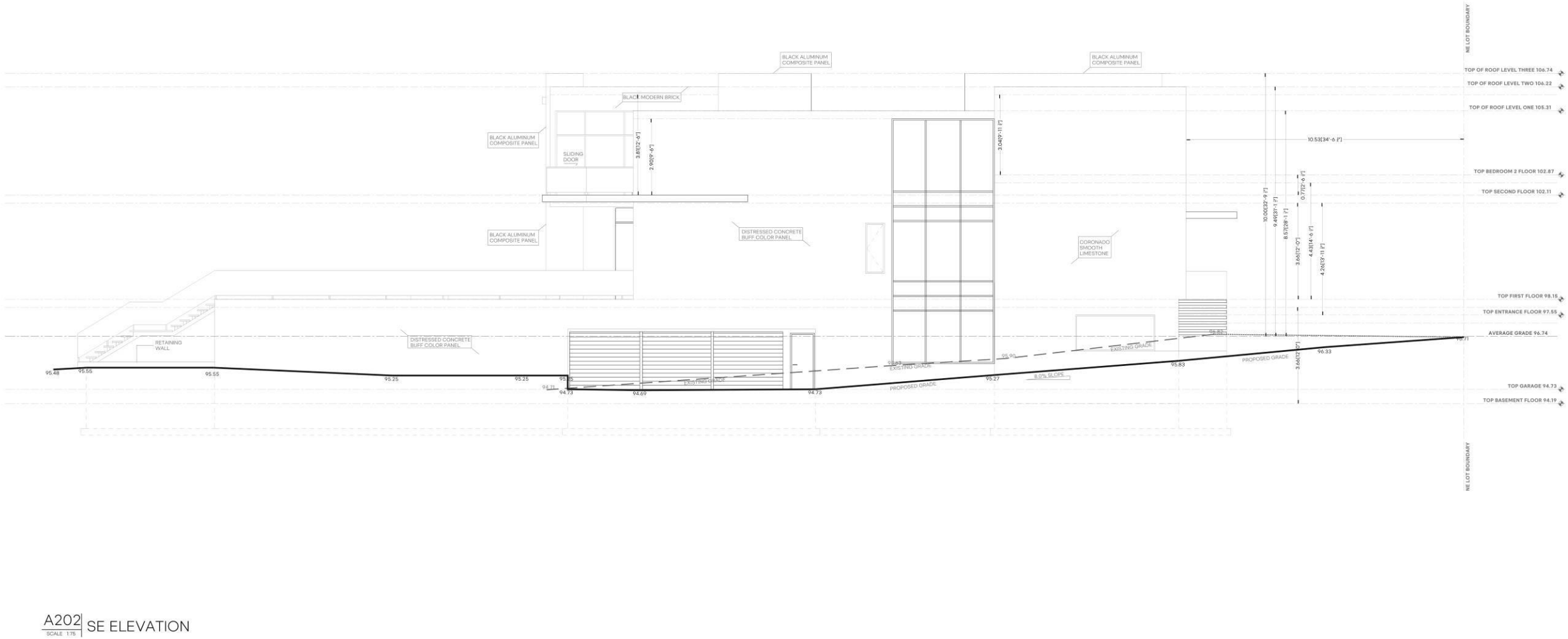
A201

PROJECT NUMBER 1809-007
DRAWN BY Y.A.
CHECKED BY F.T.

BACK
ELEVATION
SCALE 1:75



Fig. 80-H



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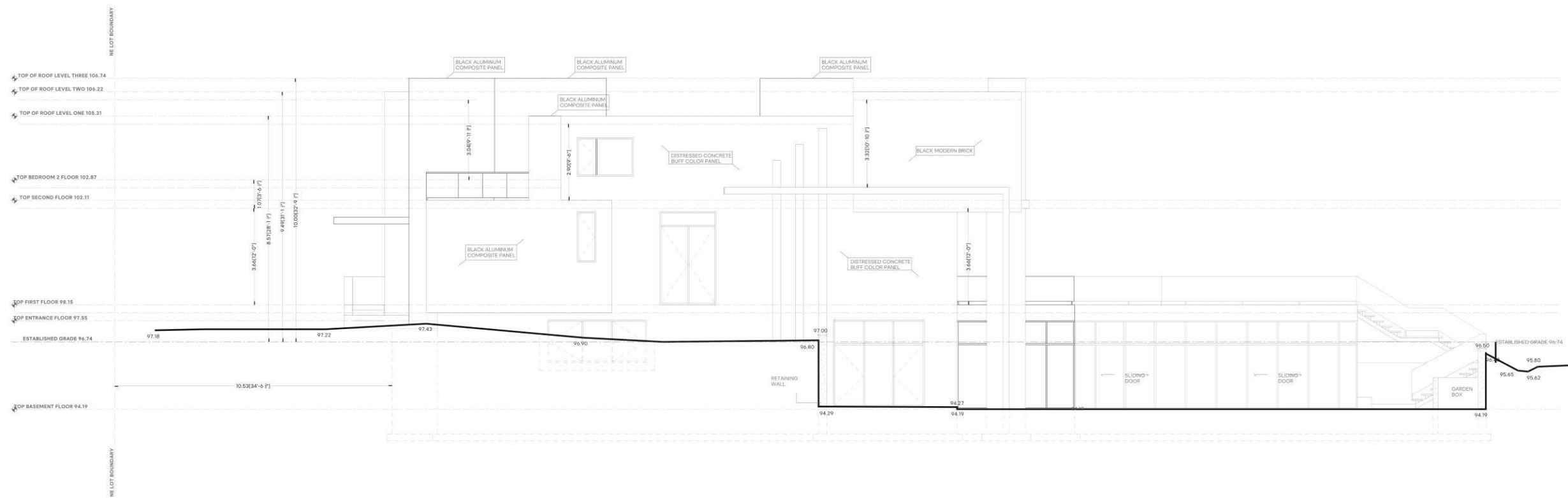
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A202
PROJECT NUMBER 1809-007
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CHECKED BY F.T.

SIDE
ELEVATION
SCALE 1:75



Fig. 80-I



A203 NW ELEVATION
SCALE 1/75



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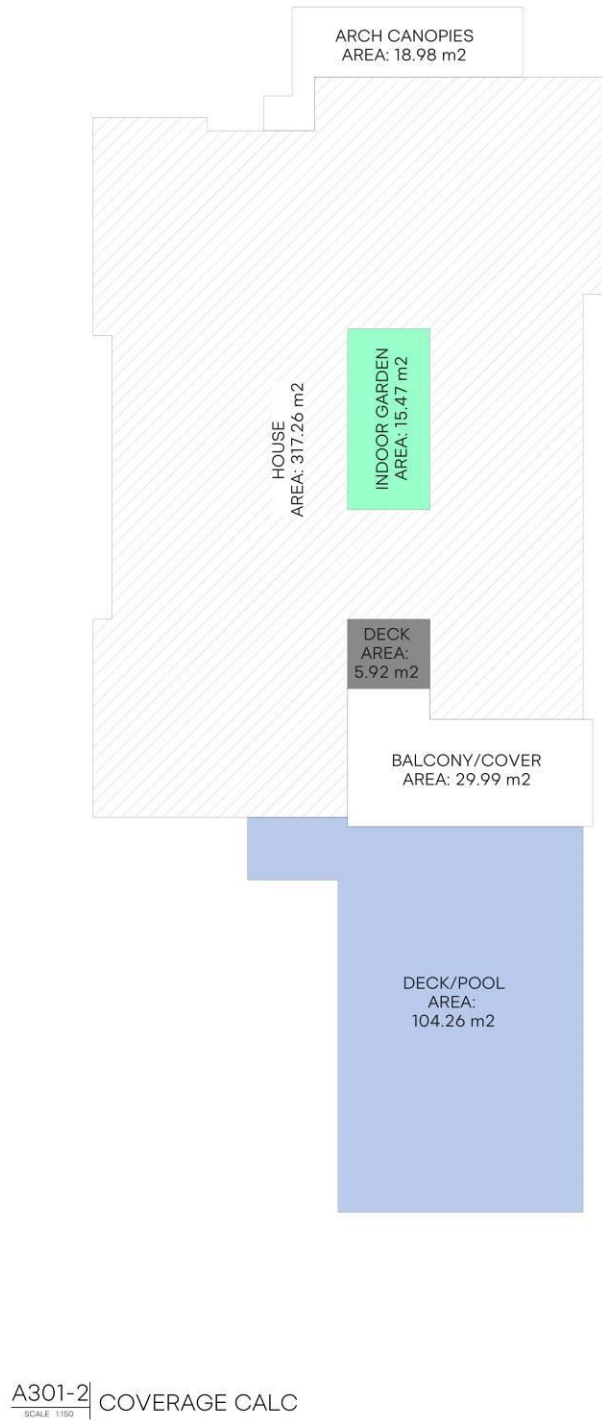
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ISSUED FOR REVIEW	23.09.19
ISSUED FOR SPA	07.04.20

A203
PROJECT NUMBER 1809-007
DRAWN BY Y.A.
CHECKED BY F.T.

SIDE
ELEVATION
SCALE 1:75



Fig. 80-J





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A300 ZONING CALC

PROJECT NUMBER 1809-007

DRAWN BY Y.A.

CHECKED BY F.T.

SCALE 1:150

0 3.00 6.00 9.00m

Fig. 80-K

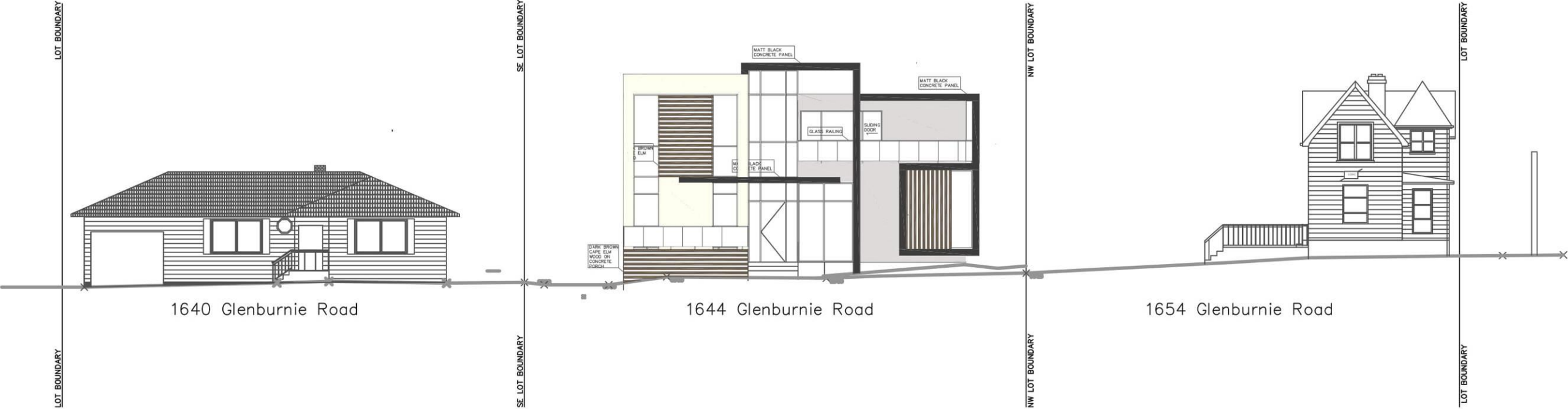


Figure 81A: Streetscape



Figure 81B: Streetscape with vegetation

**Streetscape Study for the
Heritage Impact Assessment**

1644 Glenburnie Road, Mississauga, Ontario

FRANCINE ANTONIOU ARCHITECT OAA, CAHP-ACEP, LEED AP 647 225 4246

Streetscape Elevations, Figure 81

1/16"=1'-0"

DATE: June, 2020

Elevations were provided by
6IX Design Engineering, May 2020.

Fig. 81 A to D Francine Antoniou, "Streetscape Elevations," June 2020.

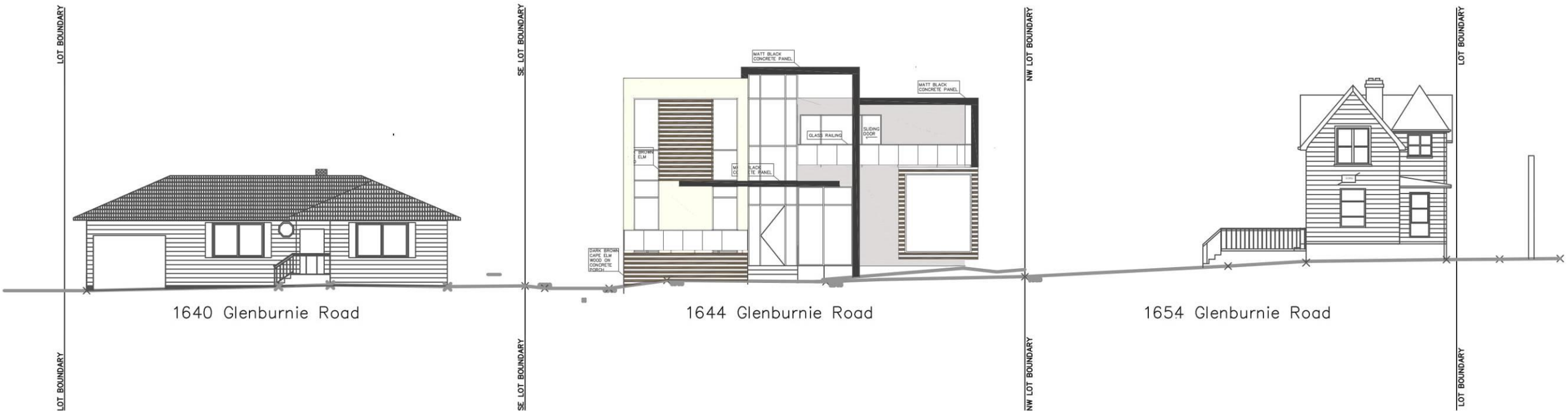


Figure 81C: Alternative Streetscape

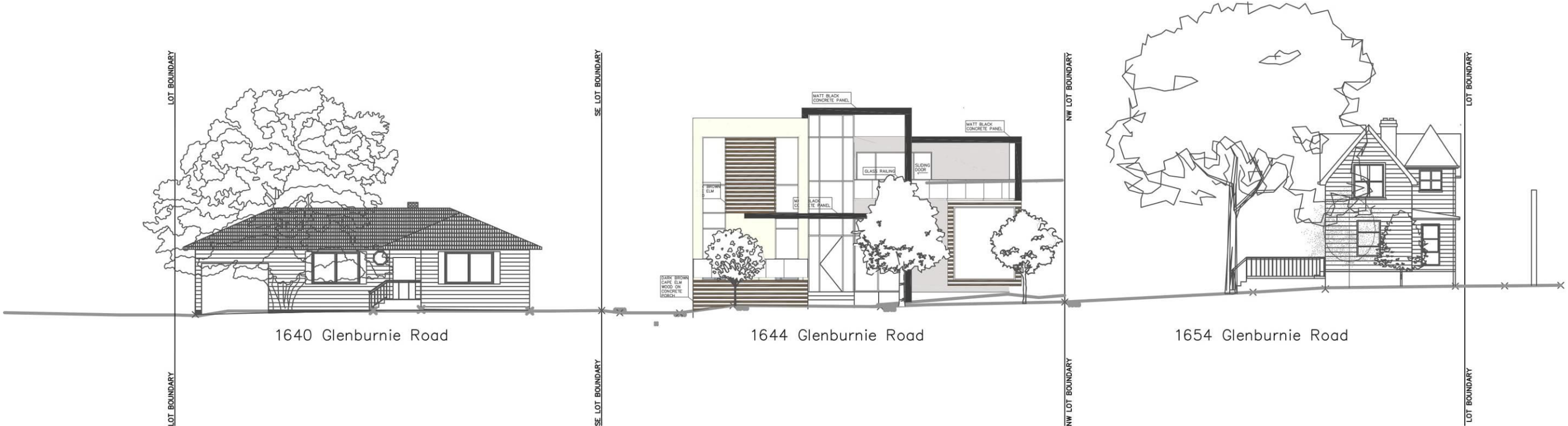


Figure 81D: Alternative Streetscape with vegetation

Streetscape Study for the
Heritage Impact Assessment

1644 Glenburnie Road, Mississauga, Ontario

FRANCINE ANTONIOU ARCHITECT OAA, CAHP-ACEP, LEED AP 647 225 4246

Streetscape Elevations, Figure 81

1/16"=1'-0"

DATE: June, 2020

Elevations were provided by
6IX Design Engineering, May 2020.

Fig. 81 A to D Francine Antoniou, "Streetscape Elevations," June 2020.

Appendix B: Bibliography

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Appendix C: Author's Qualifications

Paul Dilse has specialized in heritage planning and historical study since his graduation from the professional planning school at the University of Waterloo in 1979.

He has written official plan policies on heritage conservation for the former Municipality of Metropolitan Toronto and for the City of Cambridge (his related official plan background study, in which he delineated the boundaries of prospective heritage conservation districts, remained a reference document there for three decades). He has surveyed the entire rural and exurban municipality of the Town of Caledon to compile a comprehensive inventory of built heritage resources located on 1,643 properties. He has assessed the cultural heritage value of two French Canadian Roman Catholic churches in rural Essex County as well as the cultural heritage landscape of the David Dunlap Observatory in Richmond Hill, and successfully defended their designation under the *Ontario Heritage Act* at Conservation Review Board hearings. He has also provided expert witness testimony at the Ontario Municipal Board and Local Planning Appeal Tribunal, successfully defending the designation of the first heritage conservation district in the Town of Markham and the second heritage conservation district in the Town of Whitby as well as contributing to the positive outcome in favour of retaining a complex of rare garden apartments in the Leaside neighbourhood of Toronto.

In addition to the Thornhill-Markham heritage conservation district and Werden's Plan neighbourhood district in Whitby, he has produced heritage conservation district plans for Old Port Credit Village in Mississauga, the MacGregor/Albert neighbourhood in Waterloo, and Lower Main Street South in Newmarket. Another study of his – pertaining to the George Street and Area neighbourhood in Cobourg – has supported this area's designation as a heritage conservation district. He is also the author of a report on the feasibility of establishing heritage conservation districts in downtown Brampton. His knowledge of heritage conservation districts spans 35 years – from the time when he reviewed heritage conservation district plans for the provincial government in the early 1980s to the post-2005 era when amendments to the *Ontario Heritage Act* clarified and strengthened Part V of the Act. He has also prepared voluntary conservation-based design guidelines for the historic commercial centres of Alliston, Beeton, Tottenham and Picton.

Since 2004 when municipalities in Central and Southwestern Ontario started requesting heritage impact assessments from him, he has completed 59 such reports – 14 for

subjects in Mississauga. In addition to the heritage impact assessments, he has described and evaluated many other historic properties, for instance, Delta Collegiate Institute in Hamilton. Its 2014 designation under the *Ontario Heritage Act* was the first in Hamilton in five years.

He has written text for commemorative plaques, including several for the Ontario Heritage Trust, and has planned an extensive program to interpret the history of the Freeport Sanatorium at the Grand River Hospital in Kitchener. His major work in 2011, a history of the Legislative Building in Queen's Park and a statement on its cultural heritage value, forms part of an historic structure report commissioned by the Legislative Assembly of Ontario. In 2016, he prepared a strategic conservation plan for the Hamilton GO Centre Station, formerly, the Toronto, Hamilton & Buffalo Railway Station. Its historic significance is recognized in the station property's designation under the *Ontario Heritage Act* by the City of Hamilton, a rating as a Provincial Heritage Property of Provincial Significance and designation under the *Heritage Railway Stations Protection Act* by the Historic Sites and Monuments Board of Canada. In 2017, his report in support of the designation of Belfountain Conservation Area under the *Ontario Heritage Act* was adopted by the Town of Caledon.

Paul Dilse is qualified as a planner and historian by the Canadian Association of Heritage Professionals, of which he is a founding member.